



MEETING NOTICE

**TUALATIN ARCHITECTURAL REVIEW BOARD
FEBRUARY 6, 2017
POLICE TRAINING ROOM
8560 SW TUALATIN RD
TUALATIN, OR 97062**

- 1. CALL TO ORDER**
Staff: Aquilla Hurd-Ravich, Planning Manager; Erin Engman, Assistant Planner.
Members: Skip Stanaway, John Howorth, Chris Goodell, Patrick Gaynor, Carol Bellows, Angela Niggli.
Alternate: John Medvac
- 2. APPROVAL OF JUNE 10, 2015 ARB MINUTES**
- 3. COMMUNICATIONS FROM THE PUBLIC (NOT ON THE AGENDA)**
- 4. PUBLIC HEARINGS**
 - A.** Legacy Health Meridian Park Patient Care Expansion Recommendation and Staff Report for AR-16-0009
- 5. COMMUNICATIONS FROM BOARD MEMBERS**
- 6. ADJOURNMENT**



MEMORANDUM

CITY OF TUALATIN

Architectural Review Board

2.

FROM: Lynette Sanford, Office Coordinator

DATE: 02/06/2017

SUBJECT: APPROVAL OF JUNE 10, 2015 ARB MINUTES

Attachments

June 10, 2015 ARB Minutes



City of Tualatin

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UNOFFICIAL

ARCHITECTURAL REVIEW BOARD -

MINUTES OF June 10, 2015

MEMBERS PRESENT:

Skip Stanaway
John Howorth
Robert Perron
Chris Goodell

STAFF PRESENT:

Aquilla Hurd-Ravich
Clare Fuchs
Lynette Sanford

MEMBER ABSENT: Ed Truax, John Medvec, Terry Novak, Michael Ward

GUESTS: Adam Solomonson, Matt Oyen, Eric Sporre, Ryan McGuire, Bill Mullins, Mark Person, and Mike Swenson.

1. CALL TO ORDER AND ROLL CALL:

Mr. Stanaway called the meeting to order at 6:44 pm. Mr. Stanaway stated that a Vice Chair should be elected in the event the Chairman is unable to attend. Mr. Perron nominated Mr. Stanaway, which was SECONDED by Goodell. MOTION PASSED 4-0.

2. APPROVAL OF MINUTES:

Mr. Stanaway asked for review and approval of the August 13, 2014 ARB Minutes. MOTION by Howorth SECONDED by Perron to approve the minutes as written. MOTION PASSED 4-0.

Mr. Stanaway read the script for Quasi-Judicial Hearings. Mr. Stanaway asked the board members if they had a conflict of interest, bias or ex parte contact with the applicant. Mr. Howorth mentioned that he knows the applicant but it will not affect his decision. Mr. Stanaway noted that Pactrust is his landlord, but it will not have an influence on his decision.

3. COMMUNICATION FROM THE PUBLIC (NOT ON THE AGENDA):

None

4. PUBLIC HEARINGS:

A. Koch Manufacturing Expansion Project Recommendation and Staff Report for AR-15-05.

These minutes are not verbatim. The meeting was recorded, and copies of the recording are retained for a period of one year from the date of the meeting and are available upon request.

Clare Fuchs, Senior Planner presented the staff report for the Koch Expansion which included a PowerPoint presentation. Ms. Fuchs explained that the applicant is requesting approval to develop an approximately 310,000 square foot industrial expansion to the existing Koch industrial campus in three buildings in three phases. The five platted lots total 20 acres. The elements of review are the parking lot, architecture and screening, recreational trail connection, landscaping, bike parking, trash plan and enclosures, lighting and setbacks

Ms. Fuchs explained that the Architectural Review Board (ARB) needs to review this proposal per TDC 73.030 which states that all industrial developments with a building of 150,000 square feet or larger needs to be reviewed by the ARB.

Ms. Fuchs went through the slides which detailed the vicinity map, existing conditions, site plans, and elevations. Ms. Fuchs stated that key issues include:

- Bike/Pedestrian Stubs: SW 115th Ave is a Collector Street and TDC 73.160(1)(b) requires an access between the on-site pedestrian walkway and any current bike lane.
- Safety and Security: TDC 73.160(3) requires windows and lighting so that tenants, employees, and police watch over parking and loading areas. This proposal does not have sufficient windows to meet this requirement.
- Trash Enclosures: TDC 73.227(6)(a)(v) requires the trash enclosure to be centrally located and viewable from the building.

Ms. Fuchs noted that there have some revisions to the staff report and went through the revisions with the ARB members.

Ms. Fuchs stated that staff recommends the ARB consider the three key issues and formulate acceptable conditions of approval along with staff and the applicant. The ARB has four options:

- Approve with staff recommended finding and conditions of approval
- Approve with amended findings and conditions of approval
- Deny the application
- Continue the hearing

Mr. Stanaway inquired about condition AR-2-W regarding the easement to accommodate a 14-foot public bike and pedestrian shared use pathway and asked if this is under the ARB prevue or Community Services. Ms. Fuchs responded this is to be reviewed by the Community Services Department.

Mark Person & Adam Solomonson, Mackenzie, 1515 SE Water Ave, Suite 100, Portland, OR 97214

Mr. Person went through a PowerPoint presentation of the Koch expansion proposal. Mr. Person stated that they have been in contact with Community Services and are

eager to begin construction. The above mentioned bike and pedestrian pathway has to be negotiated between Washington County, Metro, and PacTrust. There is space currently set aside for the trail, but no design. Until it's designed, they feel the building permits should not be held up.

Mr. Solomonson went through the slides which detailed the site, exterior elevations, and contextual building photos. Mr. Solomonson pointed out that PacTrust is proposing the new buildings remain consistent with the existing buildings. Mr. Person acknowledged that there are a number of options for the shared use pathway. Mr. Person added that windows can be a challenge for an industrial building, but they are open to security cameras as a viable option. Mr. Person stated the trash enclosures are limited to the back of building due to the code requirement and ease and accessibility of the multiple tenants. Security and safety is a priority for the applicant and typically the operating times will be during the daylight hours.

Michael Swenson, 11730 118th Ave NE, Suite 600, Kirkland WA 98039

Mr. Swenson stated that the Traffic Impact Analysis was consistent with Washington County and City of Tualatin standards. They reviewed the intersection level of service and safety, identified conditions with and without the project, and identified mitigation measures. Mr. Swenson went through the slides which detailed the existing transportation conditions of the intersections of Tualatin-Sherwood Rd. Peak volume periods between the hours of 7-9 am and 4-6 pm were analyzed and recorded. The proposal includes widening the intersection to a dual left turn from Tualatin-Sherwood Rd onto 115th and to widen 115th to allow three lanes to merge into internal intersections.

Mr. Goodell inquired about the median. Mr. Swenson responded that the median extends just beyond the first driveway and is there to prohibit left turns. Mr. Howorth inquired about the percentage difference between trucks versus vehicles. Mr. Swenson replied he did not have that number; it is dependent upon the tenants and the performance time periods. They would expect a higher number of trucks during the non commuter peak hours.

Matt Oyen, Pactrust, 15350 SW Sequoia Pkwy, Portland, OR 97224

Mr. Oyen stated that the larger building can be used for distribution use. They have found that their traffic patterns consist of light to moderate truck use when vehicles are on the road.

Mr. Person acknowledged that the applicant is eager to begin construction and wants AR-2 revised to strike "or grading" since the site was previously mass graded. Ms. Fuchs added that some of the items in AR-2 require revisions to the grading plans. Discussion followed.

Mr. Stanaway inquired about the façade of the buildings and stated that the goal should be to break up the scale of a large structure. Mr. Solomonson replied that they tried to scale the entry. Mr. Stanaway thought they could use additional variation through paint

colors and landscaping and he noted the landscaping was repetitious.

Mr. Perron agreed with the repetition in the landscaping plan. He felt the types of plants proposed at this development are used primarily for residential areas and are too small for the scale of these buildings. He would like the applicant to consider plants with vertical mass. Mr. Perron added that the applicant should consider plant material that requires less water such as large trees.

Mr. Goodell inquired about keeping parking and loading safe and secure in the absence of windows. Ms. Fuchs stated that the code states that they need to have enough windows so employees can see inside. In this case, security cameras can be an option so the staff inside would be able to monitor the parking and loading areas.

After deliberation, the board members agreed to approve AR-15-05 with the following revisions:

1. Accept all markups based on the Revised Staff Report dated June 10, 2015, including AR2-W regarding the public bike and pedestrian easement.
2. AR2-W will be moved to AR-7 to be tied to the first tenant Certificate of Occupancy, or within 6 years of the date of this decision, whichever comes first.
3. Move AR2-K to AR1-C.
4. Strike "or grading" from AR-2.
5. Propose a condition to work with staff through materials, placement, material type, landscape grouping, and paint colors to breakdown the horizontal and vertical scales.
6. Propose a condition to work with staff to modify the landscape palate depending on the four sides of the building. Recommendation to simplify the landscape plan. Entrances to have a variety and color of landscape The long facades should have a simple pallet with large masses of plantings, trees (both coniferous and deciduous) to break up the long facades.

MOTION by Howorth, SECONDED by Perron to approve with the conditions as stated above. MOTION PASSED 4-0.

5. COMMUNICATION FROM BOARD MEMBERS:

None

6. ADJOURNMENT

MOTION by Stanaway to adjourn the meeting at 8:48 pm.

Lynette Sanford, Office Coordinator



MEMORANDUM

CITY OF TUALATIN

Architectural Review Board

4. A.

FROM: Erin Engman, Associate Planner

DATE: 02/06/2017

SUBJECT: Legacy Health Meridian Park Patient Care Expansion Recommendation and Staff Report for AR-16-0009

ISSUE BEFORE ARCHITECTURAL REVIEW BOARD:

Consideration to approve the Architectural Review AR-16-0009 proposal for a 163,203 square foot expansion of the existing Meridian Park Hospital. The expansion is proposed to be four stories tall and approximately 65 feet in height.

EXECUTIVE SUMMARY:

The applicant, PKA Associates on behalf of Legacy Health proposes a 163,203 square foot expansion to the existing Meridian Park Hospital. The project will relocate 96 patient beds from the current hospital building to the new expansion. A new lobby and check-in area are proposed to serve as the main entrance for both the proposed and existing hospital buildings. The remainder of the proposed main floor will be shelled space for future development. The second floor will contain shelled space for future patient beds or expanded hospital programs. The third and fourth floors will each contain 48 patient beds along with associated support spaces.

In addition to the proposed building, planned improvements include new pedestrian areas, reconfigured and new parking lots with associated grading, drainage, and lighting, and a relocated and expanded bus stop. The proposed development area, including the new building footprint and parking totals 586,646 square feet; approximately 34% of the total campus area of 1,741,529 square feet.

The project necessitates Architectural Review Board (ARB) review and decision because the commercial (medical facility) expansion is over 50,000 square feet, pursuant to TDC 73.030(2).

DISCUSSION:

Staff recommends the ARB approve AR-16-0009 with conditions of approval to meet the Tualatin Development Code (TDC) requirements.

The proposal generally reflects the TDC standards. The following conditions are highlighted because they could result in an adjustment to the proposed site plan. These conditions of approval will modify the Proposed Site Plan, Sheets A-14 to 21 (dated October 17, 2016):

1. Two accessway connections: one at SW 65th Avenue and one at SW Borland Road
2. Parking bumpers or wheel stops where pedestrian walkways cut through parking aisles in Lots A, C, and L

3. Modify Lot L to provide a 24 ft wide aisle between parking stall lines
4. Review of Borland Road frontage improvements to ensure plan set revisions comply with all applicable policies and regulations of the TDC including, but not limited to: 34.210, 56.050(3), 73.340, 73.360, 73.370, 73.380, 73.400.

Staff also asks that the Architectural Review Board consider approving a reduction in bicycle parking requirements. Pursuant to TDC 73.370(1)(n), the ARB may approve a form of bicycle parking not specified in the TDC provisions but that meets the needs of the long-term and/or short-term parking. The applicant finding provides evidence that the proposed bicycle parking exceeds long-term parking needs. Staff recommends additional conditions of approval that are included in Attachment 100 ARB Staff Report. The additional conditions are not expected to modify the site or design of the proposal.

RECOMMENDATION:

Staff recommends that the ARB approve AR-16-0009 with recommended findings and conditions of approval. The Architectural Review Board has four options:

1. Approve with staff recommended findings and conditions of approval
2. Approve with amended findings and conditions of approval
3. Continue the Hearing
4. Deny the application

Attachments

Attachment 100: ARB Staff Report
Attachment 101: Application Materials
Attachment 102: Republic Services letter
Attachment 103: Tualatin Valley Fire & Rescue (TVF&R) letter
Attachment 104: CWS letter
Attachment 105: Clackamas County letter
Attachment 106: Map 72-1
Attachment 107: Presentation



City of Tualatin

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February 6, 2017

STAFF REPORT RECOMMENDATION TO THE ARCHITECTURAL REVIEW BOARD

Case #:	AR-16-0009
Project:	Meridian Park Patient Care Expansion
Location:	19300 SW 65 th Avenue (Tax Lots: 21E-19C-01300, 01400, 01500, and 01600)
Applicant:	PKA Associates; Jim Knees, 6969 SW Hampton Street, Portland, OR 97223
Project Contact:	PKA Associates; Jim Knees, 6969 SW Hampton Street, Portland, OR 97223

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Arrangements can be made to provide these materials in alternative formats such as large type or audio recording. Please contact the Planning Division at 503.691.3026 and allow as much lead time as possible.

I. INTRODUCTION

A. Project Description

The applicant, PKA Associates on behalf of Legacy Health proposes a 163,203 square foot expansion to the existing Meridian Park Hospital. The project will decommission 96 patient beds in the current hospital building and relocate them to the new expansion. Existing programs will use the vacated space in the existing hospital.

The expansion will be four stories tall and approximately 65 ft in height. The main floor will contain a new lobby and check-in area that will serve as the main entrance for both the proposed and existing hospital buildings. The remainder of the proposed main floor will be shelled space for future development. The second floor will contain shelled space for future patient beds or expanded hospital programs. The third and fourth floors will each contain 48 patient beds along with associated support spaces.

In addition to the proposed building, planned improvements include new pedestrian areas, reconfigured and new parking lots with associated grading, drainage, and lighting, and a relocated and expanded bus stop. The proposed development area, including the new building footprint and parking totals 586,646 square feet; approximately 34% of the total campus area of 1,741,529 sf. The proposed site development areas and a breakdown by type of development is summarized on Sheet A14, dated October 17, 2016. Subsequent sheets A15 through A21 (dated October 17, 2016) show proposed site development by subarea at an enlarged scale.

New pedestrian areas are being provided around the proposed building. The areas are being designed as healing landscapes using the American Horticultural Therapy Association therapeutic garden characteristics and the new landscapes will complement the existing Therapy Garden located at the Hospital. The new plantings are designed to create a four season garden with a variety of evergreen and deciduous plantings that are both native and ornamental. Storm water planters are incorporated into the new landscape areas. New trees are located throughout the landscape areas to provide shade for the new pedestrian areas.

A variety of seating will be provided throughout the new landscape. On the south side of the new building curving walks provide access to a series of seating areas that connect the new building to the exiting campus. On the north side of the new building a curving walk connects new seating areas and lawn spaces to the existing MOB (Medical Office Buildings) and hospital. In addition, new pedestrian pathways will connect the proposed improvements with existing pedestrian pathways that lead to other campus buildings.

The proposed building removes a portion of two existing parking lots noted as A1 and A2 on Sheet A6 Overall Existing Site Plan, revised October 17, 2016. The parking spaces displaced by the proposed building and those needed to meet the requirements of the proposed new shell space will be relocated to reconfigured existing lots noted as A and B and in a new lot adjacent to the existing Lot K in the Northeast corner of the campus. The new and reconfigured existing lots are shown on Sheet A14 Overall Proposed Site Plan (dated October 17, 2016).

The project will potentially be broken into two phases. The first phase will be providing the new northeast parking lots "J", "K" and "L". The hospital then plans on temporarily relocating the main entrance of the hospital to this northeast entrance. After this work is complete the hospital will close the existing Main and North entrances to begin the construction of the proposed expansion and parking in the southwest corner of the site.

Table 1. Site Data	
<i>Attribute</i>	<i>Total</i>
Site Area (sf)	1,741,529
Site Area (acres)	39.98
Development Area (sf)	586,646
Impervious Area (sf):	408,964
Building Footprint	39,341
Parking/Sidewalks	369,623
Landscape Area (sf)	177,682
Landscape Area (percent)	30.3
Parking Spaces Total:	
Standard	1,187
Compact	358
Van/Carpool	69
ADA-Compliant	98
RV	4
Bicycle	27

B. Site Description

The 39.98-acre site is located in the Medical Center (MC) planning district. The proposed hospital building expansion is a permitted use in this zone. The site is largely developed, and is surrounded by land zoned residential and office commercial. The Tualatin River is located approximately 900 ft north of the property.

The site takes access from SW 65th Avenue via two driveways on the northwest and southwest side of the site. Additional access from SW Borland Road is provided on the southern end of the property by two driveways. There are two pedestrian access point located along SW 65th Avenue and one located along SW Borland Road. The site is fully-served with water, sanitary sewer, stormwater, and power.

C. Project Schedule

The neighborhood/developer meeting was held on Monday, April 25, 2016, at 5:00 PM at the Legacy Meridian Community Health Education Center located at 19300 SW 65th Avenue, Tualatin, OR, 97062. Staff mailed out notice of this ARB public hearing on January 17, 2017.

II. RECOMMENDED FINDINGS

A. Previous Related Land Use Actions

- Previous Architectural Reviews include: 80-28, 83-01, 83-25, 83-26, 83-33, 88-04, 88-13, 89-35, 90-42, 92-04, 92-17, 93-19, 94-25, 95-15, 96-22, 96-23, 98-04, and 01-29.
- Previous Conditional Use permits include: 1970, 80-03, 83-05, 83-06, 84-03, and 89-06.
- Other actions include LP80-04 and PMA91-01.

B. Planning Districts and Adjacent Land Uses

The subject property is located in the Medical Center (MC) Planning District where medical center uses including hospital, office, and laboratories are permitted pursuant to Tualatin Development Code (TDC) 56.020.

Adjacent planning districts and land uses are:

North:	Medical Center (MC) Low-Density Residential (RL)	Brookdale River Valley: Three-story assisted living facility at 19200 SW 65 th Avenue and vacant land
East:	Low-Density Residential (RL)	Vacant land
South:	Office Commercial (CO) Low-Density Residential (RL)	Tualatin Professional Center: Two-story medical building at 6476 SW Borland Road, Mei Building: Two-story medical building at 6370 SW Borland Road, and PGE substation
West:	Office Commercial (CO) High Density Residential (RH)	Professional Building: Two-story medical building at 19365 SW 65 th Avenue, Rolling Hills apartment complex at 19355 SW 65 th Avenue, and Meridian Village apartment complex at 19655 SW 65 th Avenue

C. Uses

56.020 Medical Center Planning District (MC) Permitted Uses

No building, structure or land shall be used, except for the following uses when conducted wholly within a completely enclosed building, except for utility facilities and wireless communication facilities, and provided retail uses on land designated Employment Area, Corridor or Industrial Area on Map 9-4 shall not be greater than 60,000 square feet of gross floor area per building or business:

(1) Medical center:

(a) hospital, including but not limited to diagnosis and treatment of sick and injured persons on an inpatient and outpatient basis; surgical, emergency, laboratory, imaging, pharmacy and physical therapy services; and facility maintenance, laundry, worship, food service and administrative functions,

(b) offices of physicians and dentists,

(c) offices and clinics of allied health care providers, including but not limited to nurse practitioners; midwives; dietitians; psychologists; opticians; physical and occupational therapists; occupational health/safety specialists; substance abuse counselors; chiropractors; and wellness centers including physical fitness facilities, nutritional counseling, health maintenance and rehabilitation services,

(d) durable medical goods sales and rentals,

(e) nursing school and other medical training facilities,

(f) nurses' and house staff physicians' housing,

(g) medical and dental laboratories,

(h) medical and dental related research laboratories and testing facilities,

(i) medical and dental nonprofit educational, charitable and research organizations and facilities,

(j) congregate care facilities, assisted living facilities and residential facilities in accordance with TDC 56.080,

(k) nursing and convalescent homes,

(l) community meeting facilities, and

(m) parking lot, parking structure, or underground parking.

Applicant Response: The relocated existing and proposed future patient care beds meet the definition of (1) Medical center (a) hospital. The future uses to be located in proposed shell

space will meet the definition of Medical Center (a through d.) The proposed parking meets the definition of Medical Center (m).

Staff notes that this requirement has been met.

D. Lot Sizes

56.040 District Size

The minimum MC District size shall be 25 acres.

Applicant Response: The entire Legacy Meridian Park Medical Center campus totals 1,741,529 square feet or 39.98 acres. Two areas of development are proposed: 234,088 square feet in the NE corner and 352,558 square feet in the SW corner of the campus. The proposed new building is located in Subarea B (See Sheet A-16) of the SW corner, the remaining subareas in both the NE and SW corners are proposed improvements related to parking.

Staff finds that this requirement is met.

E. Setback Requirements

56.050 Distances From Structures to MC District Boundaries and Setbacks

(1) The minimum distance from a property line which abuts a MC District Boundary to any structure in the MC District shall be no less than 50 feet.

Applicant Response: SW 65th Avenue and SW Borland Road form the west and south boundaries of the MC District respectively. Residential zoned land (RL) abuts the MC boundary on the north and east sides of the campus. Measured from the point closest to the boundary, the proposed new structure, which is surrounded on the north, east and south sides by existing campus buildings, is setback from the MC boundary as follows:

Table 2. Expansion Setbacks			
	<i>Direction</i>	<i>Min Required</i>	<i>Setback from boundary</i>
<i>Proposed</i>	North	50	1,045
	East	50	540
	South	50	411
	West	50	356

Staff referred to Overall Proposed Site Plan, Sheet A-14 (dated October 17, 2016) to review property lines in relation to district boundary lines. The applicant has illustrated two properties. The area of work is located on the western property that is within the MC district. This property is bordered by RL district to the north, SW 65th Avenue to the west, SW Borland Road to the south, and the second illustrated property to the east which is within the RL district. As shown in Table 2, the new structure will be setback at distances greater than 50 ft from property lines. Staff finds that this requirement is met.

(2) Setbacks from property lines within the MC District that do not abut a MC District Boundary shall be determined through the Architectural Review process.

Applicant Response: While multiple lots currently exist, all are within the campus and owned by Legacy. See Section 56.050 (1) for setback distances to property lines at the campus boundary.

Legacy Meridian Park Hospital has illustrated two properties on Sheet A-14. The area of work is located entirely within the MC District and has met the requirements of TDC 56.050(1). Staff finds that this requirement is met.

(3) Off-street parking and vehicular circulation areas in the MC District shall be set back a minimum of 10 feet from any public right-of-way or property line which abuts a MC District

boundary. Off-street parking and vehicular circulation areas shall be setback a minimum of 5 feet from any property line within the MC District that does not abut a MC District boundary, except as determined through the Architectural Review process.

Applicant Response: All proposed reconfigured and new parking lots exceed the minimum setback. The following lists the setback for each lot as measured at the point closest to a public right of way or property line.

Table 3. Parking Setbacks		
<i>Parking Lot</i>	<i>Direction</i>	<i>Setback</i>
Lot A	West	106 (Sheet A-15)
Lot B	West	14.5 (Sheet A-17)
Lot C	South	33 (Sheet A-18)
Lot K	East	10 (Sheet A-19)
Lot J	East	38 (Sheet A-20)
Lot J	North	233 (Sheet A-20)

Legacy Meridian Park Hospital has illustrated two properties on Sheet A-14. The area of work is located entirely within the MC District. Staff finds that parking and vehicular circulation areas meet the 10 ft setback requirement.

(4) No fence shall be constructed within 5 feet of a public right-of-way.

No fencing is proposed. Staff finds that this requirement is not applicable.

(5) Setbacks for a wireless communication facility shall be established through the Architectural Review process, shall consider TDC 73.510, shall be a minimum of 50 feet to a property line which abuts an MC District boundary, and shall be set back from an RL District, or an RML District with an approved small lot subdivision, no less than 175 feet for a monopole that is no more than 35 feet in height and the setback shall increase five feet for each one foot increase in height up to 80 feet in height, and the setback shall increase 10 feet for each one foot increase in height above 80 feet.

No wireless communication facilities are proposed. Staff finds that this requirement is not applicable.

F. Structure Height

56.070 Structure Height

(1) No structures are permitted within 50 feet of a property line abutting a MC District Boundary.

Applicant Response: Proposed structure is more than 50 feet from the MC District Boundary.

Staff finds the proposed structure is located at distances greater than 356 ft from the MC Boundary. This requirement is met.

(2) The maximum height for a structure within the area 50.01 to 100 feet of a property line abutting a MC District Boundary shall be no greater than 25 feet in height above grade.

Applicant Response: Proposed structure is more than 300 feet from the MC District Boundary.

Staff finds that this requirement is met.

(3) The maximum height for a structure within the area 100.01 to 300 feet of a property line abutting a MC District Boundary shall be no greater than 45 feet in height above grade.

Applicant Response: Proposed structure is more than 300 feet from the MC District Boundary.

Staff finds that this requirement is met.

(4) The maximum height for a structure within the area greater than 300 feet from a property line abutting a MC District Boundary shall be no greater than 95 feet in height above grade.

Applicant Response: The top of the parapet of the building is approximately 65 ft.

Staff notes that an 18 ft penthouse mechanical unit is proposed on Sheet A-22 (dated October 17, 2016), putting the total height of the proposed structures at 83 ft which meets the maximum height requirement.

(5) Notwithstanding (1)-(4) above, flag-poles displaying the flag of the United States of America, either alone or with the State of Oregon flag, shall be no greater than 100 feet in height above grade, except in the area within 50 feet of a property line abutting a MC District Boundary where no flagpole shall be allowed.

Applicant Response: Proposed flag poles are 30 ft high located near the main entry which is more than 50 ft from the MC District Boundary. See Sheet A-16.

Staff finds that this requirement is met.

(6) Notwithstanding (1)-(4) above the maximum structure height for a wireless communication support structure and antennas shall be no greater than 100 feet.

Applicant Response: No wireless communication towers are proposed.

Staff finds that this requirement is not applicable.

56.080 Special Standards

Special standards shall apply to congregate care, assisted living and residential facilities [56.020(1)(j)] as follows:

Staff notes that proposed uses fall within 56.020(1)(a-d). These special standards do not apply to the proposal.

56.090 Special Standards

Special standards shall apply to supporting retail and service uses [56.020(2)] as follows:

(1) Notwithstanding Section 56.020 which requires uses to be conducted wholly within a completely enclosed building, a restaurant, delicatessen or coffee shop may include an outdoor eating area, provided the outdoor area consists on an all-weather surface not greater than 900 square feet in gross floor area and screened from public rights-of-way and residential planning districts by a completely sight-obscuring evergreen hedge and/or fence as determined through the Architectural Review process.

Staff notes that a coffee bar is proposed on the main floor of the proposed expansion on Sheet A-27 (dated August 5, 2016). No outdoor eating is proposed. This requirement does not apply to the proposal.

(2) Drive-up windows and drive-through services shall not be permitted. Loading and unloading at the vehicle entrance to the emergency room is not a drive-through service.

Staff notes that no drive-up or drive-through are proposed in addition to the vehicle loading area at the main entrance. This requirement is not applicable.

(3) The use shall have pedestrian access only and, except for emergency access required by the Uniform Building Code and secondary access to outdoor eating areas, it shall be through an interior hall or lobby of the building in which the use is located.

Staff notes that the coffee bar is proposed adjacent to the lobby and waiting room areas. This requirement has been met.

(4) The floor area of one use, or a combination of uses, listed in TDC 56.020(2) in any one building shall not exceed ten (10) percent of the gross floor area of the building.

The coffee bar is approximately 96 sq ft of the 163,203 sq ft expansion. This requirement has been met.

G. Site Planning

73.050 Criteria and Standards

(1) In exercising or performing his or her powers, duties, or functions, the Planning Director shall determine whether there is compliance with the following:

(a) The proposed site development, including the site plan, architecture, landscaping, parking and graphic design, is in conformance with the standards of this and other applicable City ordinances insofar as the location, height, and appearance of the proposed development are involved;

Applicant Response: The proposed building is located and connected to the existing hospital on the Legacy Meridian Park campus. The building will house patient care beds to be relocated from the existing hospital building. The proposed building will not increase the number of licensed care beds on campus but instead will allow for a focus on individualized care and rooms designed to accommodate only one patient at a time. The building is sited so that staff and services located in the existing hospital are easily accessible to the new rooms through internal connections at each of the four new floors. The proposed building will also include shell space for patient beds anticipated to be needed in the future.

In addition to the relocated and future patient care rooms, the proposed building features a glass-walled two story lobby reception area on the southwest corner of the first floor. This new entrance lobby area will be visible as motorists approach the hospital on the internal campus loop road as they approach the hospital from any of the three campus entrances. The proposed entrance is designed to be an inviting and welcoming lobby and will be lighted at night to function as a softly lit beacon guiding patients and visitors to the front door of the hospital.

Building height is addressed in the narrative responding to Chapter 56 requirements. How the proposed development complies with site, landscaping and parking standards is addressed in the applicable sections of Chapter 73 and Chapter 72. Graphic design, as it relates to signage will be reviewed at sign application submittal.

Staff notes that the standard has been met.

(b) The proposed design of the development is compatible with the design of other developments in the general vicinity; and

Applicant Response: The overall building design reflects internal program needs. The upper floors are shaped in an "H" to provide a window in each patient room. (Sheets A 29 and A 30) On the lower floors, (A27 and A28) the southwest corner of the building is cut away to provide a covered drop off zone in front of the entry and to create a two story glassed lobby/entry area. (It is a state health department requirement to provide covered entry space.) The proposed building is connected to the main hospital building at the entry level and ground level. (A26)

Staff notes that the expansion has been designed to be compatible with the existing hospital structure with similar brick veneer and metal paneling.

(c) The location, design, size, color and materials of the exterior of all structures are compatible with the proposed development and appropriate to the design character of other developments in the vicinity.

Applicant Response: The proposed building is surrounded on three sides by existing campus buildings. These existing buildings minimize the view of the proposed building from residential uses to the north and to east of campus. In addition, there are parking lots, extensive landscaping and a substantial grade change between the proposed building and commercial development across SW 65th Avenue and SW Borland Road. Because of these site conditions, the proposed building design and materials most relate to other nearby campus buildings. Most of the building is covered with a brick veneer that matches the color and materials of the existing Medical Office Building located to the north and west of the proposed building. The brick continues the intent, begun with the Medical Office Building, to use more appropriate and substantial appearing brick as the signature campus material rather than less climate appropriate dryvit/stucco used on older campus buildings. On the floors directly above the new entrance lobby area, the brick facade is broken with a metal panel and aluminum curtain wall glazed with spandrel glass in various shades of gray to lighten the weight of the building and emphasize the welcoming nature of the hospital entrance.

Staff notes that the standard has been met.

(2) In making his or her determination of compliance with the above requirements, the Planning Director shall be guided by the objectives and standards set forth in this chapter. If the architectural review plan includes utility facilities or public utility facilities, then the City Engineer shall determine whether those aspects of the proposed plan comply with applicable standards.

Staff notes the application includes architectural features as well as utility facilities and public improvements. This staff report with recommended findings reviewed the application in a manner consistent with City Code.

(3) In determining compliance with the requirements set forth, the Community Development Director shall consider the effect of his or her action on the availability and cost of needed housing. The Community Development Director shall not use the requirements of this section to exclude needed housing types. However, consideration of these factors shall not prevent the Community Development Director from imposing conditions of approval necessary to meet the requirements of this section. The costs of such conditions shall not unduly increase the cost of housing beyond the minimum necessary to achieve the purposes of this Code. As part of the Architectural Review process, the Community Development Director has no authority to reduce dwelling unit densities.

The proposed development does not include housing. This requirement does not apply.

(4) As part of Architectural Review, the property owner may apply for approval to cut trees in addition to those allowed in TDC 34.200. The granting or denial of a tree cutting permit shall be based on the criteria in TDC 34.230.

Applicant Response: Of the 396 trees measuring 8 inches or more within the proposed development areas, 246 are proposed for removal. Refer to narrative responding to Chapter 34.

Staff notes that tree removal is detailed in the Arborist's report authored by Morgan Holen & Associates and revised July 29, 2016, as well as Sheets C-7.10 through C-7.70 (dated August 5, 2016).

(5) Conflicting Standards. In addition to the MUCOD requirements, the requirements in TDC Chapter 73 (Community Design Standards) and other applicable Chapters apply. If TDC

Chapters 57, 73 and other applicable chapters, conflict or are different, they shall be resolved in accordance with TDC 57.200(2).

The subject site is not within the Mixed Use Commercial Overlay District (MUCOD). This requirement does not apply.

73.100 Landscaping and Building Installation and Maintenance

(1) All landscaping approved through the Architectural Review Process shall be continually maintained, including necessary watering, weeding, pruning and replacement, in a manner substantially similar to that originally approved through the Architectural Review Process, unless subsequently altered with Community Development Director approval.

Staff has conditioned the applicant to retain all trees identified on Sheets L-2 to L-7 (dated August 5, 2016), unless approval is obtain through the Community Development Department to remove trees through a modification to the landscape plan. A note has also been added to this decision to meet the standard.

(2) All building exterior improvements approved through the Architectural Review Process shall be continually maintained including necessary painting and repair so as to remain substantially similar to original approval through the Architectural Review Process, unless subsequently altered with Community Development Director approval.

A note has been added to this decision to meet the standard.

73.160 Standards

The following standards are minimum requirements for commercial, industrial, public and semi-public development, and it is expected that development proposals shall meet or exceed these minimum requirements.

(1) Pedestrian and Bicycle Circulation:

(a) For commercial, public and semi-public uses:

(i) a walkway shall be provided between the main entrance to the building and any abutting public right-of-way of an arterial or collector street where a transit stop is designated or provided. The walkway shall be a minimum of 6 feet wide and shall be constructed of concrete, asphalt, or a pervious surface such as pavers or grasscrete, but not gravel or woody material, and be ADA compliant, if applicable;

Applicant Response: The main entrance of the proposed building does not directly abut a public right-of-way. There is an existing public transit stop located on campus. This transit stop, designed with input from TriMet design and construction, will be relocated and enlarged to accommodate two buses. The proposed stop includes a paved 8 foot wide transit passenger landing pad to accommodate wheelchairs and persons with disabilities. It also includes a 4 space bike rack. The transit stop is lit with pole lighting similar to existing campus lighting.

Proposed concrete, six foot wide sidewalks extends north from the transit stop along the eastern edge of the reconfigured Parking Lot A to the concrete crosswalk that accesses the front entry and along the building entry facade.

In addition, a painted striped crosswalk at the east end of the transit stop connects to new six foot concrete pathways extending along the north and east edges of Parking Lot B to connect with SW 65th Avenue and Borland Road respectively.

Staff finds that both SW 65th Avenue and SW Borland Road are designated Major Arterials. The transit stop is located off the loop road on-site on the southwest corner of Lot A on Sheet A-15 (dated October 17, 2016). A 6 ft wide sidewalk is shown on Sheet A-17 that

connects SW Borland Road through the eastern edge of Lot B to the transit stop then to the new building entrance. A 6.5 ft wide sidewalk also connects the southern driveway at SW 65th Avenue to the transit stop and then to the new building entrance on Sheet A-15 (dated October 17, 2016). This standard is met.

(ii) walkways shall be provided between the main building entrances and other on-site buildings and accessways. The walkways shall be a minimum of 6 feet wide and shall be constructed of concrete, asphalt, or a pervious surface such as pavers or grasscrete, but not gravel or woody material, and be ADA compliant, if applicable;

Applicant Response: The six foot concrete walkway along the eastern edge of Parking Lot A mentioned in the section above continues past the main entry to connect with the existing pathways connecting the Medical Office Buildings to the northwest of the proposed building. The proposed six foot concrete walkway running along the front of the proposed building connects into existing pathways that connect all campus buildings to the north and to the south. The proposed hard surfaced pathways that curve through the therapeutic garden landscapes on the north, south, and west sides of the proposed building connect into the existing campus walkway network and to the secondary side entrances of the existing hospital building.

Staff notes that not all walkways are proposed to be 6 ft wide. As noted on Sheet A-16 (dated October 17, 2016), a 5 ft 9 in sidewalk is proposed north of the Radiation Oncology building. Staff recommends iteration of this standard as a condition of approval.

Condition of Approval: *The applicant shall revise the appropriate sheets to illustrate that walkways are a minimum of 6 feet wide and install to revised plans.*

(iii) walkways through parking areas, drive aisles, and loading areas shall be visibly raised and of a different appearance than the adjacent paved vehicular areas;

Applicant Response: All new proposed walkways will be concrete to differentiate from the asphalt paving in vehicular areas.

Staff notes that not all proposed walkways are concrete. Striped walkways are proposed where walkways cross the loop road on Sheet A-14 (dated October 17, 2016) and where ADA parking in Lot C connects to offices to the north on Sheet A-18 (dated October 17, 2016). Staff also notes that no treatment is proposed between the ADA parking in Lot L to the east entrance on Sheet A-19 (dated October 17, 2016). Staff recommends iteration of this standard as a condition of approval.

Condition of Approval: *The applicant shall revise the appropriate sheets to illustrate that walkways through parking areas, drive aisles, and loading areas are visibly raised and of a different appearance than the adjacent paved vehicular areas or ADA compliant and install to revised plans.*

(iv) accessways shall be provided as a connection from the development's internal bikeways and walkways to all of the following locations that apply: abutting arterial or collector streets upon which transit stops or bike lanes are provided or designated; abutting undeveloped residential or commercial areas; adjacent undeveloped sites where an agreement to provide an accessway connection exists; and to abutting publicly-owned land intended for general public use, including schools;

Applicant Response: Bikeways are designated along SW 65th Avenue and SW Borland Road. These future bikeways can be accessed from the existing campus entrances to these streets. The proposed development area does not directly abut any residential land. Access to residential zoned land north of campus is blocked by the wetland area. Residential land to the east can be accessed from existing connections on the internal loop road.

Staff notes that SW 65th Avenue and SW Borland Road are designated Major Arterials with bike lanes and sidewalks on Figure 11-4: Bicycle and Pedestrian Plan. A transit stop is also located off the loop road on-site. Chapter 31 of the TDC defines accessways as “A non-vehicular, paved pathway designed for pedestrian and bicycle use and providing convenient linkages between a development and adjacent residential and commercial properties and areas intended for public use such as schools, parks, and adjacent collector and arterial streets where transit stops or bike lanes are provided or designated.” The applicant has illustrated 6.5 ft sidewalk connecting the southern driveway at SW 65th Avenue to the transit stop and then to the new building entrance on Sheet A-15 (dated October 17, 2016). A 6 ft wide sidewalk is shown on Sheet A-17 (dated October 17, 2016) that connects SW Borland Road through the eastern edge of Lot B to the transit stop then to the new building entrance. However, specific accessways that meet Section 73.160(1)(d) are not shown. Staff recommends iteration of this standard as a condition of approval.

Condition of Approval: *The applicant shall revise the appropriate sheets to illustrate that accessways are provided as a connection between the development’s internal walkways and abutting arterial streets upon which bike lanes are designated and install to revised plans. Accessways shall be a minimum of 8 ft wide and meet the standards set forth in Section 73.160(1)(d).*

Staff also notes that development site is abutting two undeveloped residential areas, one to the north and the other to the east. Access to the north property is constrained by challenging slopes and wetland as the applicant has stated. Legacy owns the property to the east.

(v) fences or gates which prevent pedestrian and bike access shall not be allowed at the entrance to or exit from any accessway.

Applicant Response: No fencing or gates are proposed.

Staff notes that this standard does not apply.

(vi) bikeways shall be provided which link building entrances and bike facilities on the site with the adjoining public right-of-way and accessways.

Applicant Response: Bikes can use the existing loop road, campus network of pathways, and campus entrances to access campus building entrances and bike facilities, including those proposed. Proposed bike facilities are shown on Sheet A14.

Chapter 31 of the TDC defines bikeways as “Any street, road, path or way open to bicycle travel regardless of whether such facilities are designated for the preferential use of bicycles or are to be shared with other transportation modes.” Bikes may use the loop road and accessways required through AR-5 to connect the building entrances, bike facilities, and public right-of-way. This standard is met.

(vii) Outdoor Recreation Access Routes shall be provided between the development's walkway and bikeway circulation system and parks, bikeways and greenways where a bike or pedestrian path is designated.

Applicant Response: A new proposed 6 foot concrete walkway is added to the two existing accesses along SW Borland Road. A new proposed 6 foot concrete walkway connects into the public sidewalk and entrance road at the main campus entrance on SW 65th Avenue.

Staff notes that the hospital is located 0.3 of a mile north of Atfalati Park. Figure 7 of the Tualatin TSP Update notes a future multi-use path is planned along SW 65th Avenue between Atfalati Park & the Tualatin River. The Community Services Division needs further

study of the multi-use path; therefore no comments have been provided regarding the future path. A bike lane is provided south of the hospital on SW 65th Avenue which connects to the park's entrance. Sidewalks also connect the hospital to the park. This standard is met.

(c) Curb ramps shall be provided wherever a walkway or accessway crosses a curb.

Applicant Response: Curb ramps are provided at all crosswalks that are raised from the asphalt paving area.

Staff notes that curb ramp details have not been provided as part of the plan set submittal. Generally curb ramps are depicted throughout the site; however staff noticed an instance where ramps are not shown. A curb ramp is not depicted at the perpendicular corner of the southwest corner of Lot J on Sheet A-20 (dated October 17, 2016). Staff recommends iteration of this standard as a condition of approval.

Condition of Approval: *The applicant shall revise the appropriate sheets to illustrate that curb ramps are provided wherever a walkway or accessway crosses a curb and install to revised plans.*

(d) Accessways shall be a minimum of 8 feet wide and constructed in accordance with the Public Works Construction Code if they are public accessways, and if they are private accessways they shall be constructed of asphalt, concrete or a pervious surface such as pervious asphalt or concrete, pavers or grasscrete, but not gravel or woody material, and be ADA compliant, if applicable.

Applicant Response: No new access ways are proposed.

The applicant was conditioned to provide accessways in AR-5 to meet the standard.

(e) Accessways to undeveloped parcels or undeveloped transit facilities need not be constructed at the time the subject property is developed. In such cases the applicant for development of a parcel adjacent to an undeveloped parcel shall enter into a written agreement with the City guaranteeing future performance by the applicant and any successors in interest of the property being developed to construct an accessway when the adjacent undeveloped parcel is developed. The agreement shall be subject to the City's review and approval.

Applicant Response: See narrative responding to 73.160 (1)(a)(iv).

Staff notes that there is an undeveloped RL zoned property to the north. Access to the north property is constrained by challenging slopes and wetland. Legacy owns the undeveloped RL zoned property to the east. The proposed area of work along the Borland frontage is not located adjacent to the eastern property; therefore no accessway agreement is required.

(f) Where a bridge or culvert would be necessary to span a designated greenway or wetland to provide a connection to a bike or pedestrian path, the City may limit the number and location of accessways to reduce the impact on the greenway or wetland.

Applicant Response: No accessways are proposed over the wetland.

Staff has noted the wetland located north of the property and has not asked for a future accessway connection to the north. This standard is not applicable.

(g) Accessways shall be constructed, owned and maintained by the property owner.

Applicant Response: No accessways are proposed. Legacy maintains all campus pathways, the internal loop road and campus entrances.

Staff finds that applicant is required to install at least two accessways as described in Section 73.160(1)(a)(iv) and conditioned in AR-5. This requirement has been added as a note to the decision.

(2) Drive-up Uses

The proposal does not include a drive-up facility. This section does not apply.

(3) Safety and Security

(a) Locate windows and provide lighting in a manner which enables tenants, employees and police to watch over pedestrian, parking and loading areas.

Applicant Response: The new lobby reception area is located at the southwest corner of the proposed building. Nearly the entire area is glassed allowing security and reception staff to view the pedestrian pathways and the main parking lots used by patients and visitors coming to the main hospital building. The interior of the lobby will be lit at night to wash light over the entrance and drop off area. Parking lots and pedestrian pathways will be lit to allow approaching motorists and pedestrians to be seen from inside the lobby area. At the upper floors all proposed building facades are lined with patient windows.

Proposed Parking Lot J is an extension of existing parking lots in the northeast corner of the campus which are overlooked by existing buildings. The proposed lot will be lit similar to the existing lots. Due to location, these lots are less used at night than the ones located in front of the proposed hospital entrance.

Staff notes that windows are proposed, as described on Sheets A-22 and A-23 (dated October 17, 2016). Lighting is shown, as described, on Sheet E1.00 (dated August 5, 2016). No changes are proposed to the hospital's existing loading areas on Sheet A-14 (dated October 17, 2016). Staff also notes the terrain challenges on the NE corner of the lot and that Lot J is designed with downward slope proposed away from the Birth Center on Sheets C-2.50 and C-2.6 (dated August 5, 2016), making the far north area of the parking area difficult to see from the building. With the low frequency of night use proposed for this lot, staff finds the standard is met.

(b) In commercial, public and semi-public development and where possible in industrial development, locate windows and provide lighting in a manner which enables surveillance of interior activity from the public right-of-way.

Applicant Response: The proposed building is 356 feet from SW 65th Avenue and 411 feet from Borland Road and at a different grade than these public streets. Once on campus, the interior activity of the proposed lobby area can be observed from the surrounding parking lots and pathways.

Staff finds that the inclusion of windows on all four elevations contributes significantly to site security on Sheets A-22 and A-23 (dated October 17, 2016). This standard is met.

(c) Locate, orient and select on-site lighting to facilitate surveillance of on-site activities from the public right-of-way without shining into public rights-of-way or fish and wildlife habitat areas.

Applicant Response: 12 foot and 20 foot tall pole lighting similar to existing campus pole lighting is specified for general lighting of all proposed exterior areas. Where needed the lamps are shielded to avoid glare and light shining into public rights of way and the vegetated corridor. See Sheet E0.01 for lighting schedule, Sheet E0.02 for cut sheets, and Sheets E1.00 through E1.06 for light fixture locations.

Staff finds that proposed pole lights include backlight shields, as stated on Sheet E0.02 (dated August 5, 2016). The proposed Lighting Plan on Sheet E1.00 (dated August 5, 2016)

shows that public rights-of-way will not be affected. A wetland is located approximately 160 ft from the nearest light source on Lot J. This standard has been met.

(d) Provide an identification system which clearly locates buildings and their entries for patrons and emergency services.

Applicant Response: The proposed canopy sign identifying the new main entry will comply with the recently approved sign standards for building identification for the MC district as described in Chapter 38. The existing canopy entry signs will be removed.

Staff notes that while the proposed elevations illustrate the locations for tenant signs, the locations for building addresses are not shown. All signage permits and approvals are handled separately and not as part of this review. Addressing for emergency purposes will be reviewed through the building permit process. Refer to comments from Tualatin Valley Fire & Rescue in Attachment 103.

(e) Shrubs in parking areas must not exceed 30 inches in height. Tree canopies must not extend below 8 feet measured from grade.

Applicant Response: The landscape plans will specify shrubs with a maximum mature height of 30 inches or less and will select trees whose canopies can be pruned and maintained at 8 foot or more above grade. See Sheets L1 through L7 for plant locations and Sheet L8 for plant list.

Staff notes that numerous tree and shrub types are proposed on Sheet L8 (dated August 5, 2016). Shrubs are proposed that may reach a maximum mature height over 30 inches, for example Gaultheria Shallon (up to 6 ft). No note is provided within the plan set, that states the shrubs and trees will be pruned appropriately. Staff recommends iteration of this standard as a condition of approval.

Condition of Approval: The applicant shall revise the appropriate sheets to note that shrubs in parking areas must not exceed 30 inches in height. Tree canopies must not extend below 8 feet measured from grade and prune appropriately. Additionally the applicant shall include a new notes paragraph titled "Maintenance" that states this code requirement.

(f) Above ground sewer or water pumping stations, pressure reading stations, water reservoirs, electrical substations, and above ground natural gas pumping stations shall provide a minimum 6' tall security fence or wall.

Applicant Response: None of these are proposed.

Staff notes that this standard is not applicable.

(4) Service, Delivery and Screening

(a) On and above grade electrical and mechanical equipment such as transformers, heat pumps and air conditioners shall be screened with sight obscuring fences, walls or landscaping.

Applicant Response: One new transformer is proposed at the northwest corner of the proposed building near the existing transformer. This proposed transformer will be screened with shrubs. Mechanical equipment on the rooftop of the proposed building will be screened with a metal panel screen wall. See Sheets A-22 & 23.

Staff notes that the new transformer is shown on Sheet A-15 (dated October 17, 2016), in addition to existing transformers and mechanical equipment. Proposed landscape screening is shown on sheets L-2 & 3 (dated August 5, 2016). Above grade electrical equipment for the expansion is noted and screened on sheet A-22 (dated August 5, 2016). A new cooling

tower with screen is also proposed on the eastern portion of the existing facility's roof on Sheet A-25 & 31. This standard is met.

(b) Outdoor storage, excluding mixed solid waste and source separated recyclables storage areas listed under TDC 73.227, shall be screened with a sight obscuring fence, wall, berm or dense evergreen landscaping.

Applicant Response: No outdoor storage is proposed.

Staff notes that this standard is not applicable.

(c) Above ground pumping stations, pressure reading stations, water reservoirs; electrical substations, and above ground natural gas pumping stations shall be screened with sight-obscuring fences or walls and landscaping.

Applicant Response: None of these are proposed.

Staff notes that this standard is not applicable.

(5) The Federal Americans with Disabilities Act (ADA) applies to development in the City of Tualatin. Although TDC, Chapter 73 does not include the Oregon Structural Specialty Code's (OSSC) accessibility standards as requirements to be reviewed during the Architectural Review process, compliance with the OSSC is a requirement at the Building Permit step. It is strongly recommended all materials submitted for Architectural Review show compliance with the OSSC.

Applicant Response: Site and building will be designed to meet applicable accessibility requirements of the OSSC and ANSI/ICC A117.1

The applicant has been conditioned to provide curb ramps in AR-6. Accessibility requirements of the OSSC and ANSI/ICC A117.1 will be reviewed during the building permit stage.

(6) (a) All industrial, institutional, retail and office development on a transit street designated in TDC Chapter 11 (Figure 11-5) shall provide either a transit stop pad on-site, or an on-site or public sidewalk connection to a transit stop along the subject property's frontage on the transit street.

Applicant Response: As discussed in 73.160 (1)(a)(i) an existing campus transit stop will be relocated and enlarged.

Staff notes the proposal is located in an institutional development, and Figure 11-5 identifies the on-site transit stop as a "Major Transit Stop". The applicant states that Trimet provided input for the proposed passenger landing pad; however Trimet comments were not included as part of the application submittal. Staff also did not receive comments from Trimet during the public comment period during December 19, 2016. Staff recommends a condition of approval to meet the requirement.

Condition of Approval: The applicant shall supply a letter from Trimet indicating their acceptance of the proposed transit pad design.

H. Structure Design

73.220 Safety and Security

(1) Safety and Security

(a) Locate, orient and select on-site lighting to facilitate surveillance of on-site activities from the public right-of-way or other public areas without shining into public rights-of-way or fish and wildlife habitat areas.

Applicant Response: See narrative under Section 73.160(3)(a) and (b) above.

Staff finds this standard was adequately addressed in 73.160(3)(c).

(b) Provide an identification system which clearly identifies and locates buildings and their entries.

Applicant Response: See narrative under Section 73.160(3)(d) above.

Staff notes that while the proposed elevations illustrate the locations for tenant signs, the locations for building addresses are not shown. All signage permits and approvals are handled separately and not as part of this review. Addressing for emergency purposes will be reviewed through the building permit process. Refer to comments from Tualatin Valley Fire & Rescue in Attachment 103.

(c) Shrubs in parking areas shall not exceed 30 inches in height, and tree canopies must not extend below 8 feet measured from grade, except for parking structures and underground parking where this provision shall not apply.

Applicant Response: See narrative under Section 73.160(3)(e) above.

The applicant has been conditioned in AR-7 to revise the appropriate sheets to note that shrubs in parking areas must not exceed 30 inches in height, and tree canopies must not extend below 8 feet measured from grade to meet the standard of Section 73.160(3)(e) and 73.220(1)(c).

I. Mixed Solid Waste and Source Separated Recyclables Storage Areas

73.227 Standards

The following standards are minimum requirements for mixed solid waste and source separated recyclables storage areas. To provide for flexibility in designing functional storage areas, this section provides four different methods to meet the objectives of providing adequate storage for mixed solid waste and source separated recyclables and improving the efficiency of collection. An applicant shall choose and implement one of the following four methods to demonstrate compliance: 1) minimum standards; 2) waste assessment; 3) comprehensive recycling plan; or 4) franchised hauler review, as more fully described in subsections (2), (3), (4) and (5) of this section.

The applicant has chosen (4) Comprehensive Recycling Plan Method.

(4) Comprehensive Recycling Plan Method. The comprehensive recycling plan method is most appropriate when an applicant has independently developed a comprehensive recycling plan which addresses mixed solid waste and source separated recyclable collection and storage for the proposed use. This method can be used when a comprehensive recycling plan has been developed for a specific development. It is most suited to uses such as hospitals, schools and industrial developments. The comprehensive recycling plan shall be submitted at the time plans are submitted for Architectural Review. The applicant shall submit plans and text that show how mixed solid waste and source separated recyclables generated by the proposed development will be served under a comprehensive recycling plan.

The application shall also demonstrate that the mixed solid waste and source separated recyclables volumes expected to be generated can be stored in less space than is required by the Minimum Standards Method. If the application does not demonstrate that the comprehensive recycling plan method requires less space, through the Architectural Review process the minimum standards method may be required. The comprehensive recycling plan method shall be reviewed and approved as part of the Architectural Review process.

Applicant Response: There are two separate housekeeping systems on the campus currently. This strategy will remain in effect when the patient expansion is complete.

Medical Office Buildings (MOB's):

The MOB's have independent contractors that provide housekeeping services. These services dispose of their recycling and waste in trash enclosures located near the MOB's. This project will be removing the current trash enclosure for MOB 1 and 2. The project will provide a new enclosure located in the northwest corner of parking lot A. The new trash enclosure contains 585 sf of storage which exceeds the 572 square feet required to service the total building area of 143,091.

Hospital:

The hospital has its own housekeeping department. The hospital currently has a comprehensive recycling and waste strategy. The hospital separates their waste into four categories. These wastes are stored in localized holding rooms throughout the campus see attached diagram showing size and location of both existing and proposed holding areas.

The types of source separated waste are:

Waste:

Waste is taken to a 25 yard waste compactor located at the north end of the existing hospital building west of Pod C (Sheet A14). An outside contractor, Republic Systems, manages the compactor and the waste is removed twice a week. The process for moving waste to this location is by using a series of soiled holding rooms to consolidate the material or by housekeeping staff taking the product directly to the compactor.

Medical Waste:

Medical waste is taken to a holding spot located at the north end of the existing hospital building inside the loading dock east of Pod D (Sheet A14). Republic Systems, Inc. removes the waste three times a week. The process for moving medical waste to this location is by using a series of soiled holding rooms to consolidate the material or by housekeeping staff taking the product directly to the designated storage location.

[Cardboard] Recycling:

Cardboard is taken to a 30 yard compactor located on the east side of the existing hospital next to the loading dock near the physical plant (Sheet A14). Republic Services, Inc. removes the cardboard three times a week. The process for moving waste to this location is by a using series of soiled holding rooms to consolidate the material or by housekeeping staff taking the product directly to the compactor.

[General] Recycling:

General recycling is taken to a holding spot located at the north end of the existing hospital building inside the loading dock west of Pod D. Republic Services, Inc. removes the recycling three times a week. The process for moving the recycling to this location is by using a series of soiled holding rooms to consolidate the material or by housekeeping staff taking the product directly to the designated storage location.

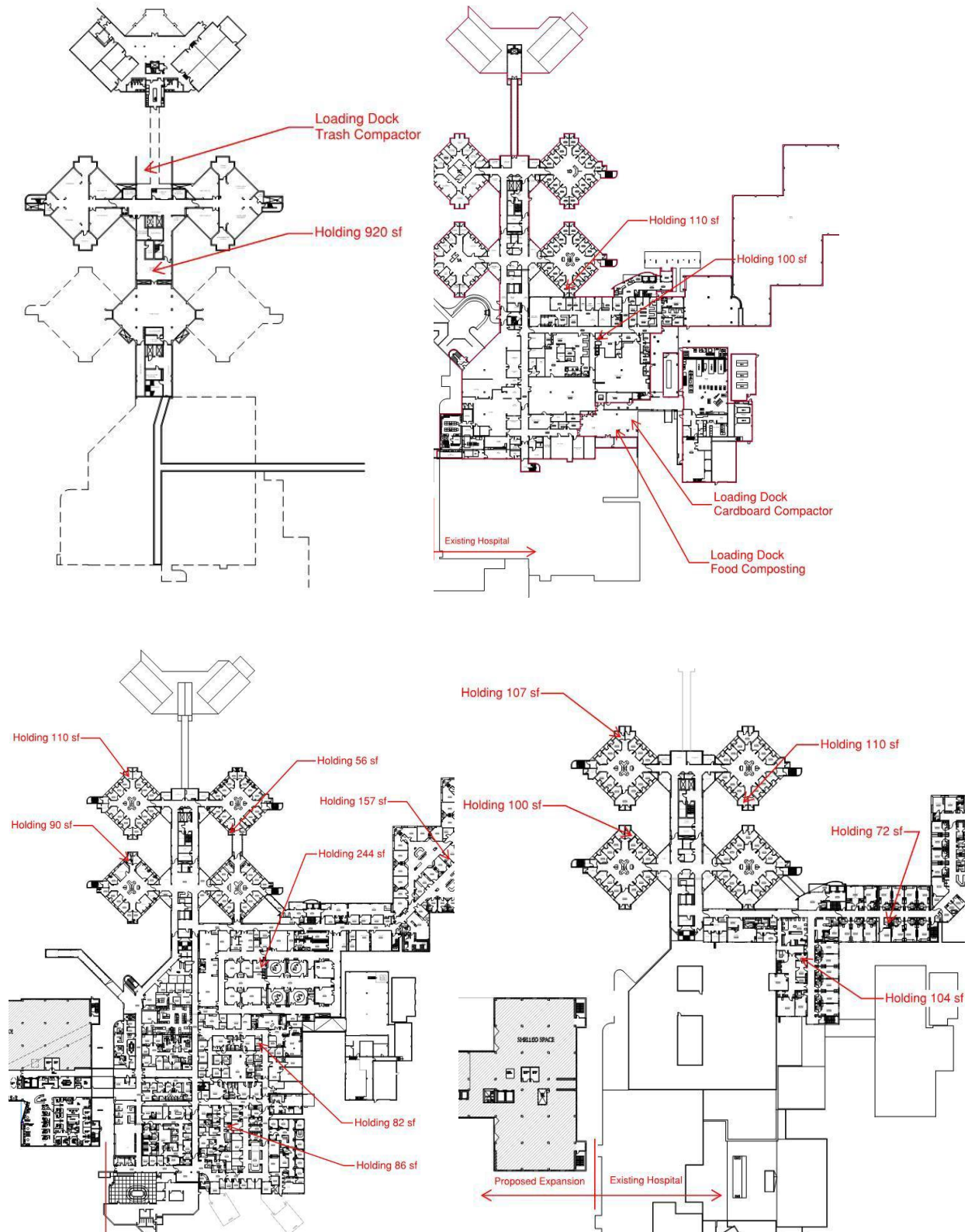
Composting:

Waste Food is taken directly to a compost bin located at the loading dock near the physical plant. Republic Services, Inc. removes the composting 2 times a week.

The total square footage of the existing hospital and proposed expansion is 493,406 square feet. Adding up the existing and proposed holding rooms provides 3,381 square feet of waste storage which exceeds the 1,983 square feet required by the minimum standard method.

Republic Services, Inc. has confirmed that they will continue to provide garbage and recycling services to the campus including the new proposed building. Increased volumes were discussed with the franchised waste hauler and it was determined that the current facilities would be adequate for both the existing and new expansion. Additional pick-ups

could be scheduled if the need arises. In addition, Republic Services, Inc reviewed the plans for the proposed trash enclosure for the MOB and confirmed that its size will accommodate the needed containers. See letter from Republic Services, Inc.



Staff has calculated trash enclosure requirements per the minimum requirement standards for both the MOB and expansion below. The MOB exceeds the minimum standards. The expansion proposes 468 sq ft of soiled utility room storage provided on the fourth and fifth floor (Sheets A-29 & 30, dated August 5, 2016). While the minimum required for the expansion is 663 sq ft, the hospital in its entirety after the expansion provides an additional 932 sq ft of trash enclosure space over the minimum requirement. Additionally the majority of the second and third floor of the expansion is shelled space and that the entirety of the basement is dedicated to utility purposes. It is anticipated that more soiled utility room storage will be provided as the shelled space is developed. This standard is met.

Table 4. Trash Enclosure Requirements				
<i>Building</i>	<i>Area (sf)</i>	<i>Applied Rate (sf)</i>	<i>Min. Required (ft)</i>	<i>Proposed (sf)</i>
<i>MOB</i>	143,091	$10+([143,091 / 1,000]*4)$	582.4	585
<i>Expansion Only</i>	163,203	$10+([163,203 / 1,000]*4)$	662.8	468
<i>Entire Hospital</i>	493,406	$10+([493,406 / 1,000]*4)$	1,983.6	2,916

(6) Location, Design and Access Standards for Storage Areas.

(a) Location Standards

(i) To encourage its use, the storage area for source separated recyclables may be co-located with the storage area for mixed solid waste.

Recyclables will be collocated with the MOB storage area. The overall hospital has a comprehensive source separated plan as described in 73.227(4). This standard has been met.

(ii) Indoor and outdoor storage areas shall comply with Building and Fire Code requirements.

Staff notes that Building and Fire Codes are administrated by others.

(iii) Storage area space requirements can be satisfied with a single location or multiple locations, and can combine both interior and exterior locations.

A singular enclosure to serve Medical Office Buildings 1 & 2 is proposed at the northwest corner of reconfigured Lot A (Sheet A14). The overall hospital has a comprehensive source separated plan as described in 73.227(4) that includes multiple interior locations. This standard has been met.

(iv) Exterior storage areas shall not be located within a required front yard setback or in a yard adjacent to a public or private street.

Staff notes that the exterior storage area proposed for the Medical Office Buildings 1 & 2 is located at the northwest corner of reconfigured Lot A (Sheet A14, dated October 17, 2016) and not within a required front yard setback or yard adjacent to a public or private street. The overall hospital storage areas are located in interior locations. This standard is met.

(v) Exterior storage areas shall be located in central and visible locations on the site to enhance security for users.

Staff notes that the exterior storage area proposed for the Medical Office Buildings 1 & 2 is located at the northwest corner of reconfigured Lot A (Sheet A14, dated October 17, 2016), making it visible from the main parking area. This standard is met.

(vi) Exterior storage areas can be located in a parking area, if the proposed use provides parking spaces required through the Architectural Review process. Storage areas shall be appropriately screened according to TDC 73.227(6)(b)(iii).

Applicant Response: One new trash and recycling enclosure to serve Medical Office Buildings 1 & 2 is proposed at the northwest corner of reconfigured Lot A (Sheet A14, dated October 17, 2016). This replaces the existing enclosure that is dislocated by the proposed building.

The enclosure will be fully screened on all sides with a brick wall that matches the proposed building brick. The enclosure is accessed through a metal panel door. See Sheet A 25 (dated October 17, 2016).

Staff notes that adequate parking has been provided in TDC 73.370. Enclosure screening is discussed in 73.227(6)(b)(iii).

(vii) Storage areas shall be accessible for collection vehicles and located so that the storage area will not obstruct pedestrian or vehicle traffic movement on site or on public streets adjacent to the site.

Applicant Response: Republic Services, Inc has confirmed that the size, design, and location of this new proposed enclosure can be serviced. See Republic Services, Inc. letter.

Staff notes that pedestrian or vehicle movement on site will not be obstructed by the storage area placement as shown on Sheet A-14 (dated October 17, 2016). This standard is met.

(b) Design Standards

(i) The dimensions of the storage area shall accommodate containers consistent with current methods of local collection at the time of Architectural Review approval.

Applicant Response: Republic Services, Inc has confirmed that the size, design, and location of this new proposed enclosure can be serviced. See Republic Services, Inc. letter.

(ii) Storage containers shall meet Fire Code standards and be made and covered with water proof materials or situated in a covered area.

Staff notes that trash and recyclable storage containers would be provided by Republic Services, and that the fire codes are administrated by others.

(iii) Exterior storage areas shall be enclosed by a sight obscuring fence or wall at least 6 feet in height. In multi-family, commercial, public and semi-public developments evergreen plants shall be placed around the enclosure walls, excluding the gate or entrance openings. Gate openings for haulers shall be a minimum of 10 feet wide and shall be capable of being secured in a closed and open position. A separate pedestrian access shall also be provided in multi-family, commercial, public and semi-public developments.

Applicant Response: The enclosure will be fully screened on all sides with a brick wall that matches the proposed building brick. The enclosure is accessed through a metal panel door. See Sheet A 25.

Staff notes that that the brick wall is 6ft 8 in to meet the requirement. Evergreen Huckleberry and Japanese Sky Pencil Holly are proposed evergreen plans located west, north, and east of the enclosure on Sheet L-2 (dated August 5, 2016). The proposed gate opening is 18 ft wide and a 4 ft wide pedestrian opening is provided on the eastern elevation as shown on Sheet A-25 (dated October 17, 2016). This standard is met.

(iv) Exterior storage areas shall have either a concrete or asphalt floor surface.

Staff notes that the applicant did not provide a finding to this requirement. There is no note on Sheet A-15 or A-25 (dated October 17, 2016) regarding the proposed floor surface. Staff recommends iteration of this standard as a condition of approval.

Condition of Approval: *The applicant shall revise the appropriate sheets to illustrate that the exterior trash enclosure area shall have either a concrete or asphalt floor surface and install to revised plans.*

(v) Storage areas and containers shall be clearly labeled to indicate the type of material accepted.

Staff notes that clearly labeled trash and recyclable storage containers would be provided by Republic Services, the trash hauler for the proposed development. This standard is met.

(c) Access Standards

(i) Access to storage areas can be limited for security reasons. However, the storage areas shall be accessible to users at convenient times of the day, and to hauler personnel on the day and approximate time they are scheduled to provide hauler service.

Applicant Response: Republic Services, Inc has confirmed that the size, design, and location of this new proposed enclosure can be serviced. See Republic Services, Inc. letter.

Staff notes that gates can be latched when closed, but storage areas would be accessible to haulers and pedestrians. This standard is met.

(ii) Storage areas shall be designed to be easily accessible to hauler trucks and equipment, considering paving, grade, gate clearance and vehicle access. A minimum of 10 feet horizontal clearance and 8 feet vertical clearance is required if the storage area is covered.

Staff notes that the proposed gate opening is 18 ft wide as shown on Sheet A-25 (dated October 17, 2016). As the proposed trash enclosures would not be covered, the minimum horizontal and vertical clearances do not apply.

(iii) Storage areas shall be accessible to collection vehicles without requiring backing out of a driveway onto a public street. If only a single access point is available to the storage area, adequate turning radius shall be provided to allow vehicles to safely exit the site in a forward motion.

Staff notes that no use of the public street would be required for access the proposed trash enclosure and adequate circulation is provided with the 24 ft parking lot aisle. This standard is met.

J. Landscaping

73.240 Landscaping General Provisions

(1) The following standards are minimum requirements.

(4) The minimum area requirement for landscaping for uses in IN, CN, CO/MR, MC and MP Planning Districts shall be twenty-five (25) percent of the total land area to be developed. When a dedication is granted in accordance with the planning district provisions on the subject property for a fish and wildlife habitat area, the minimum area requirement for landscaping may be reduced by 2.5 percent from the minimum area requirement as determined through the AR process.

Applicant Response: The proposed site development area in the Northeast corner of the campus, which includes new parking area totals 234,088 square feet of which 84,728 square feet or 36% is landscaped area. The proposed site development area in the Southwest corner of the campus, which includes the proposed new building and parking, totals 352,558 square feet of which 92,954 square feet or 26% of landscaped area. The additional landscaping to upgrade the Vegetated Corridor to good condition is in addition to this proposed landscaped area. No dedications are proposed and the site is not within the Hedges Creek Wetland Protection District.

Staff found landscaping information on Sheet A-14 (dated October 17, 2016) and L-1 (dated August 5, 2016). Staff used areas from Sheet A-14 and from the Fact Sheet (submitted with the application) to populate Table 5 below, which meets the standards. Information on Sheet L-1 captures square footage of landscaping in parking areas only and does not correspond to the total captured on Sheet A-14. Staff recommends a condition of approval so that both sheets contain corresponding landscape totals.

Table 5. Landscape Coverage			
<i>Landscape Attribute</i>	<i>NE Development</i>	<i>SW Development</i>	<i>Site Total</i>
Site Development (sf)	234,088	352,558	586,646
Landscape Area (sf)	84,728	92,954	177,682
Landscape Area (%)	36.2	26.4	30.3

Condition of Approval: *The applicant shall revise Landscape Plan, Sheet L-1 (dated August 5, 2016) to reflect the total landscape area in either a table or with statistics that correspond to the landscape total presented on Proposed Site Plan, Sheet A-14 (dated October 17, 2016). Total landscape area shall meet the minimum requirements of 73.240(1)(4).*

(8) Developments not in a Low Density Residential (RL) or Manufacturing Park (MP) Planning District, but which abut an RL or MP Planning District shall provide and perpetually maintain dense, evergreen landscaped buffers between allowed uses in the district and the adjacent Low Density Residential (RL) or Manufacturing Park (MP) Planning District as approved through the Architectural Review process.

Applicant Response: There is a delineated wetlands and vegetated corridor along the north border of between the campus and residential zoned land to the north. Legacy owns a 20 acre residential zone undeveloped parcel that lies between the developed portion of the campus and the residential subdivision to the east.

Staff notes that Incense Cedar and Douglas Fir trees are proposed along the edge of the northeast development area on Sheets L-6 and L-7 (dated August 5, 2016) to screen the MC district from the eastern RL district. Ponderosa Pine and Douglas Fir trees are also proposed to enhance the vegetated corridor to the north. This standard has been met.

(9) Yards adjacent to public streets, except as described in the Hedges Creek Wetlands Mitigation Agreement, TDC 73.240(7), shall be planted to lawn or live groundcover and trees and shrubs and be perpetually maintained in a manner providing a park-like character to the property as approved through the Architectural Review process.

Applicant Response: The areas of the campus adjacent to NW 65th Avenue and Borland Road outside of the proposed development areas will remain in landscaping as previously approved. The area between the reconfigured Parking Lot B will be landscaped according to the standards for parking lot perimeter landscape as discussed in Section 73.360 below.

Staff notes that Lot B of the SW development area is adjacent to both SW 65th Avenue and SW Borland Road. Sheet L-4 (dated August 5, 2016) illustrates that landscape improvements that include trees, shrubs, and groundcover are proposed for both frontages. This standard has been met.

(10) Yards not adjacent to public streets or Low Density Residential (RL) or Manufacturing Park (MP) Planning Districts shall be planted with trees, shrubs, grass or other live groundcover, and maintained consistent with a landscape plan indicating areas of future expansion, as approved through the Architectural Review process.

Applicant Response: The north, south and west sides of the proposed building incorporate trees, shrubs, and groundcover into therapeutic, four season garden with pedestrian seating areas and pathways. See narrative responding to 73.310 for further description of this area. Proposed parking lot perimeters and interiors are landscaped with trees, shrubs and

groundcover as discussed in Section 73.360 below. See Sheets L1 through L7 for landscape plans

Staff finds that the landscape improvements are proposed throughout the development area, including yards not adjacent to the public streets or RL district as shown on L-1 to L-8 (dated August 5, 2016). This standard is met.

(11) Any required landscaped area shall be designed, constructed, installed, and maintained so that within three years the ground shall be covered by living grass or other plant materials. (The foliage crown of trees shall not be used to meet this requirement.) A maximum of 10% of the landscaped area may be covered with un-vegetated areas of bark chips, rock or stone. Disturbed soils are encouraged to be amended to an original or higher level of porosity to regain infiltration and stormwater storage capacity.

Staff finds that applicant's narrative is silent on this requirement. The landscape plan (Sheets L-1 to L-8, dated August 5, 2016) does not illustrate the use of any bark chips, rock or stone. This standard is met.

(12) In the MP District, wetland buffer areas up to 50 feet in width may be counted toward the required percentage of site landscaping, subject to the following:

The proposal is not located in the MP District. This standard does not apply.

(13) Landscape plans for required landscaped areas that include fences should carefully integrate any fencing into the plan to guide wild animals toward animal crossings under, over, or around transportation corridors.

No new fences are proposed for the project. This standard does not apply.

73.260 Tree and Plant Specifications

(1) The following specifications are minimum standards for trees and plants:

(a) Deciduous Trees. Deciduous shade and ornamental trees shall be a minimum one and one-half inch (1-1/2") caliper measured six inches (6") above ground, balled and burlapped. Bare root trees will be acceptable to plant during their dormant season. Trees shall be characteristically shaped specimens.

(b) Coniferous Trees. Coniferous trees shall be a minimum five feet (5') in height above ground, balled and burlapped. Bare root trees will be acceptable to plant during their dormant season. Trees shall be well branched and characteristically shaped specimens.

(c) Evergreen and Deciduous Shrubs. Evergreen and deciduous shrubs shall be at least one (1) to five (5) gallon size. Shrubs shall be characteristically branched. Side of shrub with best foliage shall be oriented to public view.

(d) Groundcovers. Groundcovers shall be fully rooted and shall be well branched or leafed. English ivy (*Hedera helix*) is considered a high maintenance material which is detrimental to other landscape materials and buildings and is therefore prohibited.

(e) Lawns. Lawns shall consist of grasses, including sod, or seeds of acceptable mix within the local landscape industry. Lawns shall be 100 percent coverage and weed free.

Applicant Response: Quatrefoil, Inc. the project landscape architect has specified plant materials that meet these standards. See Sheet L8 for specified plant sizes.

Staff notes that not all deciduous or coniferous plant materials specified on L8 (dated August 5, 2016) meet the specified standards. In particular Katsura Trees at 1.25 ft cal and Plume Cedars at 4.5 ft are proposed. Staff recommends iteration of this standard as a condition of approval.

Condition of Approval: *The applicant shall revise the appropriate sheets to illustrate that Deciduous trees shall be a minimum one and one-half inch (1 1/2") caliper measured six inches (6") above ground, balled and burlapped. Coniferous trees shall be a minimum five feet (5') in height above ground, balled and burlapped. Trees shall be installed to revised plans.*

(2) Landscaping shall be installed in accordance with the provisions of Sunset New Western Garden Book (latest edition), Lane Publishing Company, Menlo Park, California or the American Nurserymen Association Standards (latest edition).

Staff notes that landscaping has been designed by a professional landscape architect and will conform to ANSI standards in Planting Notes #12 on Sheet L-8 (dated August 5, 2016). This standard has been met.

(3) The following guidelines are suggested to ensure the longevity and continued vigor of plant materials:

(a) Select and site permanent landscape materials in such a manner as to produce a hardy and drought-resistant landscaped area.

(b) Consider soil type and depth, spacing, exposure to sun and wind, slope and contours of the site, building walls and overhangs, and compatibility with existing native vegetation preserved on the site or in the vicinity.

Staff notes that Planting Notes are listed on Sheet L-8 (dated August 5, 2016). This standard has been met.

(4) All trees and plant materials shall be healthy, disease-free, damage-free, well-branched stock, characteristic of the species.

All plant materials would be new and healthy. Staff notes that Planting Notes are listed on Sheet L-8 (dated August 5, 2016). This standard is met.

(5) All plant growth in landscaped areas of developments shall be controlled by pruning, trimming or otherwise so that:

(a) It will not interfere with designated pedestrian or vehicular access; and

(b) It will not constitute a traffic hazard because of reduced visibility.

The selected plant materials are appropriate for the proposed development and climate and would not interfere with visibility or movement. The applicant has been conditioned to maintain clear vision areas in AR-7 pursuant to 73.160(3)(e) and 73.220(1)(c).

73.280 Irrigation System Required

Except for townhouse lots, landscaped areas shall be irrigated with an automatic underground or drip irrigation system.

Applicant Response: All new landscaped areas will be irrigated with an automated system.

Staff notes that an Irrigation Note has been included on the landscape sheets submitted and dated August 5, 2016. This standard is met.

73.290 Re-vegetation in Un-landscaped Areas

The purpose of this section is to ensure erosion protection, and in appropriate areas to encourage soil amendment, for those areas not included within the landscape percentage requirements so native plants will be established, and trees will not be lost.

(1) Where vegetation has been removed or damaged in areas not affected by the landscaping requirements and that are not to be occupied by structures or other improvements, vegetation shall be replanted.

Applicant Response: Unlandscaped areas where vegetation has been removed or damaged will be replanted. New trees will be added to the vegetated corridor at the northern boundary of the campus to bring this area into good condition. See Sheet C-7.60 for tree planting.

Staff finds that a NRPO-OSNA (Natural Resource Protection Overlay–Open Space Natural Area) is located along the northern end of the site. A technical memorandum submitted by SWCA on June 22, 2016 recommends enhancing the vegetated corridor to good condition as proposed by the applicant on Sheet L-7 (dated August 5, 2016). Clean Water Services has also provided comments in a letter dated December 30, 2016 (Attachment 104) which contained a condition relating to the “Sensitive Area” in bullet (i). This condition has been included in the corresponding Public Utility Report.

(2) Plant materials shall be watered at intervals sufficient to ensure survival and growth for a minimum of two growing seasons.

Trees proposed within the vegetated corridor are native and located near a wetland.

(3) The use of native plant materials is encouraged to reduce irrigation and maintenance demands.

Trees proposed within the vegetated corridor are native and located near a wetland.

(4) Disturbed soils should be amended to an original or higher level of porosity to regain infiltration and stormwater storage capacity.

Staff notes that soil amendment is addressed in Planting Notes #8 on Sheet L-8 (dated August 5, 2016). This standard has been met.

73.310 Landscape Standards – Commercial, Industrial, Public and Semi-Public Uses

(1) A minimum 5'-wide landscaped area must be located along all building perimeters which are viewable by the general public from parking lots or the public right-of-way, excluding loading areas, bicycle parking areas and pedestrian egress/ingress locations. Pedestrian amenities such as landscaped plazas and arcades may be substituted for this requirement. This requirement shall not apply where the distance along a wall between two vehicle or pedestrian access openings (such as entry doors, garage doors, carports and pedestrian corridors) is less than 8 feet.

Applicant Response: A pedestrian area surrounds the entire proposed building perimeter with the exception of the main entrance. This area contains pedestrian seating areas and garden areas connected by curved pathways as mentioned in 73.160.(1)(a)(ii) above. The planting beds, with a minimum width of 5 feet, feature healing landscapes using the American Horticultural Therapy Association therapeutic garden characteristics which will compliment the existing Therapy Garden located at the Hospital. The new plantings are designed to create a four season garden with a variety of evergreen and deciduous plantings both native and ornamental. Storm water planters are incorporated into the new landscape areas. New trees are located throughout the landscape areas to provide shade for the new pedestrian areas. See Sheet L3 for landscape plan of this area.

Staff notes that this requirement has been met on Sheet L3 (dated August 5, 2016).

(2) Areas exclusively for pedestrian use that are developed with pavers, bricks, etc., and contain pedestrian amenities, such as benches, tables with umbrellas, children's play areas, shade trees, canopies, etc., may be included as part of the site landscape area requirement.

Applicant Response: The pedestrian area that surrounds the building perimeter is included in the landscape area calculation for the development area in the SW corner of the campus.

Staff notes that Sheet L3 (dated August 5, 2016) does not clearly illustrate pedestrian amenities but has been designed as a therapeutic garden.

(3) All areas not occupied by buildings, parking spaces, driveways, drive aisles, pedestrian areas or undisturbed natural areas shall be landscaped.

Applicant Response: All the required areas are landscaped. See Sheet L1 (dated August 5, 2016) for the overall landscape plan.

Staff notes that the requirement has been met in the area of work.

73.320 Off-Street Parking Lot Landscaping Standards

(1) General Provisions. In addition to the goals stated in TDC 73.110 and 73.140, the goals of the off-street parking lot standards are to create shaded areas in parking lots, to reduce glare and heat buildup, provide visual relief within paved parking areas, emphasize circulation patterns, reduce the total number of spaces, reduce the impervious surface area and stormwater runoff and enhance the visual environment. The design of the off-street parking area shall be the responsibility of the developer and should consider visibility of signage, traffic circulation, comfortable pedestrian access, and aesthetics. Trees shall not be cited as a reason for applying for or granting a variance on placement of signs.

(2) Application. Off-street parking lot landscaping standards shall apply to any surface vehicle parking or circulation area.

Applicant Response: The perimeter of the new parking areas will be landscaped to meet the screening requirements of the code. The landscape area will vary in width to provide a park like setting for the campus. Native plantings are the primary plantings used throughout the parking area supplemented with drought tolerant ornamental plantings including shrub roses, black eyed susan, asters, and sedums. Shading and landscaping have been designed to enhance the visual environment.

Staff notes that Lot A, B, C, J, K, and L are located in the development area and are subject to the parking lot landscaping standards as shown on L-1 (dated August 5, 2016).

73.340 Off-Street Parking Lot and Loading Area Landscaping - Commercial, Industrial, Public and Semi-Public Uses, and Residential and Mixed Use Residential Uses within the Central Design District

(1) A clear zone shall be provided for the driver at ends of on-site drive aisles and at driveway entrances, vertically between a maximum of 30 inches and a minimum of 8 feet as measured from the ground level, except for parking structures and underground parking where this provision shall not apply.

Applicant Response: Clear visual zones are provided at drive aisle ends and driveway entrances.

The applicant has been conditioned to meet this standard in AR-7 pursuant to 73.160(3)(e) and 73.220(1)(c).

(2) Perimeter site landscaping of at least 5 feet in width shall be provided in all off-street parking and vehicular circulation areas (including loading areas). For conditional uses in multifamily residential planning districts the landscape width shall be at least 10 feet except for uses allowed by TDC 40.030(3), 40.030(5)(j), 40.030(5)(m), 40.030(5)(n) and 41.030(2).

(a) The landscape area shall contain:

(i) Deciduous trees an average of not more than 30 feet on center. The trees shall meet the requirements of TDC 73.360(7).

(ii) Plantings which reach a mature height of 30 inches in three years which provide screening of vehicular headlights year round.

(iii) Shrubs or ground cover, planted so as to achieve 90 percent coverage within three years.

(iv) Native trees and shrubs are encouraged.

Applicant Response: Perimeter landscape areas not less than 5 feet wide are provided. Deciduous trees are not more than 30 feet apart. Shrubs and ground cover will achieve 90 percent coverage within three years. Native trees and shrubs are used where appropriate.

Staff notes that not all deciduous trees are placed not more than 30 ft on center on Sheets L-2 to L-8 (dated August 5, 2016) but meet the requirement when spacing is considered on average. This requirement has been met.

(b) Where off-street parking areas on separate lots are adjacent to one another and are connected by vehicular access, the landscaped strips required in subsection (2) of this section are not required.

Staff finds that this standard does not apply.

73.360 Off-Street Parking Lot Landscape Islands - Commercial, Industrial, Public, and Semi-Public Uses

(1) A minimum of 25 square feet per parking stall be improved with landscape island areas. They may be lower than the surrounding parking surface to allow them to receive stormwater run-off and function as water quality facilities as well as parking lot landscaping. They shall be protected from vehicles by curbs, but the curbs may have spaces to allow drainage into the islands. They shall be dispersed throughout the parking area [see TDC 73.380(3)]. They shall be planted with groundcover or shrubs that will completely cover the island area within 3 years. They shall be planted with deciduous shade trees when needed to meet the parking lot shade tree requirements. Native plant materials are encouraged. Landscape square footage requirements shall not apply to parking structures and underground parking.

Applicant Response: The proposed new and reconfigured parking lots, A, B, C, J, K and L exceed the minimum required landscaped island areas. See Table below. All proposed island areas are protected by continuous extruded concrete curbs. Plant materials specified are of a size that will cover the entire island area. The number of shrubs, trees and groundcover comply with the landscape standards for interior parking lot landscaping.

Parking Lot	Stalls	Area Required (s.f.)	Area Provided (s.f.)	Difference
A	171	4,275	4,550	275
B	177	4,425	4,713	288
C	68	1,700	3,645	1,945
J	143	3,575	6,385	2,810
K	172	4,300	9,887	5,587
L	50	1,250	2,861	1,611

(2) Landscaped island areas with deciduous parking lot shade trees shall be a minimum of 5 feet in width (from inside of curb to curb).

Applicant Response: Islands with deciduous parking lot trees exceed 5 feet in width.

Staff finds that landscaped islands are sized between 7 to 8 ft on Sheets A-15 to 21 (dated October 17, 2016).

(3) A minimum of one deciduous shade tree shall be provided for every four (4) parking spaces to lessen the adverse impacts of glare, reduce heat from paved surfaces, and to emphasize circulation patterns. Required shade trees shall be uniformly distributed

throughout the parking lot (see TDC 73.380(3)), except that within the Central Design District landscape islands and shade trees may be placed to frame views of the Tualatin Commons water feature or identified architectural focal elements. The trees shall meet the requirements of TDC 73.360(7). Parking lot shade tree requirements shall not apply to parking structures and underground parking.

Applicant Response: Deciduous shade trees are disbursed throughout each proposed and reconfigured lot and exceed the minimum as noted in the below table.

Parking Lot	Stalls	Required Trees	Trees Provided	Exceeds
A	171	43	45	2
B	177	45	45	0
C	68	17	26	9
J	143	36	45	9
K	172	43	60	17
L	50	13	16	3

Staff finds the table was provided in the applicant narrative on p. 60 and on Sheet L-1. The applicant also provided plant material listing tables on sheets L-2 to L-8 (dated August 5, 2016) that include sufficient quantities of deciduous shade trees.

(4) Landscape islands shall be utilized at aisle ends to protect parked vehicles from moving vehicles and emphasize vehicular circulation patterns. Landscape island location requirements shall not apply to parking structures and underground parking.

Applicant Response: Landscape islands are located at each aisle end.

Staff finds that this requirement is met on Sheet A-14, dated October 17, 2016.

(5) Required plant material in landscape islands shall achieve 90 percent coverage within three years. Native shrubs and trees are encouraged.

Applicant Response: As noted in 73.160(1) proposed plant materials will achieve full coverage within three years. Where possible, native shrubs and trees are proposed. See plant list on Sheet L8.

Staff notes that this requirement was discussed in 73.340(2)(a), not 73.160(1).

(6) (a) Except as in (b) below, site access from the public street shall be defined with a landscape area not less than 5 feet in width on each side and extend 25 feet back from the property line for commercial, public, and semi-public development with 12 or more parking spaces and extend 30 feet back from the property line for industrial development, except for parking structures and underground parking which shall be determined through the Architectural Review process.

Applicant Response: The four accesses from public streets are outside the area of this proposed development area. Existing landscape as previously approved will remain.

Staff notes that Robert Hixson of Clackamas County Development Engineering provided comments in response to the development proposal on December 21, 2016, Attachment 105. The County recommends improvements to the two driveway throats located off of Borland under bullet 7 and 8 on p. 2. A condition of approval is also recommend as 5.e. on p.4 which states, "Two minimum 28-foot wide (throat width) driveway approaches in conformance with Roadway Standards Drawing D650 at the existing Borland Road approach locations. Minimum throat lengths, measured from the backs of the new separated sidewalks, shall be 50 ft." Clackamas County also recommends half street improvements to Borland Road. These improvements may impact TDC standards, including but not limited to: minimum parking requirements, landscape requirements, and tree removal review and approval [TDC 34.210, 56.050(3), 73.340, 73.360, 73.370, 73.380, 73.400]. Staff recommends a condition of approval to ensure that future revisions to the site plan in

response to Borland Road improvements conform to the Tualatin Community Development standards.

Condition of Approval: Any plan set revisions in response to Clackamas County Development Engineering review of Borland Road improvements shall comply with all applicable policies and regulations of the Tualatin Development Code (TDC) including, but not limited to: 34.210, 56.050(3), 73.340, 73.360, 73.370, 73.380, 73.400.

The applicant shall submit revised site plans showing compliance with the Tualatin Development Code and shall construct and install improvements per the revised plan set approved by the Planning Division.

(b) In the Central Design District where driveway access is on local streets, not collectors or arterials, and the building(s) on the property is(are) less than 5,000 square feet in gross floor area, or parking is the only use on the property, site access from the public street shall be defined with a landscape area not less than 5 feet in width on each side and extend 5 feet back from the property line, except for parking structures and underground parking which shall be determined through the Architectural Review process.

Staff finds that this standard does not apply. The site is not within the Central Design District.

(7) Deciduous shade trees shall meet the following criteria:

- (a) Reach a mature height of 30 feet or more;**
- (b) Cast moderate to dense shade in summer;**
- (c) Long lived, i.e., over 60 years;**
- (d) Do well in an urban environment:**
 - (i) Pollution tolerant.**
 - (ii) Tolerant of direct and reflected heat.**
- (e) Require little maintenance:**
 - (i) Mechanically strong.**
 - (ii) Insect- and disease-resistant.**
 - (iii) Require little pruning.**
- (f) Be resistant to drought conditions;**
- (g) Be barren of fruit production.**

Applicant Response: Proposed trees include a variety of maples, dogwoods, and common orientals. All proposed trees meet the criteria (a) through (g) of this section. See plant list on Sheet L8.

Staff finds the criteria is met as described on Sheet L8, dated August 5, 2016.

K. Tree Preservation

34.230 Tree Removal Criteria

The Community Development Director shall consider the following criteria when approving, approving with conditions, or denying a request to cut trees.

(1) An applicant must satisfactorily demonstrate that any of the following criteria are met:

- (a) The tree is diseased, and**
 - (i) The disease threatens the structural integrity of the tree; or**

- (ii) The disease permanently and severely diminishes the esthetic value of the tree; or
 - (iii) The continued retention of the tree could result in other trees being infected with a disease that threatens either their structural integrity or esthetic value.
- (b) The tree represents a hazard which may include but not be limited to:
- (i) The tree is in danger of falling;
 - (ii) Substantial portions of the tree are in danger of falling.
- (c) It is necessary to remove the tree to construct proposed improvements based on Architectural Review approval, building permit, or approval of a Subdivision or Partition Review.
- (2) If none of the conditions in TDC 34.240(1) are met, the Community Development Director shall evaluate the condition of each tree based on the following criteria. A tree given a rating of one on a factor will not be required to be retained.

Applicant Response: A tree preservation plan (C-7.00 – C-7.70) has been submitted that identifies the locations of all trees on site eight inches or more in diameter. The SCWA required easement boundary has been identified on the tree plan. Trees proposed for removal have also been identified. A tree assessment has been prepared and provided with this application. The trees that are being proposed for removal as a part of this application are being removed to accommodate the construction of the proposed improvements. All tree removal is detailed in the included Arborist's report, as well as C-7.10 through C-7.70. All proposed tree removal is necessary to construct the proposed improvements.

A Tree Assessment Report was provided by Morgan Holan and Associates and is dated July 29, 2016. This report inventoried 396 trees within the project boundaries with 246 (62%) planned for removal and 150 (38%) planned for retention. Staff finds this requirement has been met.

73.250 Tree Preservation

- (1) Trees and other plant materials to be retained shall be identified on the landscape plan and grading plan.

Staff notes that trees to be retained are identified on the landscape plan, Sheets L-2-L-7 (dated August 5, 2016) and on the grading plan, Sheets C-2.00-2.60 (dated August 5, 2016). Staff recommends a condition of approval to maintain landscaping as approved through the Architectural Review Process.

Condition of Approval: *In conformance with TDC 73.100(1) and 73.250(1), all trees identified as Existing Trees to Remain on Sheets L-2 to L-7 (dated August 5, 2016), shall be relabeled as Existing Trees to be Retained. These trees shall be retained unless approval is obtained through the Community Development Department to remove trees through a modification to the landscape plan.*

- (2) During the construction process:

- (a) The owner or the owner's agents shall provide above and below ground protection for existing trees and plant materials identified to remain.
- (b) Trees and plant materials identified for preservation shall be protected by chain link or other sturdy fencing placed around the tree at the drip line.
- (c) If it is necessary to fence within the drip line, such fencing shall be specified by a qualified arborist as defined in TDC 31.060.
- (d) Neither top soil storage nor construction material storage shall be located within the drip line of trees designated to be preserved.

(e) Where site conditions make necessary a grading, building, paving, trenching, boring, digging, or other similar encroachment upon a preserved tree's drip-line area, such grading, paving, trenching, boring, digging, or similar encroachment shall only be permitted under the direction of a qualified arborist. Such direction must assure that the health needs of trees within the preserved area can be met.

(f) Tree root ends shall not remain exposed.

A Tree Assessment Report was provided by Morgan Holan and Associates and is dated July 29, 2016. This report contains tree protection recommendations on p. 4. Staff finds that Tree Notes are included for Tree Protection Measures on the Tree Preservation Plan, C-7.00 through C-7.70 (dated August 5, 2016); however tree protection is not illustrated on the grading plans. Staff recommends conditioning the applicant to meet this standard.

Condition of Approval: *The applicant shall revise the grading plan (Sheets C-2.00-2.60) to graphically illustrate and note trees to be retained in conformance with the tree preservation plan and tree preservation table Sheet C7.70 (dated August 5, 2016). Retained trees shall be preserved and protected during the construction process pursuant to TDC 73.250(2).*

L. Grading

TDC 73.270 Grading

(1) After completion of site grading, top-soil is to be restored to exposed cut and fill areas to provide a suitable base for seeding and planting.

Applicant Response: *The landscape plans specify a minimum of 12 inches of top soil amended with 4 inch depth of organic amendments in all planting areas. See Item 8 under Planting Notes on Sheet L8.*

Staff finds that this requirement has been met.

(2) All planting areas shall be graded to provide positive drainage.

Applicant Response: *The landscape plans call for all planting areas to be graded smooth sloped for positive drainage. See Item 7 under Planting Notes on Sheet L8.*

Staff finds that this requirement has been met.

(3) Neither soil, water, plant materials nor mulching materials shall be allowed to wash across roadways or walkways.

Staff notes that this requirement will be enforced with an erosion control permit.

(4) Impervious surface drainage shall be directed away from pedestrian walkways, dwelling units, buildings, outdoor private and shared areas and landscape areas except where the landscape area is a water quality facility.

As shown on the attached grading plans (Sheets C-2.00-2.60), drainage on impervious surfaces will be directed to proposed water quality facilities. This standard is met.

72.040 Natural Areas

(1) Natural Areas are the wetlands and upland open space areas on Map 72-1. They provide flood control, water quality, erosion control, fish and wildlife habitat, and valuable scenic qualities. Natural Areas may include restored and enhanced wetlands, park sites and other areas accessible by the public for passive recreation.

Map 72-1 has been provided as attachment 105. A portion on the northern end of the site falls within the Open Space Preservation District (OSNA). SWCA also surveyed the property and prepared a technical memorandum following the Clean Water Services (CWS) natural

resource site assessment requirements dated June 22, 2016 which delineates a 0.47 acre wetland onsite.

(2) Wetland Natural Areas.

(a) Wetland Preservation Natural Areas (NRPO-WPNA) are shown on Map 72-1. They include all land within a delineated wetland boundary.

(b) Wetland Conservation Natural Areas (NRPO-WCNA) are shown on Map 72-1. Except as provided in Subsection (c), they include all land within a delineated wetland boundary.

(c) For uses not permitted in TDC 72.060(3), excavation, fill or removal in a NRPO-WCNA is allowed subject to the Oregon Division of State Lands (DSL) requirements and the following standards:

(i) The wetland acreage affected by the excavation, fill or removal shall not exceed 30% of the subject property's delineated wetland acreage. The wetland acreage affected shall include excavation, fill or removal activities conducted since March 1, 1996.

(ii) The excavation, fill or removal shall not reduce or block water features such as springs, drainage courses and streams.

(iii) The wetland's functions and values listed in the City of Tualatin Natural Resource Inventory and Local Wetlands Inventory (December, 1995) shall be retained or improved through mitigation and/or enhancement. The wetland's functions and values may be assessed using the Oregon Freshwater Wetland Assessment Methodology (DSL, 1996, as amended).

(iv) Mitigation shall be conducted either on the subject property or within the same stream watershed as the subject wetland unless the applicant demonstrates the impracticality of doing so.

Applicant Response: SWCA Environmental Consultants determined, via a site visit, that one wetland and its vegetated corridor was present on the northern boundary of the campus. SWCA prepared a technical memorandum following the Clean Water Services (CWS) natural resource site assessment requirements listed under Chapter 3 of the June 2007 (amended August 2008) Design and Construction Standards (Resolution and Order Numbers 07-20 and 08-28) that is included in this application package. SWCA delineated the wetland and determined the extent and condition of the required 50 foot wide upland vegetated corridor. The location of the wetland and vegetated corridor boundaries, as determined by SWCA, are shown on Figure 5 of their technical memo and also on Sheet A-6 of the drawing package.

Staff notes that no excavation, fill, or removal is proposed in the wetland area. As noted, Clean Water Services will review water quality protection requirements of the surveyed wetland. Additionally the applicant is proposing to enhance the wetland to "good" condition as shown on Sheet L-7 (dated August 5, 2016). Clean Water Services provided a Service Provider Letter on August 15, 2016 that included special conditions regarding the vegetated corridor. The corresponding Public Facilities Report includes Clean Water Services recommended conditions of approval. This requirement has been met.

(3) Open Space Natural Areas

(a) Open Space Natural Areas (NRPO-OSNA) are shown on Map 72-1. They include upland forests, upland forests associated with slopes or streams, upland meadows, upland meadows associated with slopes or streams, the geologic features of the Tonquin Scablands, areas with slopes greater than 25%, areas within 50 feet of a delineated wetland and areas within 50 feet of a stream top of bank.

(b) The top of bank for the Tualatin River is as stated in TDC 72.030 and for all other streams is the bankfull stage. Slope is the vertical elevation divided by the horizontal distance of vertical change and is measured for a horizontal distance of 100 feet.

(c) Land use and permit applications shall show the NRPO-OSNA Boundary and shall include information on wetland delineations, location of streams, top of bank, topography and a vegetation inventory.

Staff notes that there is a NRPO-OSNA (Natural Resource Protection Overlay–Open Space Natural Area) located on the northern end of the site; however, no additional disruption in these areas would occur as a result of the proposed development.

Staff also notes that the applicant provided a Natural Resource Site Assessment and Request for Service Provider Letter CWS File Number 16-001957 authored by SWCA Environmental Consultants which is included as an attachment to these findings. The wetland and delineated wetland boundary are also illustrated on the proposed site plan and landscape plans. This requirement has been met.

72.060 Development Restrictions in Greenways and Natural Areas

(1) Except as provided in Subsection (2), no building, structure, grading, excavation, placement of fill, vegetation removal, impervious surface, use, activity or other development shall occur within Riverbank, Creek and Other Greenways, and Wetland and Open Space Natural Areas.

Staff finds that the proposed project would not result in any additional disruption in the adjacent wetland or NRPO-OSNA (Natural Resource Protection Overlay–Open Space Natural Area) areas.

(2) The following uses, activities and types of development are permitted within Riverbank, Creek and Other Greenways, and Wetland and Open Space Natural Areas provided they are designed to minimize intrusion into riparian areas:

(f) Landscaping, when part of a landscape plan approved through the Architectural Review process. City initiated landscape projects are exempt from the Architectural Review process. Landscaping in Greenways and Natural Areas shall comply with the approved Plant List in the Parks and Recreation Master Plan. When appropriate, technical advice shall be obtained from the Oregon Department of Fish and Wildlife, U.S. Soil Conservation Service, or similar agency, to ensure the proposed landscaping will enhance the preservation of any existing fish or wildlife habitats in the vicinity.

Applicant Response: SWCA reviewed the proposed development and states that "although a water quality–sensitive resource and vegetated corridor are present on the subject property, the proposed development plans are located in the southern half of the subject property and will not impact these on-site resources." SWCA found that portions of the vegetated corridor are in degraded condition. SWCA's planting recommendations are incorporated into the proposed landscape as shown on the Vegetated Corridor Buffer Planting Table of the technical memo and on sheet L-7 of the drawing package.

Staff finds this standard is met.

M. Off-Street Parking and Loading

73.370 Off-Street Parking and Loading

(1) General Provisions.

(b) At the time of enlargement of an existing multi-family residential, commercial, institutional or industrial structure or use, TDC 73.370 shall apply to the existing and enlarged structure or use.

(c) Except where otherwise specified, the floor area measured shall be the gross floor area of the building primary to the function of the particular use of the property other than space devoted to off-street parking or loading.

(d) Where employees are specified, the term shall apply to all persons, including proprietors, working on the premises during the peak shift.

(e) Calculations to determine the number of required parking spaces and loading berths shall be rounded to the nearest whole number.

(f) If the use of a property changes, thereby increasing off-street parking or loading requirements, the increased parking/loading area shall be provided prior to commencement of the new use.

(g) Parking and loading requirements for structures not specifically listed herein shall be determined by the Community Development Director, based upon requirements of comparable uses listed.

Applicant Response: The proposed development is a new building on an existing Medical campus. As previously discussed, the proposed building removes a portion of existing parking in two lots. The new building contains square footage housing relocated existing beds as well as new building area. Required off-street parking spaces, off-street vanpool and carpool parking spaces, off-street bicycle parking, and off-street loading berths have been calculated as required in (b) through (f) above for existing and new proposed campus uses as well as the spaces lost to the new building footprint.

Staff notes that the proposal is for a 163,203 sq ft expansion of the Legacy Meridian hospital building.

(n) Bicycle parking facilities shall include long-term parking that consists of covered, secure stationary racks, lockable enclosures, or rooms (indoor or outdoor) in which the bicycle is stored and short-term parking provided by secure stationary racks (covered or not covered), which accommodate a bicyclist's lock securing the frame and both wheels. The Community Development Director, their designee, or the Architectural Review Board may approve a form of bicycle parking not specified in these provisions but that meets the needs of long-term and/or short-term parking pursuant to Section 73.370.

Applicant Response: Meridian Park Medical Center currently has 27 bicycle stalls available for staff and patient use. Of those stalls, 23 of the 27 are covered and able to be secured. TriMet over the last 20 years has been providing commute surveys for Meridian Park (see attached survey). The 2014 employee commute survey showed that there were 2 bike commute trips reported and 2 bike/walk commute trips for a total of 4 users per day. From a patient perspective it is unlikely that a significant number of patients coming to the hospital's facilities would arrive by bike, therefore mostly staff would be using bike parking. Meridian Park is currently only using about 15% of its total bike parking capacity. The current campus square footage is 506,000 square feet. The proposed additional square footage totals 163,203 square feet, about a 32% increase in floor area. If the current number of bicycle commuters tripled, only 12 bicycle stalls are needed. Even doubling the stalls needed falls within the current total number of stalls that are located on campus. It appears that the current quantity of bicycle parking stalls is adequate for the long term growth of the campus. Of the existing parking space total, a covered short term bike rack with 4 spaces will be relocated to the proposed building entry and 4 indoor long term storage spaces will be relocated inside the proposed building. See Sheet A-14 for locations.



EMPLOYEE COMMUTE OPTIONS - Ninth Follow-up Survey Results		
Legacy Health System - Meridian Park Medical Center		
19300 SW 65th Avenue		
Tualatin, OR. 97062		
Employee population (Eco-affected)		635
Questionnaires returned (Out of 323)		254
Response rate		79%
<u>Survey</u>	<u>Date</u>	<u>Auto Trip Rate</u>
Baseline	Sep-96	93%
First Follow-up	Sep-97	91%
Second Follow-up	Sep-98	89%
Third Follow-up	Jul-01	80%
Fourth Follow-up	Jun-03	88%
Fifth Follow-up	Jul-04	89%
Sixth Follow-up	Jun-06	90%
Seventh Follow-up	Jul-08	90%
Eighth Follow-up	Aug-10	92%
Ninth Follow-up	Apr-14	95%
<u>Three year ECO goal (10% reduction in Baseline auto trip rate)</u>		
Auto trip rate goal		84%
Weekly auto trips to reduce		263

This report summarizes your employees' responses to the Employee Commute Options survey. The results identify the modes of transportation your employees use to commute to your worksite and the number of weekly auto trips their choices generate. This report assumes that your company will need to comply with the Department of Environmental Quality's Employee Commute Options (ECO) Rule that targets a 10% reduction in auto trips taken to the worksite.

Weekly Employee Trips

The table below shows the number of employee trips TO this worksite during the week prior to the survey.

Number of trips	Total Weekly Trips	Drove alone	Carpool/Vanpool (by # of people in Carpool)						Bus/Max	Bike	Walk	Bike+Walk	Tele-Commute	Compressed Work Wk.
			2	3	4	5	6+	Total						
Reported	911	862	8	1	0	0	0	9	0	2	0	2	0	38
Total*	2278	2155	20	3	0	0	0	23	0	5	0	5	0	95
Total Auto Trips*	2188	2155	10	1	0	0	0	11	0	0	0	0	0	0
Percentage of Total Trips														
Baseline	91%	4%	0%	0%	0%	0%	0%	4%	0.6%	1%	1%	2%	0%	3%
First Follow-up	89%	4%	0%	0%	0%	0%	0%	4%	0.4%	1%	1%	1%	0%	5%
Second Follow-up	86%	5%	1%	0%	0%	0%	0%	6%	1.6%	1%	2%	3%	1%	3%
Third Follow-up	78%	3%	0%	0%	0%	0%	0%	3%	4.4%	2%	1%	3%	0%	11%
Fourth Follow-up	86%	5%	1%	0%	0%	0%	0%	5%	5.1%	1%	1%	2%	0%	2%
Fifth Follow-up	87%	4%	1%	0%	0%	0%	0%	4%	4.7%	2%	0%	2%	0%	2%
Sixth Follow-up	88%	3%	0%	0%	0%	0%	0%	3%	2.1%	1%	2%	3%	0%	3%
Seventh Follow-up	88%	3%	1%	0%	0%	0%	0%	4%	1.6%	1%	1%	2%	1%	3%
Eighth Follow-up	90%	2%	0%	0%	0%	0%	0%	3%	1.0%	1%	1%	2%	0%	3%
Ninth Follow-up	95%	1%	0%	0%	0%	0%	0%	1%	0.0%	0%	0%	0%	0%	4%
Change from baseline**	-4%	-3%	0%	0%	0%	0%	0%	-3%	-0.6%	0%	-1%	-2%	0%	2%

*Adjusted to ECO-affected employees, N= 635.

**In percentage points, (Current Survey - Baseline). Figures may not add up due to rounding.

As shown in the table below, a total of 613 bicycle parking spaces would be required to meet code. Staff agrees that it is unlikely that many patients would bike to the hospital and that employees would likely be the primary users of bike parking. In review of the Trimet commuter survey, staff notes that 2 bike trips were reported but the number was adjusted for a total of 5 bike trips to account for hospital staff that did not participate in the survey. Staff agrees with the applicant's finding that 27 bike parking spaces remain adequate for the hospital use. Pursuant to TDC 73.370(1)(n) staff recommends that the Architectural Review Board approve the proposed 27 bicycle spaces that met the long-term and short-term parking needs as reflected in the finding.

Use	Bicycle Parking Requirement	Building GFA	Required Bike Parking
Hospital	1.00 spaces / 1,000 sf of gross floor area	470,345	471
Medical Offices	0.33 spaces / 1,000 sf of gross floor area	175,787	58
Warehousing	2, or 0.10 spaces / 1,000 sf of gross floor area; whichever is greater	13,820	2
Assembly	1.0 spaces / 4 seats	327 ~ seats	82
Total			613

(o) Each bicycle parking space shall be at least 6 feet long and 2 feet wide, and overhead clearance in covered areas shall be at least 7 feet, unless a lower height is approved through the Architectural Review process.

(p) A 5-foot-wide bicycle maneuvering area shall be provided beside or between each row of bicycle parking. It shall be constructed of concrete, asphalt or a pervious surface such as pavers or grasscrete, but not gravel or woody material, and be maintained.

(q) Access to bicycle parking shall be provided by an area at least 3 feet in width. It shall be constructed of concrete, asphalt or a pervious surface such as pavers or grasscrete, but not gravel or woody material, and be maintained.

(r) Required bicycle parking shall be located in convenient, secure, and well-lighted locations approved through the Architectural Review process. Lighting, which may be provided, shall be deflected to not shine or create glare into street rights-of-way or fish and wildlife habitat areas.

The covered short term bike rack with 4 spaces located at the proposed building entry is shown on Sheet A-15. These spaces are 6 ft by 2 ft with 24 ft of clearance. More than 5 ft of paved maneuvering area is located north of the parking rack. The proposed location of the Main Entrance Canopy area is convenient, secure, and well lighted, meeting the requirements.

(s) Long-term bicycle parking facilities may be provided inside a building in suitable secure and accessible locations.

The indoor long term bike storage located inside the south rear entry of the proposed building is shown on Sheet A-27 (dated August 5, 2016). This requirement has been met.

(u) Bicycle parking areas and facilities shall be identified with appropriate signing as specified in the Manual on Uniform Traffic Control Devices (MUTCD) (latest edition). At a minimum, bicycle parking signs shall be located at the main entrance and at the location of the bicycle parking facilities.

The plans do not clearly indicate that appropriate bicycle parking signage is provided. Staff recommends iteration of this standard as a condition of approval.

Condition of Approval: *The applicant shall revise the appropriate sheets to illustrate that bicycle parking areas and facilities shall be identified with appropriate signing as specified in the Manual on Uniform Traffic Control Devices (MUTCD) (latest edition) and install to revised plans.*

(x) Required vanpool and carpool parking shall meet the 9-foot parking stall standards in Figure 73-1 and be identified with appropriate signage.

Applicant Response: *The proposed vanpool and carpool parking spaces are 9 feet wide and will be identified with signs as specified in the Section 38 Sign Regulations for Medical Campus.*

Staff notes that adequate vanpool and carpool parking dimensions are shown on site plans Sheet A-14 to A-21(dated October 17, 2016).

(2) Off-Street Parking Provisions.

(a) The following are the minimum and maximum requirements for off-street motor vehicle parking in the City, except for minimum parking requirements for the uses in TDC 73.370(2)(a) (Residential Uses: iii, iv, v, vi, vii; Places of Public Assembly: I, ii, iv; Commercial Amusements: I, ii; and Commercial: I, ii, xi, xii, xiv) within the Core Area Parking District (CAPD). Minimum standards for off-street motor vehicle parking for the uses in 73.370(2) (a) Residential Uses: iii, iv, v, vi, vii; Places of Public Assembly: I, ii, iv; Commercial Amusements:

I, ii; and Commercial: I, ii, xi, xii, xiv in the CAPD are in TDC 73.370(2)(b). The maximum requirements are divided into Zone A and Zone B, as shown on the Tualatin Parking Zone Map, Figure 73-3. The following are exempt from calculation of maximum parking requirements: parking structures; fleet parking; parking for vehicles for sale, lease or rent; car/vanpool parking; dedicated valet parking; and user-paid parking.

Use	Minimum Parking Requirement	Maximum Parking Requirement
Hospital	1.00 spaces / 500 sf of gross floor area	None
Medical Offices	3.90 spaces / 1,000 sf of gross floor area	Zone B: 5.9 spaces / 1,000 sf of gross floor area
Warehousing	0.3 spaces /1,000 sf of gross floor area	Zone B: 0.5 spaces / 1,000 sf of gross floor area
Assembly	1.0 spaces / 4 seats	Zone B: 0.5 spaces / seat

Applicant Response: The campus is located in Zone B. For the purposes of calculating parking, the minimum ratios for hospital, medical office, warehouse, and assembly (Education Center) were used. As discussed in the narrative for Section 73.370(1), 4 spaces more than the minimum required are proposed. See parking ratio calculations by building type.

Applicant Response from 73.370(1): In total campus wide, 1,713 spaces are required and 1,716 are provided.

SUMMARY OF BUILDING FLOOR AREAS									
Floor Level	Existing Campus								New Expansion
	19250 SW 65th Medical Plaza I	19360 SW 65th Medical Plaza II	6475 SW Borland Rd Physician Offices	6485 SW Borland Rd Physician Offices	6489 SW Borland Rd Radiation/Oncology	19300 SW 65th Education Center	19300 SW 65th Physical Plant	19300 SW 65th Hospital	New Patient Care Expansion
Basement	[-]	[-]	[-]	[-]	[-]	[-]	[-]	[-]	[-]
Level 1	22,137	20,136	3,948	7,210	6,767	9,241	13,820	76,346	6,911
Level 2	21,867	18,866	8,641	6,130	[-]	[-]	[-]	150,963	39,341
Level 3	21,967	19,709	[-]	[-]	[-]	[-]	[-]	58,981	32,441
Level 4	[-]	18,409	[-]	[-]	[-]	[-]	[-]	[-]	42,255
Level 5	[-]	[-]	[-]	[-]	[-]	[-]	[-]	[-]	42,255
Total Area (sq ft)	86,871	77,120	12,688	13,340	6,787	9,241	13,820	307,142	183,208
Use	Medical Office Building					Assembly	Warehouse	Hospital	Hospital

SUMMARY OF PARKING									
Parking Ratio	3.90/1000	3.90/1000	3.90/1000	3.90/1000	3.90/1000	327/4 (*)	0.3/1000	1/500	1/500
Pkg Spaces Req'd	257.29	300.77	49.10	52.03	26.39	81.75	4.15	614.28	326.41
						OSGC Table 11.05.1	Hospital req'd 2%	Hospital req'd 2%	Hospital req'd 2%
ADA Spaces Req'd	25.7	30.1	4.9	5.2	2.6	4.0	0.1	12.3	6.5
						TOTAL SPACES REQUIRED			
						Total Vehicle Parking Spaces Required			
						1,712.2			
						TOTAL ADA SPACES REQUIRED			
						See summary chart above			
						91.5			

*EDUCATION CENTER OCCUPANT CALCULATION			
Use	Floor Area (sq ft)	Load Factor (sf/occupant)	Number of Occupants
Assembly	4,811	15	321
Business	262	100	3
Kitchen	485	200	2
Storage	380	300	1
Totals			327

* Because of use, occupant load used to calculate required parking

SUMMARY OF NEW CAMPUS PARKING							
	Standard	Compact	ADA accessible Car per table above	ADA accessible Van 1/5 of ADA required	Carpool	RV	TOTAL
Parking Area A	91	38	37	5	0	0	171
Parking Area B	161	0	0	0	16	0	177
Parking Area C	64	0	3	1	0	0	68
Parking Area D	109	0	8	2	15	0	134
Parking Area E	4	43	2	0	0	0	49
Parking Area F	84	36	2	0	22	4	148
Parking Area G	72	25	0	0	0	0	97
Parking Area J	79	64	0	0	0	0	143
Parking Area K	124	32	0	0	16	0	172
Parking Area L	34	8	6	2	0	0	50
Parking Area M	59	0	2	2	0	0	63
Parking Area N	21	24	10	3	0	0	58
Parking Structure P	285	88	8	5	0	0	386
Total	1187	358	78	20	69	4	1716
TOTAL SPACES PROVIDED							1716

TOTAL SPACES PROVIDED	
Total Vehicle Parking Spaces Provided	1,716

TOTAL ADA SPACES PROVIDED	
Total ADA Vehicle Parking Spaces Provided	98

Staff notes that 3 additional spaces are proposed over the requirement. Staff reviewed bicycle parking requirements in TDC 73.370(1)(n) and recommends approving the proposed 27 spaces based off of the 2014 employee commute survey data provided. This requirement has been met.

(3) Off-Street Vanpool and Carpool Parking Provisions.

The minimum number of off-street Vanpool and Carpool parking for commercial, institutional and industrial uses is as follows:

Number of Required Parking Spaces	Number of Vanpool or Carpool Spaces
0 to 10	1
10 to 25	2
26 and greater	1 for each 25 spaces

Applicant Response: Of the 1,716 spaces proposed, 69 are vanpool or carpool spaces. See previous chart for Summary of New Campus Parking for location of these space by lot.

Staff notes that the minimum parking required is 1,713 spaces, requiring 69 vanpool or carpool spaces. The requirement has been met.

73.380 Off-Street Parking Lots

A parking lot, whether an accessory or principal use, intended for the parking of automobiles or trucks, shall comply with the following:

(1) Off-street parking lot design shall comply with the dimensional standards set forth in Figure 73-1 of this section, except for parking structures and underground parking where stall

length and width requirements for a standard size stall shall be reduced by .5 feet and vehicular access at the entrance if gated shall be a minimum of 18 feet in width.

Applicant Response: All proposed and reconfigured parking stalls comply with the dimensional standards of Figure 73-1.

Staff finds that standard stalls are 9 x 18.5 ft and compact stalls are 9 x 15 ft. There is one, one-way aisle proposed in Lot A (Sheet A-14, dated August 5, 2016) at 12.17 ft that meets the standard. Two-way access aisles without parking vary between 23.33-26 ft, which meets the standard. Lot L (Sheet A-19, dated August 5, 2016) has a proposed aisle width between stall lines of 22.3 ft when the standard is 24 ft. Staff recommends a condition of approval to meet the standard.

Condition of Approval: *The applicant shall revise the appropriate sheets to illustrate that aisles between parking stall lines are at least 24 ft in width and install to revised plans.*

(2) Parking stalls for sub-compact vehicles shall not exceed 35 percent of the total parking stalls required by TDC 73.370(2). Stalls in excess of the number required by TDC 73.370(2) can be sub-compact stalls.

Applicant Response: 357 sub-compact spaces are proposed equaling 20.8% of the total 1,713 parking spaces required.

Staff notes that 358 compact spaces are being proposed which still meets the standard.

(3) Off-street parking stalls shall not exceed eight continuous spaces in a row without a landscape separation, except for parking structures and underground parking. For parking lots within the Central Design District that are designed to frame views of the central water feature or identified architectural focal elements as provided in TDC 73.350(3), this requirement shall not apply and the location of parking lot landscape islands shall be determined through the Architectural Review process.

Applicant Response: The number of continuous spaces without a landscape separation proposed varies from 4 to 8. See Sheets A15 through A21 for parking lot site plans.

Staff notes that the proposal contains continuous spaces of 3 to 8, meeting the requirement.

(4) Parking stalls shall be constructed of asphalt or concrete, or a pervious surface such as pavers or grasscrete, but not gravel or woody material. Drive aisles and parking stalls shall be maintained adequately for all-weather use and drained to avoid water flow across sidewalks. Pervious surfaces such as pervious concrete, pavers and grasscrete, but not gravel or woody material, are encouraged for parking stalls in or abutting the Natural Resource Protection Overlay District, Other Natural Areas identified in Figure 3-4 of the Parks and Recreation Master Plan, or in a Clean Water Services Vegetated Corridor. Parking lot landscaping shall be provided pursuant to the requirements of TDC 73.350 and TDC 73.360. Walkways in parking lots shall be provided pursuant to TDC 73.160.

Applicant Response: Parking lot drive aisles and parking spaces are either concrete or asphalt. Parking lot drainage, landscaping and walkways are addressed in previous Sections.

(5) Except for parking to serve residential uses, parking areas adjacent to or within residential planning districts or adjacent to residential uses shall be designed to minimize disturbance of residents.

Applicant Response: Parking lot J is set back 38 feet from the property line abutting residential land to the north and east. Land to the north is buffered by a change in grade and an existing wetland and uplands area. Residential land to the east of campus is owned by Legacy, parking is set back from this property line and/or is buffered by the internal campus

loop road. Other proposed parking is oriented toward public rights of way and shielded from residential areas by existing and the proposed campus building.

Staff notes that extensive perimeter landscaping, including Douglas fir trees are proposed along Lot J on Sheets L-6 to 7 (dated August 5, 2016).

(6) Artificial lighting, which may be provided, shall be deflected to not shine or create glare in a residential planning district, an adjacent dwelling, street right-of-way in such a manner as to impair the use of such way or a Natural Resource Protection Overlay District, Other Natural Areas identified in Figure 3-4 of the Parks and Recreation Master Plan, or a Clean Water Services Vegetated Corridor.

Applicant Response: Proposed lighting will have deflectors to shield residential areas, public rights of way, and the NRPO from glare.

Staff finds that there is a NRPO-OSNA (Natural Resource Protection Overlay–Open Space Natural Area) located on the northern end of the site. However, as shown in the site lighting plan, (Sheet E1.00, dated August 5, 2016), the proposed development will not result in an unacceptable level of light spilling onto these adjacent areas.

(8) Service drives to off-street parking areas shall be designed and constructed to facilitate the flow of traffic, provide maximum safety of traffic access and egress, and maximum safety for pedestrians and vehicular traffic on the site.

Applicant Response: Proposed parking ties into existing, already approved campus loop road.

Staff notes this standard is met on Sheet A-14, dated October 17, 2016.

(9) Parking bumpers or wheel stops or curbing shall be provided to prevent cars from encroaching on the street right-of-way, adjacent landscaped areas, or adjacent pedestrian walkways.

Applicant Response: Wheel stops and curbing are proposed to prevent cars from encroaching on campus walkways and landscaped areas.

Staff notes that pedestrian walkways are proposed throughout parking areas. There are three instances proposed where pedestrian areas are not separated from parking stalls by parking bumpers, wheel stops, or curbing as required. This requirement is not met along the ADA parking lane with concrete walkway in Lot A (Sheet A-15), along the ADA parking lane with striped crosswalk proposed in Lot C (Sheet A-18), and along the ADA parking lane with undisclosed pedestrian treatment proposed in Lot L (Sheet A-19). Staff recommends iteration of this standard as a condition of approval.

Condition of Approval: The applicant shall revise the appropriate sheets to illustrate that parking bumpers, wheel stops, curbing, or ADA compliant treatment is provided along adjacent pedestrian walkways in Lot A, C, and L on Sheet A-14 (dated October 17, 2016) and install to revised plans.

(10) Disability parking spaces and accessibility shall be provided in accordance with applicable federal and state requirements.

Applicant Response: Disability parking stalls will be designed to meet applicable accessibility requirements of the OSSC and ANSI/ICC A117.

(11) On-site drive aisles without parking spaces, which provide access to parking areas with regular spaces or with a mix of regular and sub-compact spaces, shall have a minimum width of 22 feet for two-way traffic and 12 feet for one-way traffic. On-site drive aisles without

parking spaces, which provide access to parking areas with only sub-compact spaces, shall have a minimum width of 20 feet for two-way traffic and 12 feet for one-way traffic.

Applicant Response: On site drive aisles without parking are two way and 24 feet wide.

Staff notes that all drive aisles between on-site parking areas are 22.33-26 ft wide. This requirement has been met.

73.390 Off-Street Loading Facilities

(1) The minimum number of off-street loading berths for commercial, industrial, public and semi-public uses is as follows:

Square Feet of Floor Area	Number of Berths
Less than 5,000	0
5,000 - 25,000	1
25,000 - 60,000	2
60,000 and over	3

Applicant Response: The existing docks meet the requirement of 3 berths.

Staff notes that loading berth requirements were met by AR97-12. No new berths are proposed as part of the hospital expansion. The existing docks are not located in the defined area of work.

N. Access

73.400 Access

(1) The provision and maintenance of vehicular and pedestrian ingress and egress from private property to the public streets as stipulated in this Code are continuing requirements for the use of any structure or parcel of real property in the City of Tualatin. Access management and spacing standards are provided in this section of the TDC and TDC Chapter 75. No building or other permit shall be issued until scale plans are presented that show how the ingress and egress requirement is to be fulfilled. If the owner or occupant of a lot or building changes the use to which the lot or building is put, thereby increasing ingress and egress requirements, it shall be unlawful and a violation of this code to begin or maintain such altered use until the required increase in ingress and egress is provided.

Applicant Response: The campus is accessed from four existing points, two on SW 65th Avenue and two on Borland Road. These campus entrances connect to the private loop road that serves all campus buildings and parking lots. Based on the transportation analysis report prepared by Kittelson & Associates, Inc. proposed building can be constructed "while maintaining acceptable operations" at these access points. Based on this analysis, no improvements at the access points are proposed.

Staff received comments from Robert Hixson, Development Engineering at Clackamas County dated December 21, 2016 with conditions to improve Borland Road frontage to Clackamas County standards, sign and public utility easement, and improvements to the two Borland Road driveway approaches. These comments may be reviewed in Attachment 105 and have been included in the Public Facilities Review or AR16-0009.

P. Signs

The applicant has separately submitted sign permit applications for any changed or new signage. Signage is not approved or reviewed through this AR application.

Q. Time Limit on Approval

73.056 Architectural Review approvals shall expire after two years unless:

(1) A building, or grading permit submitted in conjunction with a building permit application, has been issued and substantial construction pursuant thereto has taken place and an inspection performed by a member of the Building Division; or

(2) The Architectural Review (AR) applicant requests in writing an extension and the City approves it. If the Community Development Director and City Engineer or their designees approved the AR, then the Community Development Director and City Engineer shall decide upon the extension request. If the Architectural Review Board (ARB) approved the AR, then the ARB shall decide upon the extension request. The applicant shall provide notice of extension request to past recipients of the AR notice of application and post a sign pursuant to TDC 31.064. Before approving an extension, the deciding party shall find the request meets these criteria:

(a) The applicant submitted a written extension request prior to the original expiration date.

(b) There have been no significant changes in any conditions, ordinances, regulations or other standards of the City or applicable agencies that affect the previously approved project so as to warrant its resubmittal for AR.

(c) If the previously approved application included a special study, the applicant provided with the extension a status report that shows no significant changes on the site or within the vicinity of the site. A letter from a recognized professional also would satisfy this criterion if it states that conditions have not changed after the original approval and that no new study is warranted.

(d) If the AR applicant neglected site maintenance and allowed the site to become blighted, the deciding party shall factor this into its decision.

(e) The deciding party shall grant no more than a single one-year extension for an AR approval.

(f) If the Community Development Director and City Engineer or their designees are the deciding party, then they shall decide within thirty (30) days of receipt of the request. If the ARB is the deciding party, then the ARB shall decide within sixty (60) days of receipt of the request. If the deciding party fails to decide within the applicable time period, the decision shall default to approval.

III. RECOMMENDED CONDITIONS

Based on the Findings and Conclusions presented, AR-16-0009 staff recommends approval, subject to the following Architectural Review conditions of approval:

CONDITIONS OF APPROVAL DOCUMENTATION:

AR-1 Prior to obtaining building permits on the subject site, the applicant shall submit one revised paper plan set—24 x 36, a paper narrative, and electronically in Adobe PDF file format—for review and approval to the Planning Division that meet the conditions of approval below. The narrative shall explain how and on what page each condition of approval has been met. The submittal shall contain page numbers and a table of contents. No piecemeal submittals will be accepted. Each submittal shall be given directly to planning staff and will be reviewed in two weeks.

PRIOR TO APPLICATION FOR A GRADING PERMIT:

AR-2 The applicant shall revise the grading plan (Sheets C-2.00-2.60) to graphically illustrate and note trees to be retained in conformance with the tree preservation plan and tree preservation table Sheet C7.70 (dated August 5, 2016). Retained trees shall

be preserved and protected during the construction process pursuant to TDC 73.250(2).

PRIOR TO APPLICATION FOR A BUILDING PERMIT:

- AR-3** The applicant shall revise the appropriate sheets to illustrate that walkways are a minimum of 6 feet wide and install to revised plans. TDC 73.160(1)(a)(ii)
- AR-4** The applicant shall revise the appropriate sheets to illustrate that walkways through parking areas, drive aisles, and loading areas are visibly raised and of a different appearance than the adjacent paved vehicular areas or ADA compliant and install to revised plans. TDC 73.160(1)(a)(iii)
- AR-5** The applicant shall revise the appropriate sheets to illustrate that accessways are provided as a connection between the development's internal walkways and abutting arterial streets upon which bike lanes are designated and install to revised plans. Accessways shall be a minimum of 8 ft wide and meet the standards set forth in TDC 73.160(1)(a)(iv), 73.160(1)(d)
- AR-6** The applicant shall revise the appropriate sheets to illustrate that curb ramps are provided wherever a walkway or accessway crosses a curb and install to revised plans. TDC 73.160(1)(c)
- AR-7** The applicant shall revise the appropriate sheets to note that shrubs in parking areas must not exceed 30 inches in height. Tree canopies must not extend below 8 feet measured from grade and prune appropriately. TDC 73.160(3)(e), 73.220(1)(c)
- AR-8** The applicant shall supply a letter from Trimet indicating their acceptance of the proposed transit pad design. TDC 73.160(6)(a)
- AR-9** The applicant shall revise the appropriate sheets to illustrate that the exterior trash enclosure area shall have either a concrete or asphalt floor surface and install to revised plans. TDC 73.227(6)(b)(iv)
- AR-10** In conformance with TDC 73.100(1) and 73.250(1), all trees identified as Existing Trees to Remain on Sheets L-2 to L-7 (dated August 5, 2016), shall be relabeled as Existing Trees to be Retained. These trees shall be retained unless approval is obtained through the Community Development Department to remove trees through a modification to the landscape plan.
- AR-11** The applicant shall revise Landscape Plan, Sheet L-1 (dated August 5, 2016) to reflect the total landscape area in either a table or with statistics that correspond to the landscape total presented on Proposed Site Plan, Sheet A-14 (dated October 17, 2016). Total landscape area shall meet the minimum requirements of 73.240(1)(4).
- AR-12** The applicant shall revise the appropriate sheets to illustrate that Deciduous trees shall be a minimum one and one-half inch (1 1/2") caliper measured six inches (6") above ground, balled and burlapped. Coniferous trees shall be a minimum five feet (5') in height above ground, balled and bur lapped. Trees shall be installed to revised plans. TDC 73.260(1)
- AR-13** Any plan set revisions in response to Clackamas County Development Engineering review of Borland Road improvements shall comply with all applicable policies and regulations of the Tualatin Development Code (TDC) including, but not limited to: 34.210, 56.050(3), 73.340, 73.360, 73.370, 73.380, 73.400.

The applicant shall submit revised site plans showing compliance with the Tualatin Development Code and shall construct and install improvements per the revised plan set approved by the Planning Division.

- AR-14** The applicant shall revise the appropriate sheets to illustrate that bicycle parking areas and facilities shall be identified with appropriate signing as specified in the Manual on Uniform Traffic Control Devices (MUTCD) (latest edition) and install to revised plans. TDC 73.370(1)(u)
- AR-15** The applicant shall revise the appropriate sheets to illustrate that parking bumpers, wheel stops, curbing, or ADA compliant treatment is provided along adjacent pedestrian walkways in Lot A, C, and L on Sheet A-14 (dated October 17, 2016) and install to revised plans. TDC 73.380(9)
- AR-16** The applicant shall revise the appropriate sheets to illustrate that aisles between parking stall lines are at least 24 ft in width and install to revised plans. TDC 73.380(1)
- AR-17** The applicant shall comply with the incorporated Public Facilities Recommendation (PFR) from the Engineering Division.

PRIOR TO ISSUANCE OF CERTIFICATE OF OCCUPANCY:

- AR-18** The Applicant shall construct proposed buildings and all site improvements as illustrated on approved plans and conditions of approval.

THE FOLLOWING CODE REQUIREMENTS APPLY TO THE SITE IN AN ON-GOING MANNER:

- The applicant must submit sign permit applications separately from this AR for any proposed signage.
- Accessways shall be constructed, owned and maintained by the property owner. TDC 73.160(1)(g)
- All landscaping approved through architectural review (AR) must be continually maintained, including necessary watering, weeding, pruning and replacement, in a manner substantially similar to that originally approved by the AR decision, unless subsequently altered with Community Development Director's approval. TDC 73.100(1)
- All building exterior improvements approved through the Architectural Review Process must be continually maintained, including necessary painting and repair, so as to remain substantially similar to original approval through the Architectural Review Process, unless subsequently altered with Community Development Director's approval. TDC 73.100(2)
- Site landscaping and street trees shall be maintained to meet the vision clearance requirements of TDC 73.400(16).

Submitted by:



Erin Engman
Assistant Planner



Aquilla Hurd-Ravich
Planning Manager

Attachments:

- 101: Application Materials—submitted August 16, 2016 and revised in part October 24, 2016
- 102: Republic Services letter, dated July 8, 2016
- 103: Tualatin Valley Fire & Rescue (TVF&R) letter, dated December 13, 2016
- 104: CWS letter, dated December 30, 2016
- 105: Clackamas County letter, dated December 21, 2016
- 106: Map 72-1



REPUBLIC
SERVICES

10295 SW Ridder Road, Wilsonville, OR 97070
O: 503.404-2135 F: 503.682-9004 republicservices.com

July 8, 2016

Kate Weinert
PKA Architects P.C. AIA

Re: Meridian Park

Dear Kate;

Thank you, for sending me the site plans and enclosure specifications for the changes being planned for Meridian Park Hospital.

My Company: Republic Services of Clackamas & Washington Counties has the franchise agreement to service this area with the City of Tualatin. We provide complete Commercial waste removal services and recycling services as needed on a weekly basis for this location.

I do not see any interference of where the enclosure is being planned that will affect your garbage & recycling services. The size of the enclosure will be fine for my containers. Please remember to allow the gates be in an open locked position over 120 degrees when we are servicing. The compactors will continue to be serviced as usual.

Thanks for your help and concerns for our services prior to this project being developed.

Sincerely,

Frank J. Loneragan
Operations Manager
Republic Services Inc.

SITE INFORMATION:

PLANNING DISTRICT: _____

SITE SQUARE FOOTAGE: _____

DEVELOPMENT AREA SQUARE FOOTAGE: _____

LANDSCAPE SQUARE FOOTAGE: _____

PARKING LOT LANDSCAPE SQUARE FOOTAGE: _____

PARKING SQUARE FOOTAGE: _____

NUMBER OF PARKING SPACES: _____

STANDARD: _____

COMPACT: _____

ADA: _____

CARPOOL: _____

VANPOOL: _____

BUILDING SQUARE FOOTAGE: _____

LOADING DOORS: _____



WASTE HAULING PLAN - PROPOSED

SCALE: 1"= 100'

LEGACY HEALTH MERIDIAN PARK PATIENT CARE EXPANSION

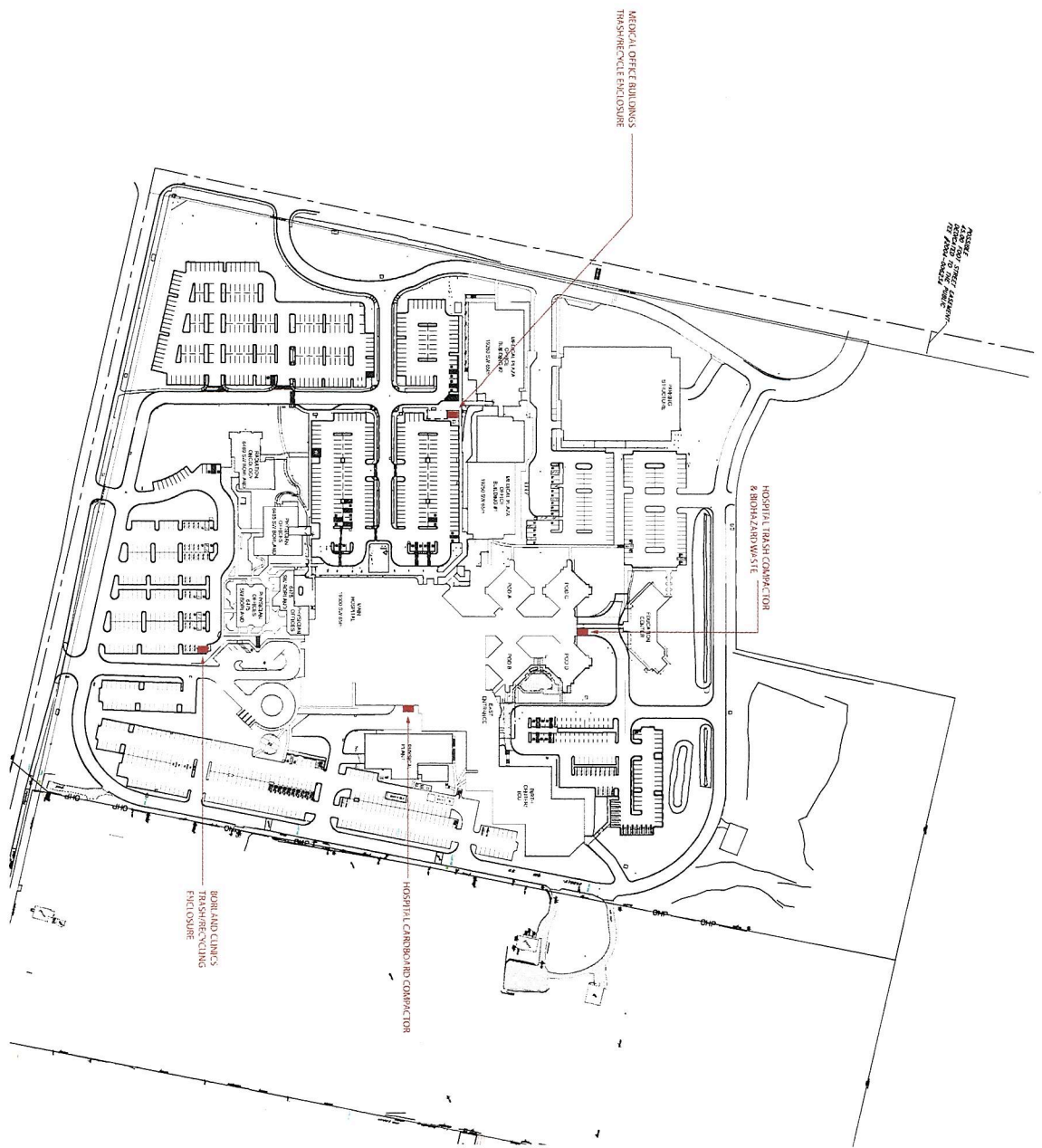


79 Design 7/5/16

78 Jovanna 7/8/16

WASTE HAULING PLAN - EXISTING

SCALE: 1"= 100'





December 13, 2016

City of Tualatin
Erin Engman-Assistant Planner
18880 SW Martinazzi Rd.
Tualatin, OR 97062

Re: AR16-0009 , 19300 SW 65th Avenue, Tualatin, OR 97062
Tax Lot I.D: 21E 19C 01200, 01300, 01400, 01500, and 01600

Dear Erin,

Thank you for the opportunity to review the proposed site plan surrounding the above named development project. Tualatin Valley Fire & Rescue endorses this proposal predicated on the following criteria and conditions of approval:

FIRE APPARATUS ACCESS:

- FIRE APPARATUS ACCESS ROAD DISTANCE FROM BUILDINGS AND FACILITIES:** Access roads shall be within 150 feet of all portions of the exterior wall of the first story of the building as measured by an approved route around the exterior of the building or facility. An approved turnaround is required if the remaining distance to an approved intersecting roadway, as measured along the fire apparatus access road, is greater than 150 feet. (OFC 503.1.1))
- DEAD END ROADS AND TURNAROUNDS:** Dead end fire apparatus access roads in excess of 150 feet in length shall be provided with an approved turnaround. Diagrams of approved turnarounds are shown below: (OFC 503.2.5 & D103.1)
- ADDITIONAL ACCESS ROADS – COMMERCIAL/INDUSTRIAL HEIGHT:** Buildings exceeding 30 feet in height or three stories in height shall have at least two separate means of fire apparatus access. (D104.1)
- ADDITIONAL ACCESS ROADS – COMMERCIAL/INDUSTRIAL SQUARE FOOTAGE:** Buildings or facilities having a gross building area of more than 62,000 square feet shall have at least two approved separate means of fire apparatus access. Exception: Projects having a gross building area of up to 124,000 square feet that have a single approved fire apparatus access road when all buildings are equipped throughout with approved automatic sprinkler systems. (OFC D104.2)
- AERIAL FIRE APPARATUS ROADS:** Buildings with a vertical distance between the grade plane and the highest roof surface that exceeds 30 feet in height shall be provided with a fire apparatus access road constructed for use by aerial apparatus with an unobstructed driving surface width of not less than 26 feet. For the purposes of this section, the highest roof surface shall be determined by measurement to the eave of a pitched roof, the intersection of the roof to the exterior wall, or the top of the parapet walls, whichever is greater. Any portion of the building may be used for this measurement, provided that it is accessible to firefighters and is capable of supporting ground ladder placement. (OFC D105.1, D105.2)

6. **AERIAL APPARATUS OPERATIONS:** At least one of the required aerial access routes shall be located within a minimum of 15 feet and a maximum of 30 feet from the building, and shall be positioned parallel to one entire side of the building. The side of the building on which the aerial access road is positioned shall be approved by the fire code official. Overhead utility and power lines shall not be located over the aerial access road or between the aerial access road and the building. (D105.3, D105.4)
7. **MULTIPLE ACCESS ROADS SEPARATION:** Where two access roads are required, they shall be placed a distance apart equal to not less than one half of the length of the maximum overall diagonal dimension of the area to be served (as identified by the Fire Code Official), measured in a straight line between accesses. (OFC D104.3) Exception: Buildings equipped throughout with an approved automatic fire sprinkler system (the approval of this alternate method of construction shall be accomplished in accordance with the provisions of ORS 455.610(5).
8. **FIRE APPARATUS ACCESS ROAD WIDTH AND VERTICAL CLEARANCE:** Fire apparatus access roads shall have an unobstructed driving surface width of not less than 20 feet (26 feet adjacent to fire hydrants (OFC D103.1)) and an unobstructed vertical clearance of not less than 13 feet 6 inches. The fire district will approve access roads of 12 feet for up to three dwelling units and accessory buildings. (OFC 503.2.1 & D103.1)
9. **NO PARKING SIGNS:** Where fire apparatus roadways are not of sufficient width to accommodate parked vehicles and 20 feet of unobstructed driving surface, "No Parking" signs shall be installed on one or both sides of the roadway and in turnarounds as needed. Signs shall read "NO PARKING - FIRE LANE" and shall be installed with a clear space above grade level of 7 feet. Signs shall be 12 inches wide by 18 inches high and shall have red letters on a white reflective background. (OFC D103.6)
10. **NO PARKING:** Parking on emergency access roads shall be as follows (OFC D103.6.1-2):
1. 20-26 feet road width – no parking on either side of roadway
 2. 26-32 feet road width – parking is allowed on one side
 3. Greater than 32 feet road width – parking is not restricted
11. **PAINTED CURBS:** Where required, fire apparatus access roadway curbs shall be painted red (or as approved) and marked "NO PARKING FIRE LANE" at 25 foot intervals. Lettering shall have a stroke of not less than one inch wide by six inches high. Lettering shall be white on red background (or as approved). (OFC 503.3)
12. **FIRE APPARATUS ACCESS ROADS WITH FIRE HYDRANTS:** Where a fire hydrant is located on a fire apparatus access road, the minimum road width shall be 26 feet and shall extend 20 feet before and after the point of the hydrant. (OFC D103.1)
13. **SURFACE AND LOAD CAPACITIES:** Fire apparatus access roads shall be of an all-weather surface that is easily distinguishable from the surrounding area and is capable of supporting not less than 12,500 pounds point load (wheel load) and 75,000 pounds live load (gross vehicle weight). Documentation from a registered engineer that the final construction is in accordance with approved plans or the requirements of the Fire Code may be requested. (OFC 503.2.3)
14. **TURNING RADIUS:** The inside turning radius and outside turning radius shall not be less than 28 feet and 48 feet respectively, measured from the same center point. (OFC 503.2.4 & D103.3)
15. **ACCESS ROAD GRADE:** Fire apparatus access roadway grades shall not exceed 12%. When fire sprinklers* are installed, a maximum grade of 15% will be allowed.

0-12%	Allowed
13-15%	Special consideration with submission of written Alternate Methods and Materials request. Ex: Automatic fire sprinkler (13-D) system* in lieu of grade.
≥16%	Special consideration on a case by case basis with submission of written Alternate Methods and Materials request Ex: Automatic fire sprinkler (13-D)

system* plus additional engineering controls in lieu of grade.**
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*The approval of fire sprinklers as an alternate shall be accomplished in accordance with the provisions of ORS 455.610(5) and OAR 918-480-0100 and installed per section 903.3.1.1, 903.3.1.2, or 903.3.1.3 of the Oregon Fire Code (OFC 503.2.7 & D103.2)

** See Forest Dwelling Access section for exceptions.

16. **ANGLE OF APPROACH/GRADE FOR TURNAROUNDS:** Turnarounds shall be as flat as possible and have a maximum of 5% grade with the exception of crowning for water run-off. (OFC 503.2.7 & D103.2)
17. **ANGLE OF APPROACH/GRADE FOR INTERSECTIONS:** Intersections shall be level (maximum 5%) with the exception of crowning for water run-off. (OFC 503.2.7 & D103.2)
18. **AERIAL APPARATUS OPERATING GRADES:** Portions of aerial apparatus roads that will be used for aerial operations shall be as flat as possible. Front to rear and side to side maximum slope shall not exceed 10%.
19. **GATES:** Gates securing fire apparatus roads shall comply with all of the following (OFC D103.5, and 503.6):
 1. Minimum unobstructed width shall be not less than 20 feet (or the required roadway surface width).
 2. Gates serving three or less single-family dwellings shall be a minimum of 12 feet in width.
 3. Gates shall be set back at minimum of 30 feet from the intersecting roadway or as approved.
 4. Electric gates shall be equipped with a means for operation by fire department personnel
 5. Electric automatic gates shall comply with ASTM F 2200 and UL 325.
20. **ACCESS DURING CONSTRUCTION:** Approved fire apparatus access roadways shall be installed and operational prior to any combustible construction or storage of combustible materials on the site. Temporary address signage shall also be provided during construction. (OFC 3309 and 3310.1)
21. **TRAFFIC CALMING DEVICES:** Shall be prohibited on fire access routes unless approved by the Fire Code Official. (OFC 503.4.1).

FIREFIGHTING WATER SUPPLIES:

22. **MUNICIPAL FIREFIGHTING WATER SUPPLY EXCEPTIONS:** The requirements for firefighting water supplies may be modified as approved by the fire code official where any of the following apply: (OFC 507.5.1 Exceptions)
 1. Buildings are equipped throughout with an approved automatic fire sprinkler system (the approval of this alternate method of construction shall be accomplished in accordance with the provisions of ORS 455.610(5)).
 2. There are not more than three Group R-3 or Group U occupancies.
23. **COMMERCIAL BUILDINGS – REQUIRED FIRE FLOW:** The minimum fire flow and flow duration for buildings other than one- and two-family dwellings shall be determined in accordance with residual pressure (OFC Table B105.2). The required fire flow for a building shall not exceed the available GPM in the water delivery system at 20 psi.
Note: OFC B106, Limiting Fire-Flow is also enforced, except for the following:
 - In areas where the water system is already developed, the maximum needed fire flow shall be either 3,000 GPM or the available flow in the system at 20 psi, whichever is greater.
 - In new developed areas, the maximum needed fire flow shall be 3,000 GPM at 20 psi.
 - Tualatin Valley Fire & Rescue does not adopt Occupancy Hazards Modifiers in section B105.4-B105.4.1
24. **FIRE FLOW WATER AVAILABILITY:** Applicants shall provide documentation of a fire hydrant flow test or flow test modeling of water availability from the local water purveyor if the project includes a new structure or increase in the floor area of an existing structure. Tests shall be conducted from a fire hydrant within 400 feet for commercial projects, or 600 feet for residential development. Flow tests will be accepted if they were performed within 5 years as long as no adverse modifications have been made to the supply system. Water availability information may not be required to be submitted for every project. (OFC Appendix B)

25. **WATER SUPPLY DURING CONSTRUCTION:** Approved firefighting water supplies shall be installed and operational prior to any combustible construction or storage of combustible materials on the site. (OFC 3312.1)

FIRE HYDRANTS:

26. **FIRE HYDRANTS – COMMERCIAL BUILDINGS:** Where a portion of the building is more than 400 feet from a hydrant on a fire apparatus access road, as measured in an approved route around the exterior of the building, on-site fire hydrants and mains shall be provided. (OFC 507.5.1)
- This distance may be increased to 600 feet for buildings equipped throughout with an approved automatic sprinkler system.
 - The number and distribution of fire hydrants required for commercial structure(s) is based on Table C105.1, following any fire-flow reductions allowed by section B105.3.1. Additional fire hydrants may be required due to spacing and/or section 507.5 of the Oregon Fire Code.
27. **FIRE HYDRANT NUMBER AND DISTRIBUTION:** The minimum number and distribution of fire hydrants available to a building shall not be less than that listed in Table C 105.1. (OFC Appendix C)
28. **FIRE HYDRANT(S) PLACEMENT:** (OFC C104)
- Existing hydrants in the area may be used to meet the required number of hydrants as approved. Hydrants that are up to 600 feet away from the nearest point of a subject building that is protected with fire sprinklers may contribute to the required number of hydrants. (OFC 507.5.1)
 - Hydrants that are separated from the subject building by railroad tracks shall not contribute to the required number of hydrants unless approved by the fire code official.
 - Hydrants that are separated from the subject building by divided highways or freeways shall not contribute to the required number of hydrants. Heavily traveled collector streets may be considered when approved by the fire code official.
 - Hydrants that are accessible only by a bridge shall be acceptable to contribute to the required number of hydrants only if approved by the fire code official.
29. **PRIVATE FIRE HYDRANT IDENTIFICATION:** Private fire hydrants shall be painted red in color. Exception: Private fire hydrants within the City of Tualatin shall be yellow in color. (OFC 507)
30. **FIRE HYDRANT DISTANCE FROM AN ACCESS ROAD:** Fire hydrants shall be located not more than 15 feet from an approved fire apparatus access roadway unless approved by the fire code official. (OFC C102.1)
31. **REFLECTIVE HYDRANT MARKERS:** Fire hydrant locations shall be identified by the installation of blue reflective markers. They shall be located adjacent and to the side of the center line of the access roadway that the fire hydrant is located on. In the case that there is no center line, then assume a center line and place the reflectors accordingly. (OFC 507)
32. **PHYSICAL PROTECTION:** Where fire hydrants are subject to impact by a motor vehicle, guard posts, bollards or other approved means of protection shall be provided. (OFC 507.5.6 & OFC 312)
33. **CLEAR SPACE AROUND FIRE HYDRANTS:** A 3 foot clear space shall be provided around the circumference of fire hydrants. (OFC 507.5.5)
34. **FIRE DEPARTMENT CONNECTION (FDC) LOCATIONS:** FDCs shall be located within 100 feet of a fire hydrant (or as approved). Hydrants and FDC's shall be located on the same side of the fire apparatus access roadway or drive aisle, fully visible, and recognizable from the street or nearest point of the fire department vehicle access or as otherwise approved. (OFC 912.2.1 & NFPA 13)
- Fire department connections (FDCs) shall normally be located remotely and outside of the fall-line of the building when required. FDCs may be mounted on the building they serve, when approved.

- FDCs shall be plumbed on the system side of the check valve when sprinklers are served by underground lines also serving private fire hydrants.

BUILDING ACCESS AND FIRE SERVICE FEATURES

35. **EMERGENCY RESPONDER RADIO COVERAGE:** In new buildings where the design reduces the level of radio coverage for public safety communications systems below minimum performance levels, a distributed antenna system, signal booster, or other method approved by TVF&R and Washington County Consolidated Communications Agency shall be provided. (OSSC 915.1; OFC 510.1)
- a. Emergency responder radio system testing and/or system installation is required for this building. Please contact me (using my contact info below) for further information including an alternate means of compliance that is available. If the alternate method is preferred, it must be requested from TVF&R prior to issuance of building permit.
36. **KNOX BOX:** A Knox Box for building access may be required for structures and gates. See Appendix C for further information and detail on required installations. Order via www.tvfr.com or contact TVF&R for assistance and instructions regarding installation and placement. (OFC 506.1)
37. **UTILITY IDENTIFICATION:** Rooms containing controls to fire suppression and detection equipment shall be identified as "Fire Control Room." Signage shall have letters with a minimum of 4 inches high with a minimum stroke width of 1/2 inch, and be plainly legible, and contrast with its background. (OFC 509.1)
38. **PREMISES IDENTIFICATION:** New and existing buildings shall have approved address numbers; building numbers or approved building identification placed in a position that is plainly legible and visible from the street or road fronting the property, including monument signs. These numbers shall contrast with their background. Numbers shall be a minimum of 4 inches high with a minimum stroke width of 1/2 inch. (OFC 505.1)

If you have questions or need further clarification, please feel free to contact me at (503) 649-8577.

Sincerely,



Ty Darby
Deputy Fire Marshal

Cc: file

M E M O R A N D U M

Date: December 30, 2016

To: Erin Engman, Assistant Planner, City of Tualatin

From: Jackie Sue Humphreys, Clean Water Services (the District)

Subject: Meridian Park Patient Care Expansion, AR-16-0009, 21E19C 01200, 01300, 01400, 01500, 01600

Please include the following comments when writing your conditions of approval:

PRIOR TO ANY WORK ON THE SITE

A Clean Water Services (the District) Storm Water Connection Permit Authorization must be obtained. Application for the District's Permit Authorization must be in accordance with the requirements of the Design and Construction Standards, Resolution and Order No. 07-20, (or current R&O in effect at time of Engineering plan submittal), and is to include:

- a. Detailed plans prepared in accordance with Chapter 2, Section 2.04.
- b. Detailed grading and erosion control plan. An Erosion Control Permit will be required. Area of Disturbance must be clearly identified on submitted construction plans. If site area and any offsite improvements required for this development exceed one-acre of disturbance, project will require a 1200-CN Erosion Control Permit. If site area and any offsite improvements required for this development exceed five-acres of disturbance, project will require a 1200-C Erosion Control Permit.
- c. Detailed plans showing the development having direct access by gravity to public storm and sanitary sewer.
- d. Provisions for water quality in accordance with the requirements of the above named design standards. Water Quality is required for all new development and redevelopment areas per R&O 07-20, Section 4.05.5, Table 4-1. Access shall be provided for maintenance of facility per R&O 07-20, Section 4.02.4.

- e. If use of an existing offsite or regional Water Quality Facility is proposed, it must be clearly identified on plans, showing its location, condition, capacity to treat this site and, any additional improvements and/or upgrades that may be needed to utilize that facility.
- f. If private lot LIDA systems proposed, must comply with the current CWS Design and Construction Standards. A private maintenance agreement, for the proposed private lot LIDA systems, needs to be provided to the City for review and acceptance.
- g. Show all existing and proposed easements on plans. Any required storm sewer, sanitary sewer, and water quality related easements must be granted to the City.
- h. Application may require additional permitting and plan review from the District's Source Control Program. For any questions or additional information, please contact Source Control at (503) 681-5175.
- i. Site contains a "Sensitive Area." Applicant shall comply with the conditions as set forth in the Service Provider Letter No. 16-001957, dated August 15, 2016.
- j. Clean Water Services shall require an easement over the Vegetated Corridor conveying storm and surface water management to Clean Water Services that would prevent the owner of the Vegetated Corridor from activities and uses inconsistent with the purpose of the corridor and any easements therein.
- k. Detailed plans showing the sensitive area and corridor delineated, along with restoration and enhancement of the corridor.
- l. If there is any activity within the sensitive area, the applicant shall gain authorization for the project from the Oregon Department of State Lands (DSL) and US Army Corps of Engineers (USACE). The applicant shall provide Clean Water Services or its designee (appropriate city) with copies of all DSL and USACE project authorization permits.
- m. Any proposed offsite construction activities will require an update or amendment to the current Service Provider Letter for this project.

CONCLUSION

This Land Use Review does not constitute the District's approval of storm or sanitary sewer compliance to the NPDES permit held by the District. The District, prior to issuance of any connection permits, must approve final construction plans and drainage calculations.



M. BARBARA CARTMILL
DIRECTOR

DEPARTMENT OF TRANSPORTATION AND DEVELOPMENT

Memorandum

DEVELOPMENT SERVICES BUILDING

150 BEAVERCREEK ROAD | OREGON CITY, OR 97045

TO: Erin Engman, Assistant Planner

FROM: Development Engineering, Robert Hixson *Robert Hixson*

DATE: December 21, 2016

RE: AR-16-0009, Legacy Meridian Park Patient Care Expansion
T2S., R1E., Section 19C, Tax Lots 1200, 1300, 1400, 1500, and 1600

Development Engineering staff have visited the site and reviewed the submitted materials. We have the following comments:

Facts and Findings:

1. The applicant has proposed the expansion of the Legacy Meridian Park Hospital facility. A large approximately 163,000 square foot expansion is proposed which is an approximately 30% increase in floor area. The existing size of the hospital facilities is approximately 506,000 square feet. A traffic impact study indicates that the expansion would result in an approximately 30% increase in traffic associated with the site. The estimated cost of the improvements is \$54,000,000.00.
2. The subject property has frontage along Borland Road and has two existing access points intersecting with Borland Road. Borland Road is under the jurisdiction of Clackamas County and is classified as a minor arterial. Since Borland Road is under County jurisdiction, frontage improvements and access are required to comply with County standards. In addition, onsite issues associated with access which may impact the operations and safety of Borland Road are also of concern to Clackamas County.
3. A pre-application meeting to discuss the proposed expansion occurred on March 28, 2016, and the County identified issues, including but not necessarily limited to, the short throats of the accesses and the need to improve the Borland Road frontage with the addition of a bike lane and frontage improvements consistent with County standards.
4. For this minor arterial improvements would include, but would not necessarily be limited to, up to a half-street improvement including one-half of a 14-foot wide center turn lane, a 12-foot wide west bound travel lane, a six-foot wide west bound bike lane, curb, or curb and gutter where curb line slopes are less than one percent, minimum five-foot wide landscape strip with street trees, two driveway approaches in accordance with Roadway Standards Drawing D650, minimum seven foot wide sidewalk and an offsite pavement taper.
5. In regards to the bike lane requirement along Borland Road, the traffic impact analysis finds that as properties develop bicycle facilities will be provided along

Borland Road with frontage improvements, as appropriate. Clackamas County Development Engineering staff concurs with this conclusion.

6. Based on the proposed significant increase in traffic associated with the proposed expansion of onsite facilities, Clackamas County finds that a bike lane and frontage improvements are now warranted and appropriate. In addition onsite improvements associated with the two driveway throats will be required to help minimize conflicts at the intersections of the throats and onsite loop road which could have adverse safety and operational impacts on Borland Road traffic.
7. Borland Road frontage improvements have not been proposed by the applicant and potential conflicts associated with the short onsite driveway throats and the onsite loop road are proposed to be addressed with onsite signing advising drivers to not block the onsite intersections of the throats and loop road. This proposed signing indicates potential problems with the short throats and modifications beyond signing will be recommended to address this concern.
8. With road widening to accommodate a six-foot wide bike lane and construction of a separated sidewalk, the existing driveway throats will be shortened by over 10 feet and Roadway Standards section 330.1 d requires minimum 20-foot long throats measured from the back of the sidewalk. Considering the amount of traffic associated with the existing and expanded facility Clackamas County recommends lengthening the throats to minimum lengths of 50 feet.
9. Borland Road right-of-way needs will be established by survey data provided by the applicant. The resulting northerly right-of-way limit on the applicant's side of Borland Road shall be located a minimum of six-inches behind the back of the separated sidewalk. Any right-of-way needed in addition to the existing right-of-way shall be dedicated by the applicant.
10. Behind the resulting northerly right-of-way line the applicant shall grant a minimum eight-foot wide sign and public utility easement.
11. The applicant shall provide and maintain adequate intersection sight distances and adequate stopping sight distances for the two site driveway approach intersections with Borland Road in accordance with Roadway Standards section 240.
12. Applicant shall comply with County Roadway Standards clear zone requirements in accordance with Roadway Standards section 245.
13. Applicant shall obtain a Development Permit from Clackamas County prior to the initiation of any construction activities in or adjacent to Borland Road right-of-way.
14. Engineering staff concur with the capacity evaluations of the two site driveway approach intersections with Borland Road which find v/c ratios will not exceed Clackamas County standards.

15. The use of public rights-of-way for construction vehicle and materials staging is not authorized by the Roadway Standards and poses a potentially deleterious effect of the proposed use, because it contributes to congestion, reduces sight distance, and occupies shoulders intended for emergencies and other purposes. To protect the public from such effects, the applicant shall be required to submit a construction vehicle management plan for review and approval by the County DTD, Construction and Development Section, before the County issues a Development Permit. This may be accomplished with adequate notes on the plans indicating that no material or vehicle staging will occur within the right-of-way.

Preface to recommended conditions of approval:

The following items are project requirements from the Department of Transportation and Development's Development Engineering Division. These conditions of approval are not intended to include every engineering requirement necessary for the successful completion of this project, but are provided to illustrate to the applicant specific details regarding the required improvements that may prove helpful in determining the cost and scope of the project. These conditions are based upon the requirements detailed in the County's Comprehensive Plan (Comp Plan), the County's Zoning and Development Ordinance (ZDO) and the County's Site Development and Roadway Construction Standards (Roadway Standards). Additional requirements, beyond those stated in the conditions of approval, may be required. The applicant may discuss the requirements of the project with staff at any time.

The requirements specifically required by the Comp Plan and the ZDO cannot be modified by the Development Engineering Division. However, the requirements detailed in these conditions of approval, derived from the Roadway Standards, are based upon nationally accepted standards and engineering judgment and may be modified pursuant to Section 170 of the Roadway Standards. The applicant is required to provide sufficient justification to staff in the request. Staff shall determine if a modification is warranted.

Development Engineering recommended conditions of approval:

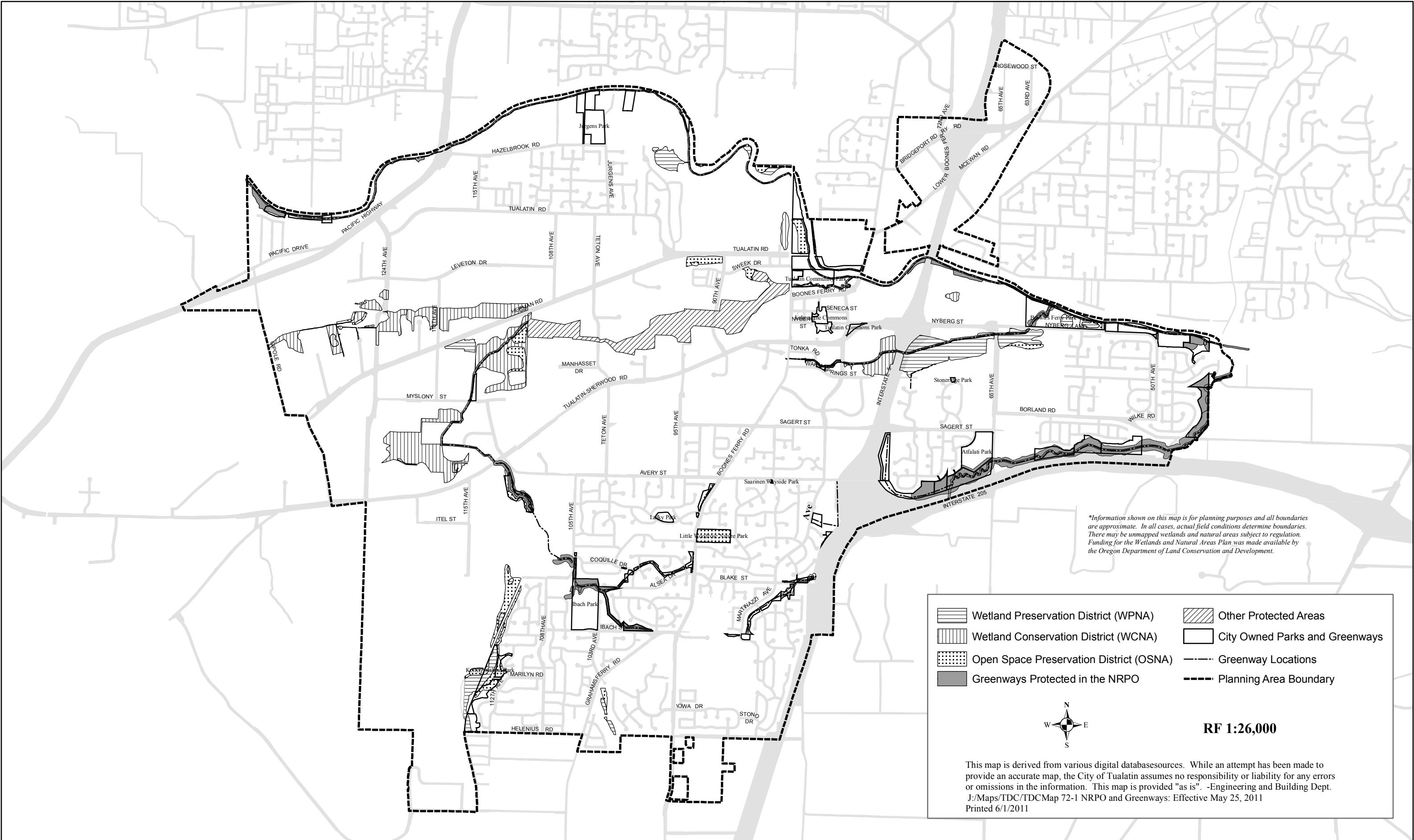
- 1) All frontage improvements in, or adjacent to Clackamas County right-of-way, shall be in compliance with *Clackamas County Roadway Standards*.
- 2) The applicant shall obtain a Development Permit from Clackamas County Department of Transportation and Development prior to the initiation of any construction activities associated with the project.
- 3) The applicant shall verify by a professional survey that sufficient right-of-way width exists along the entire site frontage, on the northerly side of Borland Road, with the northerly right-of-way line located a minimum of six-inches behind the new separated sidewalk, or shall dedicate additional right-of-way as necessary to provide it. Contact Deana Mulder for the specifics regarding exhibits to be included with submittals.

- 4) The applicant shall grant an eight-foot wide easement for signs and public utilities along the entire Borland Road site frontage. Contact Deana Mulder for the specifics regarding exhibits to be included with submittals.
- 5) The applicant shall design and construct improvements along the entire site frontage of Borland Road. These improvements shall consist of:
 - a) Up to a half-street improvement. Structural section for Borland Road improvements shall consist of seven and one-half inches of Level 2 Hot Mix Asphalt Concrete (HMAC), Performance Grade (PG) 70-22, ½" dense top lift over ¾" dense lower lift or lifts, placed in lifts no more than two inches in thickness, over four inches of ¾"-0 aggregate leveling course, over 10 inches of 1-1/2"-0 aggregate base course, over geotextile fabric, over compacted undisturbed subgrade.
 - b) Standard curb, or curb and gutter if curb line slope is less than one percent, and pavement widening with the face of the new curb located 25 feet from the centerline of the Borland Road right-of-way or an alternative centerline determined by Engineering staff. Centerline of the right-of-way shall be established by a registered survey.
 - c) Drainage facilities in conformance with *ZDO* section 1008 and *Clackamas County Roadway Standards* Chapter four.
 - d) A minimum seven-foot wide unobstructed sidewalk behind a minimum five-foot wide landscape strip with street trees. The applicant shall relocate mailboxes, fire hydrants, utility poles, etc, when they are located within the limits of the sidewalk or construct an eyebrow so that the full width of the sidewalk is provided around the obstruction. Mailboxes shall be relocated or replaced in accordance with standards established by the local Post Office. Additional easement, as necessary, shall be granted to provide for any sidewalk eyebrows. Sidewalks at transit stops shall be a minimum of eight feet in width and curb tight.
 - e) Two minimum 28-foot wide (throat width) driveway approaches in conformance with *Roadway Standards* Drawing D650 at the existing Borland Road approach locations. Minimum throat lengths, measured from the backs of the new separated sidewalk, shall be 50 feet.
 - f) Appropriate pavement tapers. Tapers shall be in accordance with *Roadway Standards* Subsection 250.6.4 for transitions.
- 6) All curbs shall typically be type "C", or curb and gutter if curb line slope is less than one percent, if they carry, direct or channel surface water.
- 7) The applicant shall design and construct curb/sidewalk ramps at the easterly driveway approach which is opposite a new street associated with the new subdivision to the south.

- 8) The applicant shall provide a copy of the Clean Water Services approved drainage study, surface water management plan, and Engineer's detention calculations to DTD Engineering, Deana Mulder.
- 9) The applicant shall provide and maintain adequate intersection sight distances and adequate stopping sight distances at the driveway approach intersections with Borland Road in accordance with Roadway Standards section 240 requirements. Adequate intersection sight distance for drivers turning left into the site shall also be provided and maintained. In addition, no plantings at maturity, retaining walls, embankments, fences or any other objects shall be allowed to obstruct minimum sight distance requirements. **Plans submitted in anticipation of issuance of a Development Permit shall include an exhibit illustrating sight lines for the site driveways intersections with Borland Road to insure sight lines are not obstructed by street trees or any other objects along the subject property frontage.**
- 10) Applicant shall comply with County Roadway Standards clear zone requirements in accordance with Roadway Standards section 245 along the Borland Road frontage.
- 11) The applicant shall install and maintain 30-inch "STOP" signs, behind the separated sidewalk, with the bottom of the signs positioned seven feet above the surface of the sidewalk, at the driveway approach intersections with Borland Road. (Manual on Uniform Traffic Control Devices)
- 12) All traffic control devices on private property, located where private driveways intersect County facilities shall be installed and maintained by the applicant, and shall meet standards set forth in the *Manual on Uniform Traffic Control Devices* and relevant Oregon supplements.
- 13) Prior to the issuance of a building permit, the applicant shall submit to Clackamas County Engineering Office:
 - a) Written approval from the local Fire District for the planned access, circulation, fire lanes and water source supply. The approval shall be in the form of site and utility plans stamped and signed by the Fire Marshal.
 - b) Written approval from Clean Water Services for surface water management facilities, surface water detention facilities, and erosion control measures.
 - c) A set of street improvement and site frontage improvement construction plans, including a striping and signing plan for Borland Road and the Borland Road frontage, for review, in conformance with *Clackamas County Roadway Standards* Section 140, to Deana Mulder in Clackamas County's Engineering Office and obtain written approval, in the form of a Development Permit.
 - i) The permit will be for road, driveway approach, driveway throat, curb, sidewalk, drainage, and other road and frontage improvements.
 - ii) The fee for the Development Permit will be calculated in accordance with the current fee schedule.

- iii) The applicant shall have an Engineer, registered in the state of Oregon, design and stamp construction plans for all required improvements, or provide alternative plans acceptable to the Engineering Division.
- 14) Before the County issues a Development Permit, the applicant shall submit a construction vehicle management and staging plan for review and approval by the County DTD, Construction and Development Section, if the public road right-of-way is planned to be used for construction staging. That plan shall show that construction vehicles and materials will not be staged or queued-up on public streets and shoulders without specific authority from DTD. If the public road right-of-way is not planned to be used for construction staging, a note shall be added to the construction drawings stating that the public road right-of-way will not be used for construction staging.
- 15) Prior to certificate of occupancy, the applicant shall provide an Engineer's cost estimate to Clackamas County Engineering for any unfinished improvements required by conditions of approval. The estimate shall be submitted for review and approval of quantities of asphalt concrete, aggregates, curbs, sidewalks and any other required improvements and associated construction costs.

Map 72-1: Natural Resources Protection Overlay District (NRPO) and Greenway Locations





Legacy Health Meridian Park Patient Care Expansion

ARCHITECTURAL REVIEW BOARD HEARING
FEBRUARY 6, 2017



HEARING AGENDA

- Staff Presentation
- Applicant Presentation
- Public Comment
- ARB Deliberation and Decision



HEARING PURPOSE

- **Architectural Review:** application for a 163,203 sq ft expansion of the existing Meridian Park Hospital
- Elements of review limited to Architectural Features:
 - Architecture
 - Parking Lot
 - Bike Parking
 - Landscaping
 - Tree Mitigation
 - Lighting
 - Trash Plan and Enclosures



ARCHITECTURAL REVIEW BOARD

- **Why is this proposal subject to ARB review?**
 - ARB reviews and comments on all commercial development over 50,000 sq ft per TDC 73.030
- **Scope of ARB:**
 - The ARB shall make the final decision on this AR proposal. Decision may be appealed to the City Council



REVIEW OF APPLICATION

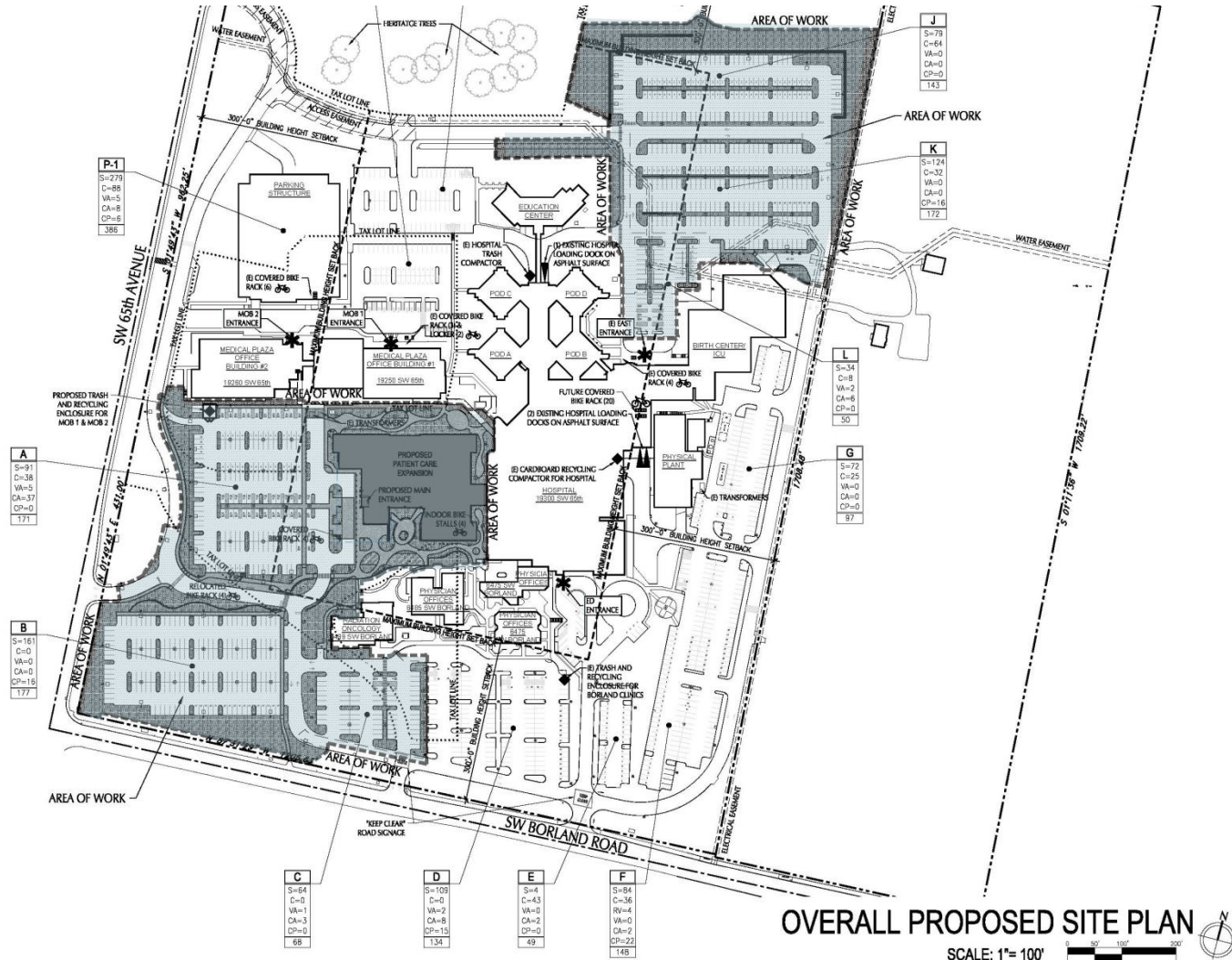
- **Important Dates**
 - Submitted application on July 28, 2016
 - Deemed complete on November 14, 2016
 - 120-day period ends on March 14, 2017
- **Analysis and Findings**
 - Staff finds that the proposed architectural features shown on the site plan are consistent with the Tualatin Development Code, when subject to the recommended conditions of approval



FEBRUARY 6, 2017



PROPOSED SITE PLAN





PROPOSED ELEVATIONS



WEST ELEVATION
1/16" = 1'-0"



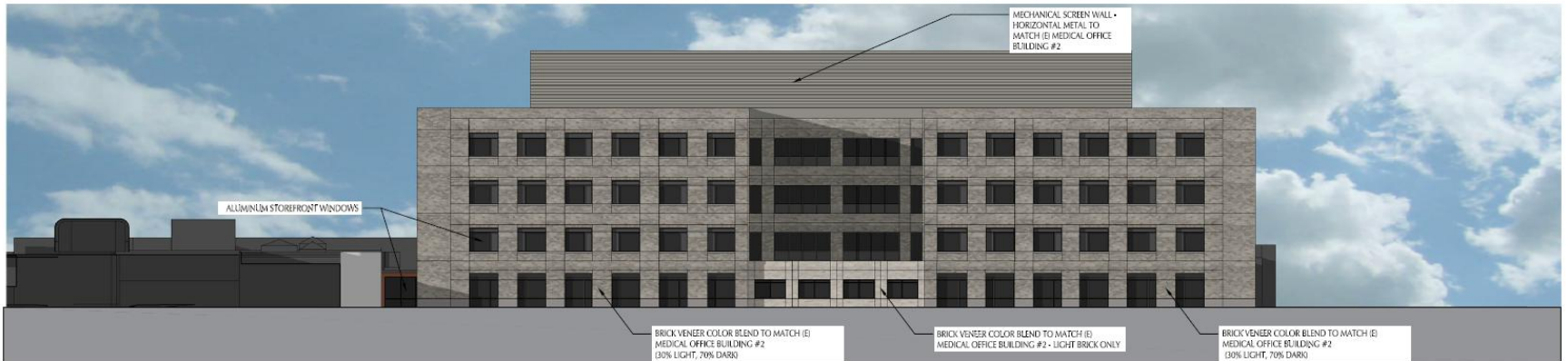
SOUTH ELEVATION
1/16" = 1'-0"



PROPOSED ELEVATIONS



EAST ELEVATION
1/16" = 1'-0"



NORTH ELEVATION
1/16" = 1'-0"



RECOMMENDED CONDITIONS

- **Pedestrian Circulation**
 - Conditions: AR-3, 4, 5, 6
- **Architectural Feature Review Elements:**
 - 6 ft wide walkways
 - Visibly raised walkways through parking area
 - Curb ramps
 - 8 ft wide accessway to connect walkways to abutting Arterial streets

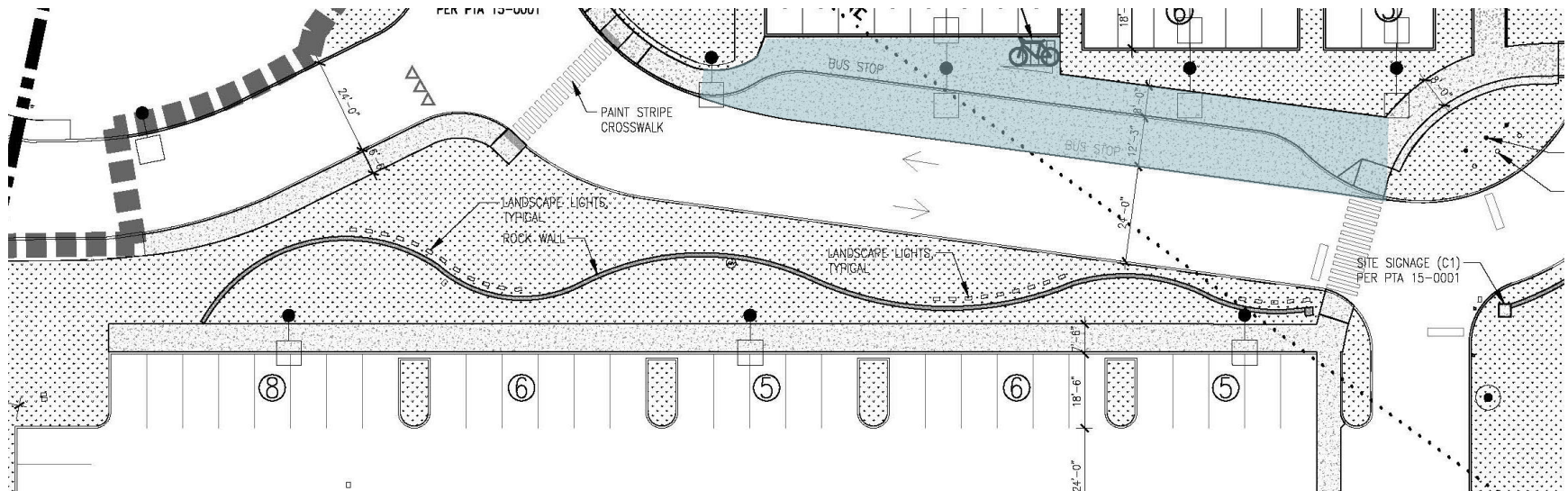


11



RECOMMENDED CONDITIONS

- **Public Transit**
 - Condition: AR-8
- **Architectural Feature Review Elements:**
 - TriMet acceptance of transit pad design



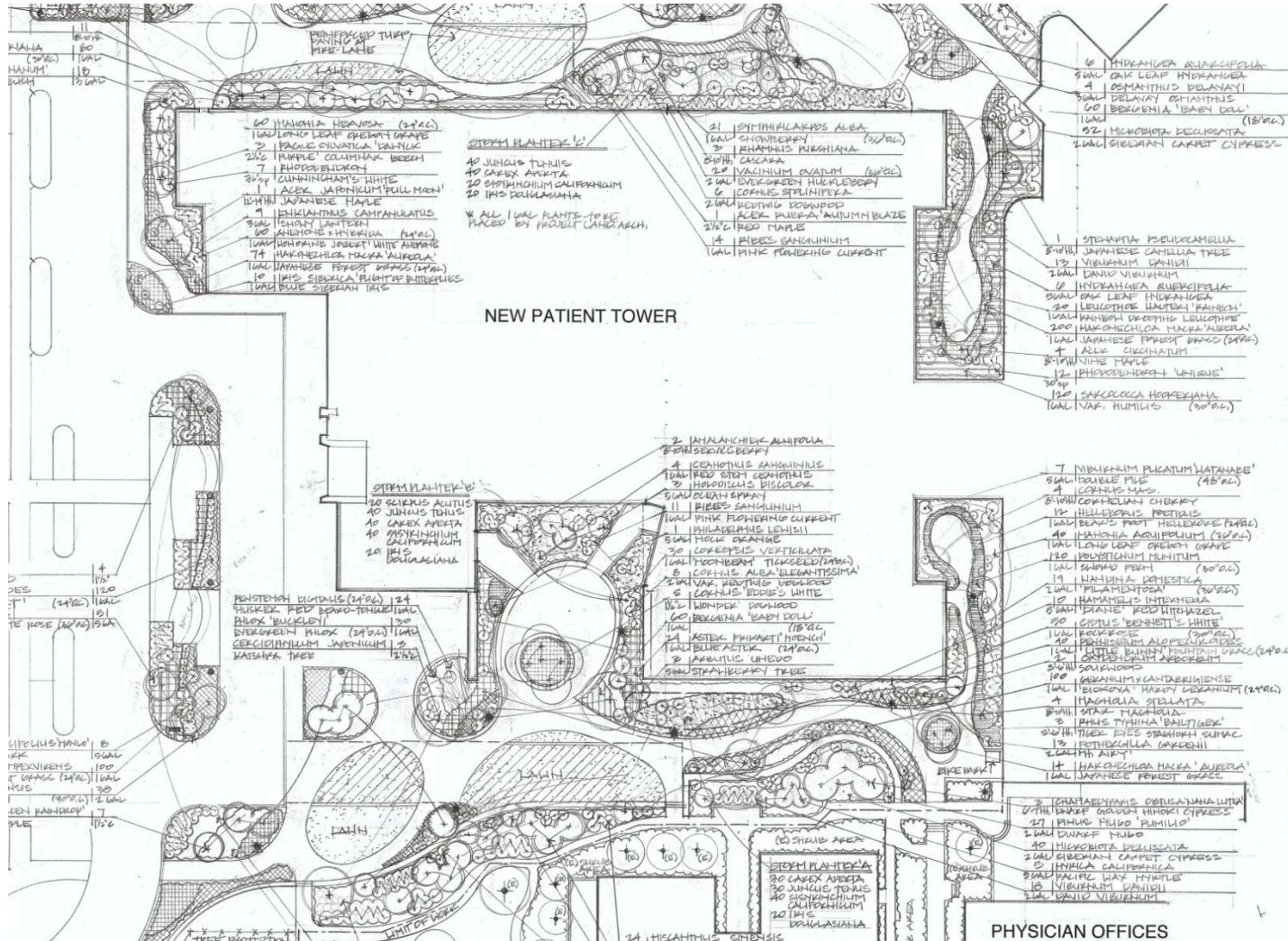


RECOMMENDED CONDITIONS

- **Landscaping**
 - Conditions: AR-10, 11, 12
- **Architectural Feature Review Elements:**
 - Proposed trees to meet minimum size requirements
 - Deciduous: 1.5” caliper
 - Coniferous: 5’ in height



REVIEW ELEMENTS: LANDSCAPING



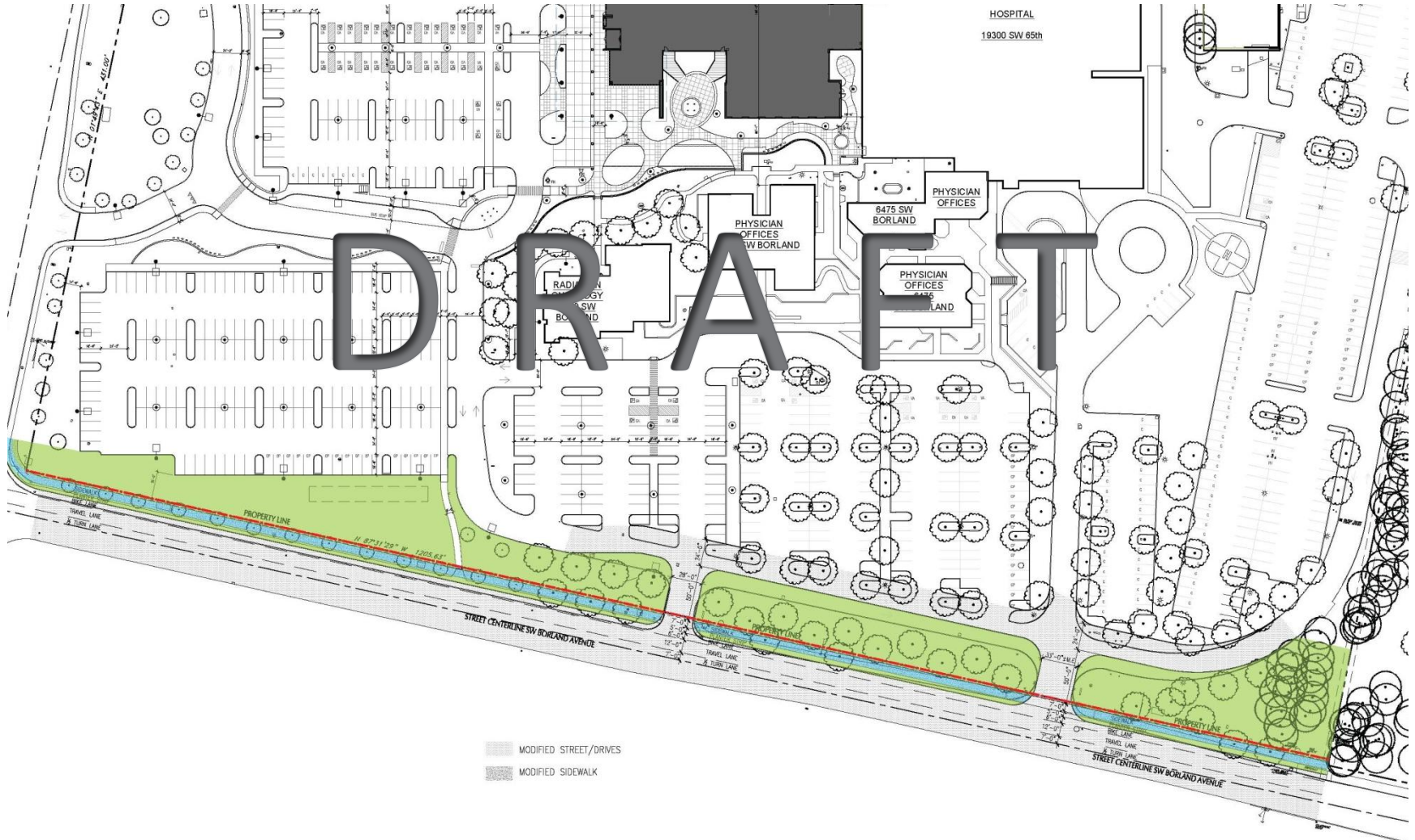


RECOMMENDED CONDITIONS

- **Site Revisions**
 - Condition: AR-13
- **Public Facilities Review Condition:**
 - Borland Road improvements to Clackamas Co. standards
- **Architectural Feature Review Elements:**
 - Potential impacts to: parking lot, landscaping, and tree mitigation



REVIEW ELEMENTS: BORLAND ROAD





RECOMMENDED CONDITIONS

- **Bicycle Parking**
 - Condition: AR-14
- **Architectural Feature Review Elements:**
 - MUTCD signage
- **Architectural Review Board Consideration:**
 - Reduced bike parking requirements



REVIEW ELEMENTS: BIKE PARKING

- **613 bike stalls required by Code**
 - TDC 73.370(1)(n) allows applicant to demonstrate why fewer bike stalls are sufficient
- **Applicant Response:**
 - 27 bicycle stalls are available for staff and patient use
 - Unlikely that patients would arrive by bike
 - 2014 Trimet employee commute survey: 2 trips reported; adjusted to 5
 - Current bike parking used: 7-19%
 - Proposed additional: 32% increase in floor area
 - If commuters tripled: 15 stalls needed
- **Staff recommends approval of reduced bike parking requirement**



RECOMMENDED CONDITIONS

- **Parking Design**
 - Conditions: AR-15, 16
- **Architectural Feature Review Elements:**
 - Wheel stops
 - 24 ft aisle width between parking stall lines



FEBRUARY 6, 2017



RECOMMENDATION OPTIONS

- Approve with staff recommended findings and conditions of approval
- Approve with amended findings and conditions of approval
- Continue the Hearing
- Deny the application



REPUBLIC
SERVICES

10295 SW Ridder Road, Wilsonville, OR 97070
O: 503.404-2135 F: 503.682-9004 republicservices.com

July 8, 2016

Kate Weinert
PKA Architects P.C. AIA

Re: Meridian Park

Dear Kate;

Thank you, for sending me the site plans and enclosure specifications for the changes being planned for Meridian Park Hospital.

My Company: Republic Services of Clackamas & Washington Counties has the franchise agreement to service this area with the City of Tualatin. We provide complete Commercial waste removal services and recycling services as needed on a weekly basis for this location.

I do not see any interference of where the enclosure is being planned that will affect your garbage & recycling services. The size of the enclosure will be fine for my containers. Please remember to allow the gates be in an open locked position over 120 degrees when we are servicing. The compactors will continue to be serviced as usual.

Thanks for your help and concerns for our services prior to this project being developed.

Sincerely,

Frank J. Loneragan
Operations Manager
Republic Services Inc.

SITE INFORMATION:

PLANNING DISTRICT: _____

SITE SQUARE FOOTAGE: _____

DEVELOPMENT AREA SQUARE FOOTAGE: _____

LANDSCAPE SQUARE FOOTAGE: _____

PARKING LOT LANDSCAPE SQUARE FOOTAGE: _____

PARKING SQUARE FOOTAGE: _____

NUMBER OF PARKING SPACES: _____

STANDARD: _____

COMPACT: _____

ADA: _____

CARPOOL: _____

VANPOOL: _____

BUILDING SQUARE FOOTAGE: _____

LOADING DOORS: _____



WASTE HAULING PLAN - PROPOSED

SCALE: 1"= 100'

LECAOY HEALTH MERIDIAN PARK PATIENT CARE EXPANSION

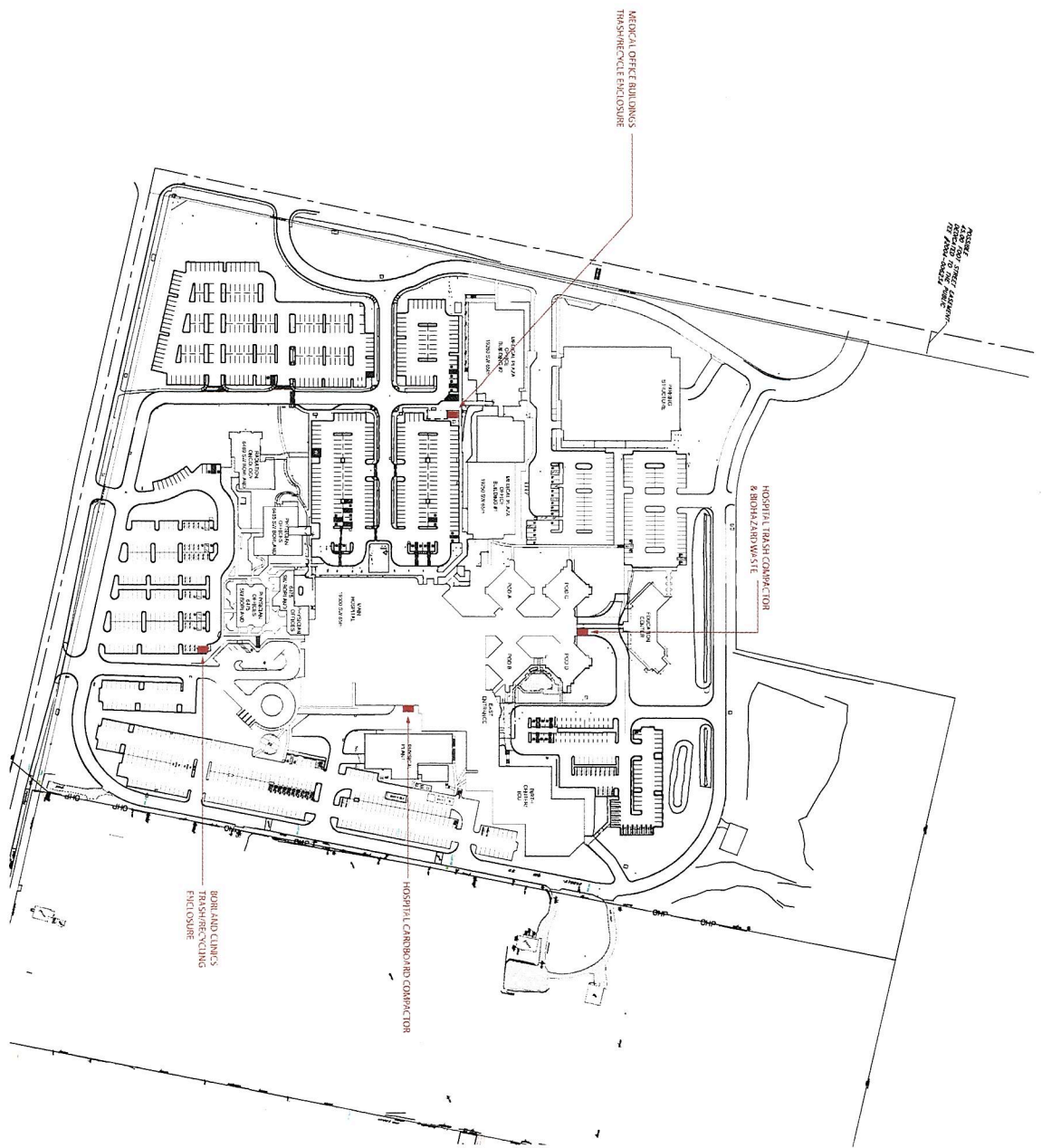


79 Design 7/5/16

78 Jorgensen 7/8/16

WASTE HAULING PLAN - EXISTING

SCALE: 1"= 100'





December 13, 2016

City of Tualatin
Erin Engman-Assistant Planner
18880 SW Martinazzi Rd.
Tualatin, OR 97062

Re: AR16-0009 , 19300 SW 65th Avenue, Tualatin, OR 97062
Tax Lot I.D: 21E 19C 01200, 01300, 01400, 01500, and 01600

Dear Erin,

Thank you for the opportunity to review the proposed site plan surrounding the above named development project. Tualatin Valley Fire & Rescue endorses this proposal predicated on the following criteria and conditions of approval:

FIRE APPARATUS ACCESS:

- FIRE APPARATUS ACCESS ROAD DISTANCE FROM BUILDINGS AND FACILITIES:** Access roads shall be within 150 feet of all portions of the exterior wall of the first story of the building as measured by an approved route around the exterior of the building or facility. An approved turnaround is required if the remaining distance to an approved intersecting roadway, as measured along the fire apparatus access road, is greater than 150 feet. (OFC 503.1.1))
- DEAD END ROADS AND TURNAROUNDS:** Dead end fire apparatus access roads in excess of 150 feet in length shall be provided with an approved turnaround. Diagrams of approved turnarounds are shown below: (OFC 503.2.5 & D103.1)
- ADDITIONAL ACCESS ROADS – COMMERCIAL/INDUSTRIAL HEIGHT:** Buildings exceeding 30 feet in height or three stories in height shall have at least two separate means of fire apparatus access. (D104.1)
- ADDITIONAL ACCESS ROADS – COMMERCIAL/INDUSTRIAL SQUARE FOOTAGE:** Buildings or facilities having a gross building area of more than 62,000 square feet shall have at least two approved separate means of fire apparatus access. Exception: Projects having a gross building area of up to 124,000 square feet that have a single approved fire apparatus access road when all buildings are equipped throughout with approved automatic sprinkler systems. (OFC D104.2)
- AERIAL FIRE APPARATUS ROADS:** Buildings with a vertical distance between the grade plane and the highest roof surface that exceeds 30 feet in height shall be provided with a fire apparatus access road constructed for use by aerial apparatus with an unobstructed driving surface width of not less than 26 feet. For the purposes of this section, the highest roof surface shall be determined by measurement to the eave of a pitched roof, the intersection of the roof to the exterior wall, or the top of the parapet walls, whichever is greater. Any portion of the building may be used for this measurement, provided that it is accessible to firefighters and is capable of supporting ground ladder placement. (OFC D105.1, D105.2)

6. **AERIAL APPARATUS OPERATIONS:** At least one of the required aerial access routes shall be located within a minimum of 15 feet and a maximum of 30 feet from the building, and shall be positioned parallel to one entire side of the building. The side of the building on which the aerial access road is positioned shall be approved by the fire code official. Overhead utility and power lines shall not be located over the aerial access road or between the aerial access road and the building. (D105.3, D105.4)
7. **MULTIPLE ACCESS ROADS SEPARATION:** Where two access roads are required, they shall be placed a distance apart equal to not less than one half of the length of the maximum overall diagonal dimension of the area to be served (as identified by the Fire Code Official), measured in a straight line between accesses. (OFC D104.3) Exception: Buildings equipped throughout with an approved automatic fire sprinkler system (the approval of this alternate method of construction shall be accomplished in accordance with the provisions of ORS 455.610(5).
8. **FIRE APPARATUS ACCESS ROAD WIDTH AND VERTICAL CLEARANCE:** Fire apparatus access roads shall have an unobstructed driving surface width of not less than 20 feet (26 feet adjacent to fire hydrants (OFC D103.1)) and an unobstructed vertical clearance of not less than 13 feet 6 inches. The fire district will approve access roads of 12 feet for up to three dwelling units and accessory buildings. (OFC 503.2.1 & D103.1)
9. **NO PARKING SIGNS:** Where fire apparatus roadways are not of sufficient width to accommodate parked vehicles and 20 feet of unobstructed driving surface, "No Parking" signs shall be installed on one or both sides of the roadway and in turnarounds as needed. Signs shall read "NO PARKING - FIRE LANE" and shall be installed with a clear space above grade level of 7 feet. Signs shall be 12 inches wide by 18 inches high and shall have red letters on a white reflective background. (OFC D103.6)
10. **NO PARKING:** Parking on emergency access roads shall be as follows (OFC D103.6.1-2):
 1. 20-26 feet road width – no parking on either side of roadway
 2. 26-32 feet road width – parking is allowed on one side
 3. Greater than 32 feet road width – parking is not restricted
11. **PAINTED CURBS:** Where required, fire apparatus access roadway curbs shall be painted red (or as approved) and marked "NO PARKING FIRE LANE" at 25 foot intervals. Lettering shall have a stroke of not less than one inch wide by six inches high. Lettering shall be white on red background (or as approved). (OFC 503.3)
12. **FIRE APPARATUS ACCESS ROADS WITH FIRE HYDRANTS:** Where a fire hydrant is located on a fire apparatus access road, the minimum road width shall be 26 feet and shall extend 20 feet before and after the point of the hydrant. (OFC D103.1)
13. **SURFACE AND LOAD CAPACITIES:** Fire apparatus access roads shall be of an all-weather surface that is easily distinguishable from the surrounding area and is capable of supporting not less than 12,500 pounds point load (wheel load) and 75,000 pounds live load (gross vehicle weight). Documentation from a registered engineer that the final construction is in accordance with approved plans or the requirements of the Fire Code may be requested. (OFC 503.2.3)
14. **TURNING RADIUS:** The inside turning radius and outside turning radius shall not be less than 28 feet and 48 feet respectively, measured from the same center point. (OFC 503.2.4 & D103.3)
15. **ACCESS ROAD GRADE:** Fire apparatus access roadway grades shall not exceed 12%. When fire sprinklers* are installed, a maximum grade of 15% will be allowed.

0-12%	Allowed
13-15%	Special consideration with submission of written Alternate Methods and Materials request. Ex: Automatic fire sprinkler (13-D) system* in lieu of grade.
≥16%	Special consideration on a case by case basis with submission of written Alternate Methods and Materials request Ex: Automatic fire sprinkler (13-D)

system* plus additional engineering controls in lieu of grade.**
--

*The approval of fire sprinklers as an alternate shall be accomplished in accordance with the provisions of ORS 455.610(5) and OAR 918-480-0100 and installed per section 903.3.1.1, 903.3.1.2, or 903.3.1.3 of the Oregon Fire Code (OFC 503.2.7 & D103.2)

** See Forest Dwelling Access section for exceptions.

16. **ANGLE OF APPROACH/GRADE FOR TURNAROUNDS:** Turnarounds shall be as flat as possible and have a maximum of 5% grade with the exception of crowning for water run-off. (OFC 503.2.7 & D103.2)
17. **ANGLE OF APPROACH/GRADE FOR INTERSECTIONS:** Intersections shall be level (maximum 5%) with the exception of crowning for water run-off. (OFC 503.2.7 & D103.2)
18. **AERIAL APPARATUS OPERATING GRADES:** Portions of aerial apparatus roads that will be used for aerial operations shall be as flat as possible. Front to rear and side to side maximum slope shall not exceed 10%.
19. **GATES:** Gates securing fire apparatus roads shall comply with all of the following (OFC D103.5, and 503.6):
 1. Minimum unobstructed width shall be not less than 20 feet (or the required roadway surface width).
 2. Gates serving three or less single-family dwellings shall be a minimum of 12 feet in width.
 3. Gates shall be set back at minimum of 30 feet from the intersecting roadway or as approved.
 4. Electric gates shall be equipped with a means for operation by fire department personnel
 5. Electric automatic gates shall comply with ASTM F 2200 and UL 325.
20. **ACCESS DURING CONSTRUCTION:** Approved fire apparatus access roadways shall be installed and operational prior to any combustible construction or storage of combustible materials on the site. Temporary address signage shall also be provided during construction. (OFC 3309 and 3310.1)
21. **TRAFFIC CALMING DEVICES:** Shall be prohibited on fire access routes unless approved by the Fire Code Official. (OFC 503.4.1).

FIREFIGHTING WATER SUPPLIES:

22. **MUNICIPAL FIREFIGHTING WATER SUPPLY EXCEPTIONS:** The requirements for firefighting water supplies may be modified as approved by the fire code official where any of the following apply: (OFC 507.5.1 Exceptions)
 1. Buildings are equipped throughout with an approved automatic fire sprinkler system (the approval of this alternate method of construction shall be accomplished in accordance with the provisions of ORS 455.610(5)).
 2. There are not more than three Group R-3 or Group U occupancies.
23. **COMMERCIAL BUILDINGS – REQUIRED FIRE FLOW:** The minimum fire flow and flow duration for buildings other than one- and two-family dwellings shall be determined in accordance with residual pressure (OFC Table B105.2). The required fire flow for a building shall not exceed the available GPM in the water delivery system at 20 psi.
Note: OFC B106, Limiting Fire-Flow is also enforced, except for the following:
 - In areas where the water system is already developed, the maximum needed fire flow shall be either 3,000 GPM or the available flow in the system at 20 psi, whichever is greater.
 - In new developed areas, the maximum needed fire flow shall be 3,000 GPM at 20 psi.
 - Tualatin Valley Fire & Rescue does not adopt Occupancy Hazards Modifiers in section B105.4-B105.4.1
24. **FIRE FLOW WATER AVAILABILITY:** Applicants shall provide documentation of a fire hydrant flow test or flow test modeling of water availability from the local water purveyor if the project includes a new structure or increase in the floor area of an existing structure. Tests shall be conducted from a fire hydrant within 400 feet for commercial projects, or 600 feet for residential development. Flow tests will be accepted if they were performed within 5 years as long as no adverse modifications have been made to the supply system. Water availability information may not be required to be submitted for every project. (OFC Appendix B)

25. **WATER SUPPLY DURING CONSTRUCTION:** Approved firefighting water supplies shall be installed and operational prior to any combustible construction or storage of combustible materials on the site. (OFC 3312.1)

FIRE HYDRANTS:

26. **FIRE HYDRANTS – COMMERCIAL BUILDINGS:** Where a portion of the building is more than 400 feet from a hydrant on a fire apparatus access road, as measured in an approved route around the exterior of the building, on-site fire hydrants and mains shall be provided. (OFC 507.5.1)
- This distance may be increased to 600 feet for buildings equipped throughout with an approved automatic sprinkler system.
 - The number and distribution of fire hydrants required for commercial structure(s) is based on Table C105.1, following any fire-flow reductions allowed by section B105.3.1. Additional fire hydrants may be required due to spacing and/or section 507.5 of the Oregon Fire Code.
27. **FIRE HYDRANT NUMBER AND DISTRIBUTION:** The minimum number and distribution of fire hydrants available to a building shall not be less than that listed in Table C 105.1. (OFC Appendix C)
28. **FIRE HYDRANT(S) PLACEMENT:** (OFC C104)
- Existing hydrants in the area may be used to meet the required number of hydrants as approved. Hydrants that are up to 600 feet away from the nearest point of a subject building that is protected with fire sprinklers may contribute to the required number of hydrants. (OFC 507.5.1)
 - Hydrants that are separated from the subject building by railroad tracks shall not contribute to the required number of hydrants unless approved by the fire code official.
 - Hydrants that are separated from the subject building by divided highways or freeways shall not contribute to the required number of hydrants. Heavily traveled collector streets may be considered when approved by the fire code official.
 - Hydrants that are accessible only by a bridge shall be acceptable to contribute to the required number of hydrants only if approved by the fire code official.
29. **PRIVATE FIRE HYDRANT IDENTIFICATION:** Private fire hydrants shall be painted red in color. Exception: Private fire hydrants within the City of Tualatin shall be yellow in color. (OFC 507)
30. **FIRE HYDRANT DISTANCE FROM AN ACCESS ROAD:** Fire hydrants shall be located not more than 15 feet from an approved fire apparatus access roadway unless approved by the fire code official. (OFC C102.1)
31. **REFLECTIVE HYDRANT MARKERS:** Fire hydrant locations shall be identified by the installation of blue reflective markers. They shall be located adjacent and to the side of the center line of the access roadway that the fire hydrant is located on. In the case that there is no center line, then assume a center line and place the reflectors accordingly. (OFC 507)
32. **PHYSICAL PROTECTION:** Where fire hydrants are subject to impact by a motor vehicle, guard posts, bollards or other approved means of protection shall be provided. (OFC 507.5.6 & OFC 312)
33. **CLEAR SPACE AROUND FIRE HYDRANTS:** A 3 foot clear space shall be provided around the circumference of fire hydrants. (OFC 507.5.5)
34. **FIRE DEPARTMENT CONNECTION (FDC) LOCATIONS:** FDCs shall be located within 100 feet of a fire hydrant (or as approved). Hydrants and FDC's shall be located on the same side of the fire apparatus access roadway or drive aisle, fully visible, and recognizable from the street or nearest point of the fire department vehicle access or as otherwise approved. (OFC 912.2.1 & NFPA 13)
- Fire department connections (FDCs) shall normally be located remotely and outside of the fall-line of the building when required. FDCs may be mounted on the building they serve, when approved.

- FDCs shall be plumbed on the system side of the check valve when sprinklers are served by underground lines also serving private fire hydrants.

BUILDING ACCESS AND FIRE SERVICE FEATURES

35. **EMERGENCY RESPONDER RADIO COVERAGE:** In new buildings where the design reduces the level of radio coverage for public safety communications systems below minimum performance levels, a distributed antenna system, signal booster, or other method approved by TVF&R and Washington County Consolidated Communications Agency shall be provided. (OSSC 915.1; OFC 510.1)
- a. Emergency responder radio system testing and/or system installation is required for this building. Please contact me (using my contact info below) for further information including an alternate means of compliance that is available. If the alternate method is preferred, it must be requested from TVF&R prior to issuance of building permit.
36. **KNOX BOX:** A Knox Box for building access may be required for structures and gates. See Appendix C for further information and detail on required installations. Order via www.tvfr.com or contact TVF&R for assistance and instructions regarding installation and placement. (OFC 506.1)
37. **UTILITY IDENTIFICATION:** Rooms containing controls to fire suppression and detection equipment shall be identified as "Fire Control Room." Signage shall have letters with a minimum of 4 inches high with a minimum stroke width of 1/2 inch, and be plainly legible, and contrast with its background. (OFC 509.1)
38. **PREMISES IDENTIFICATION:** New and existing buildings shall have approved address numbers; building numbers or approved building identification placed in a position that is plainly legible and visible from the street or road fronting the property, including monument signs. These numbers shall contrast with their background. Numbers shall be a minimum of 4 inches high with a minimum stroke width of 1/2 inch. (OFC 505.1)

If you have questions or need further clarification, please feel free to contact me at (503) 649-8577.

Sincerely,



Ty Darby
Deputy Fire Marshal

Cc: file

M E M O R A N D U M

Date: December 30, 2016

To: Erin Engman, Assistant Planner, City of Tualatin

From: Jackie Sue Humphreys, Clean Water Services (the District)

Subject: Meridian Park Patient Care Expansion, AR-16-0009, 21E19C 01200, 01300, 01400, 01500, 01600

Please include the following comments when writing your conditions of approval:

PRIOR TO ANY WORK ON THE SITE

A Clean Water Services (the District) Storm Water Connection Permit Authorization must be obtained. Application for the District's Permit Authorization must be in accordance with the requirements of the Design and Construction Standards, Resolution and Order No. 07-20, (or current R&O in effect at time of Engineering plan submittal), and is to include:

- a. Detailed plans prepared in accordance with Chapter 2, Section 2.04.
- b. Detailed grading and erosion control plan. An Erosion Control Permit will be required. Area of Disturbance must be clearly identified on submitted construction plans. If site area and any offsite improvements required for this development exceed one-acre of disturbance, project will require a 1200-CN Erosion Control Permit. If site area and any offsite improvements required for this development exceed five-acres of disturbance, project will require a 1200-C Erosion Control Permit.
- c. Detailed plans showing the development having direct access by gravity to public storm and sanitary sewer.
- d. Provisions for water quality in accordance with the requirements of the above named design standards. Water Quality is required for all new development and redevelopment areas per R&O 07-20, Section 4.05.5, Table 4-1. Access shall be provided for maintenance of facility per R&O 07-20, Section 4.02.4.

- e. If use of an existing offsite or regional Water Quality Facility is proposed, it must be clearly identified on plans, showing its location, condition, capacity to treat this site and, any additional improvements and/or upgrades that may be needed to utilize that facility.
- f. If private lot LIDA systems proposed, must comply with the current CWS Design and Construction Standards. A private maintenance agreement, for the proposed private lot LIDA systems, needs to be provided to the City for review and acceptance.
- g. Show all existing and proposed easements on plans. Any required storm sewer, sanitary sewer, and water quality related easements must be granted to the City.
- h. Application may require additional permitting and plan review from the District's Source Control Program. For any questions or additional information, please contact Source Control at (503) 681-5175.
- i. Site contains a "Sensitive Area." Applicant shall comply with the conditions as set forth in the Service Provider Letter No. 16-001957, dated August 15, 2016.
- j. Clean Water Services shall require an easement over the Vegetated Corridor conveying storm and surface water management to Clean Water Services that would prevent the owner of the Vegetated Corridor from activities and uses inconsistent with the purpose of the corridor and any easements therein.
- k. Detailed plans showing the sensitive area and corridor delineated, along with restoration and enhancement of the corridor.
- l. If there is any activity within the sensitive area, the applicant shall gain authorization for the project from the Oregon Department of State Lands (DSL) and US Army Corps of Engineers (USACE). The applicant shall provide Clean Water Services or its designee (appropriate city) with copies of all DSL and USACE project authorization permits.
- m. Any proposed offsite construction activities will require an update or amendment to the current Service Provider Letter for this project.

CONCLUSION

This Land Use Review does not constitute the District's approval of storm or sanitary sewer compliance to the NPDES permit held by the District. The District, prior to issuance of any connection permits, must approve final construction plans and drainage calculations.



M. BARBARA CARTMILL
DIRECTOR

DEPARTMENT OF TRANSPORTATION AND DEVELOPMENT

Memorandum

DEVELOPMENT SERVICES BUILDING

150 BEAVERCREEK ROAD | OREGON CITY, OR 97045

TO: Erin Engman, Assistant Planner

FROM: Development Engineering, Robert Hixson *Robert Hixson*

DATE: December 21, 2016

RE: AR-16-0009, Legacy Meridian Park Patient Care Expansion
T2S., R1E., Section 19C, Tax Lots 1200, 1300, 1400, 1500, and 1600

Development Engineering staff have visited the site and reviewed the submitted materials. We have the following comments:

Facts and Findings:

1. The applicant has proposed the expansion of the Legacy Meridian Park Hospital facility. A large approximately 163,000 square foot expansion is proposed which is an approximately 30% increase in floor area. The existing size of the hospital facilities is approximately 506,000 square feet. A traffic impact study indicates that the expansion would result in an approximately 30% increase in traffic associated with the site. The estimated cost of the improvements is \$54,000,000.00.
2. The subject property has frontage along Borland Road and has two existing access points intersecting with Borland Road. Borland Road is under the jurisdiction of Clackamas County and is classified as a minor arterial. Since Borland Road is under County jurisdiction, frontage improvements and access are required to comply with County standards. In addition, onsite issues associated with access which may impact the operations and safety of Borland Road are also of concern to Clackamas County.
3. A pre-application meeting to discuss the proposed expansion occurred on March 28, 2016, and the County identified issues, including but not necessarily limited to, the short throats of the accesses and the need to improve the Borland Road frontage with the addition of a bike lane and frontage improvements consistent with County standards.
4. For this minor arterial improvements would include, but would not necessarily be limited to, up to a half-street improvement including one-half of a 14-foot wide center turn lane, a 12-foot wide west bound travel lane, a six-foot wide west bound bike lane, curb, or curb and gutter where curb line slopes are less than one percent, minimum five-foot wide landscape strip with street trees, two driveway approaches in accordance with Roadway Standards Drawing D650, minimum seven foot wide sidewalk and an offsite pavement taper.
5. In regards to the bike lane requirement along Borland Road, the traffic impact analysis finds that as properties develop bicycle facilities will be provided along

Borland Road with frontage improvements, as appropriate. Clackamas County Development Engineering staff concurs with this conclusion.

6. Based on the proposed significant increase in traffic associated with the proposed expansion of onsite facilities, Clackamas County finds that a bike lane and frontage improvements are now warranted and appropriate. In addition onsite improvements associated with the two driveway throats will be required to help minimize conflicts at the intersections of the throats and onsite loop road which could have adverse safety and operational impacts on Borland Road traffic.
7. Borland Road frontage improvements have not been proposed by the applicant and potential conflicts associated with the short onsite driveway throats and the onsite loop road are proposed to be addressed with onsite signing advising drivers to not block the onsite intersections of the throats and loop road. This proposed signing indicates potential problems with the short throats and modifications beyond signing will be recommended to address this concern.
8. With road widening to accommodate a six-foot wide bike lane and construction of a separated sidewalk, the existing driveway throats will be shortened by over 10 feet and Roadway Standards section 330.1 d requires minimum 20-foot long throats measured from the back of the sidewalk. Considering the amount of traffic associated with the existing and expanded facility Clackamas County recommends lengthening the throats to minimum lengths of 50 feet.
9. Borland Road right-of-way needs will be established by survey data provided by the applicant. The resulting northerly right-of-way limit on the applicant's side of Borland Road shall be located a minimum of six-inches behind the back of the separated sidewalk. Any right-of-way needed in addition to the existing right-of-way shall be dedicated by the applicant.
10. Behind the resulting northerly right-of-way line the applicant shall grant a minimum eight-foot wide sign and public utility easement.
11. The applicant shall provide and maintain adequate intersection sight distances and adequate stopping sight distances for the two site driveway approach intersections with Borland Road in accordance with Roadway Standards section 240.
12. Applicant shall comply with County Roadway Standards clear zone requirements in accordance with Roadway Standards section 245.
13. Applicant shall obtain a Development Permit from Clackamas County prior to the initiation of any construction activities in or adjacent to Borland Road right-of-way.
14. Engineering staff concur with the capacity evaluations of the two site driveway approach intersections with Borland Road which find v/c ratios will not exceed Clackamas County standards.

15. The use of public rights-of-way for construction vehicle and materials staging is not authorized by the Roadway Standards and poses a potentially deleterious effect of the proposed use, because it contributes to congestion, reduces sight distance, and occupies shoulders intended for emergencies and other purposes. To protect the public from such effects, the applicant shall be required to submit a construction vehicle management plan for review and approval by the County DTD, Construction and Development Section, before the County issues a Development Permit. This may be accomplished with adequate notes on the plans indicating that no material or vehicle staging will occur within the right-of-way.

Preface to recommended conditions of approval:

The following items are project requirements from the Department of Transportation and Development's Development Engineering Division. These conditions of approval are not intended to include every engineering requirement necessary for the successful completion of this project, but are provided to illustrate to the applicant specific details regarding the required improvements that may prove helpful in determining the cost and scope of the project. These conditions are based upon the requirements detailed in the County's Comprehensive Plan (Comp Plan), the County's Zoning and Development Ordinance (ZDO) and the County's Site Development and Roadway Construction Standards (Roadway Standards). Additional requirements, beyond those stated in the conditions of approval, may be required. The applicant may discuss the requirements of the project with staff at any time.

The requirements specifically required by the Comp Plan and the ZDO cannot be modified by the Development Engineering Division. However, the requirements detailed in these conditions of approval, derived from the Roadway Standards, are based upon nationally accepted standards and engineering judgment and may be modified pursuant to Section 170 of the Roadway Standards. The applicant is required to provide sufficient justification to staff in the request. Staff shall determine if a modification is warranted.

Development Engineering recommended conditions of approval:

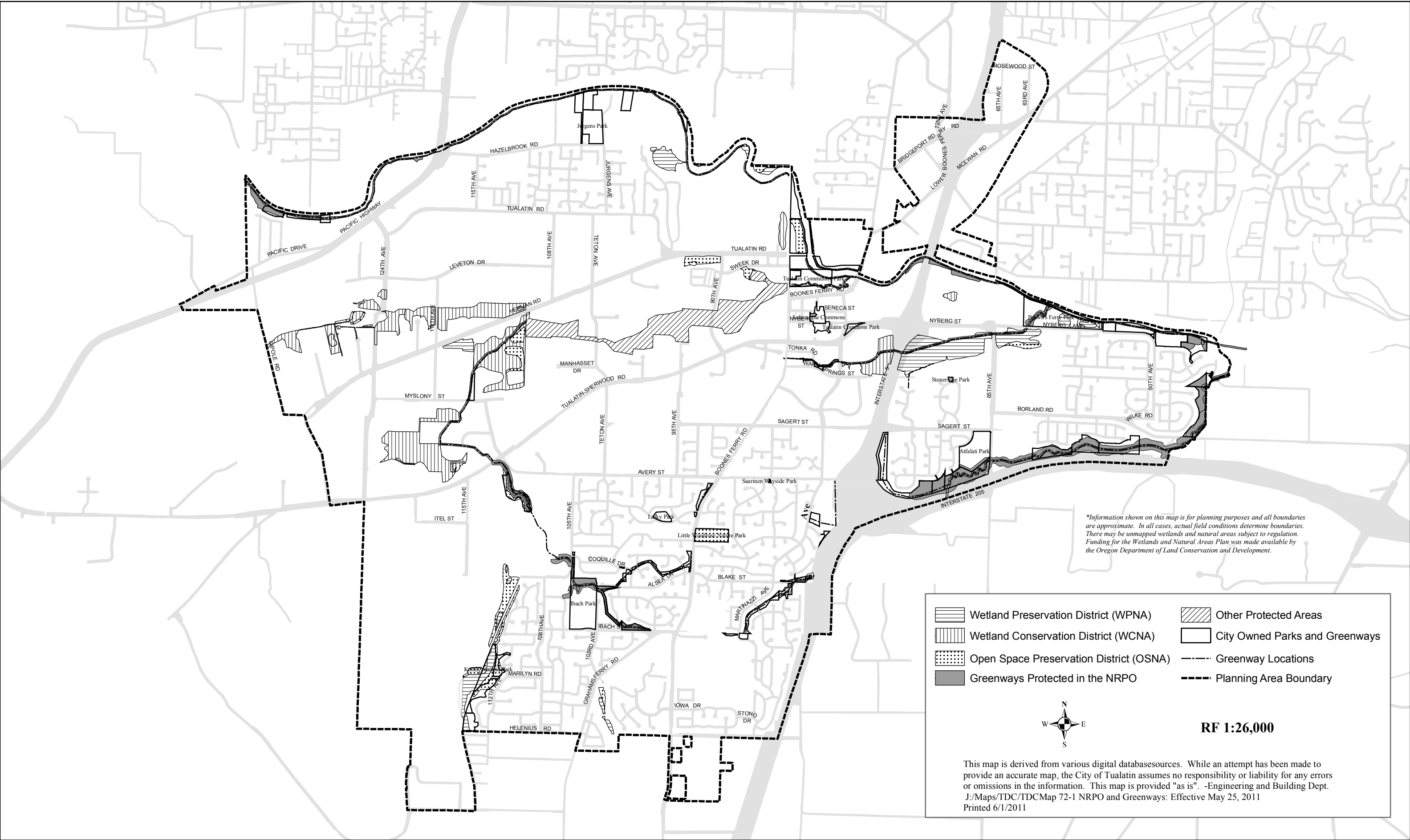
- 1) All frontage improvements in, or adjacent to Clackamas County right-of-way, shall be in compliance with *Clackamas County Roadway Standards*.
- 2) The applicant shall obtain a Development Permit from Clackamas County Department of Transportation and Development prior to the initiation of any construction activities associated with the project.
- 3) The applicant shall verify by a professional survey that sufficient right-of-way width exists along the entire site frontage, on the northerly side of Borland Road, with the northerly right-of-way line located a minimum of six-inches behind the new separated sidewalk, or shall dedicate additional right-of-way as necessary to provide it. Contact Deana Mulder for the specifics regarding exhibits to be included with submittals.

- 4) The applicant shall grant an eight-foot wide easement for signs and public utilities along the entire Borland Road site frontage. Contact Deana Mulder for the specifics regarding exhibits to be included with submittals.
- 5) The applicant shall design and construct improvements along the entire site frontage of Borland Road. These improvements shall consist of:
 - a) Up to a half-street improvement. Structural section for Borland Road improvements shall consist of seven and one-half inches of Level 2 Hot Mix Asphalt Concrete (HMAC), Performance Grade (PG) 70-22, ½" dense top lift over ¾" dense lower lift or lifts, placed in lifts no more than two inches in thickness, over four inches of ¾"-0 aggregate leveling course, over 10 inches of 1-1/2"-0 aggregate base course, over geotextile fabric, over compacted undisturbed subgrade.
 - b) Standard curb, or curb and gutter if curb line slope is less than one percent, and pavement widening with the face of the new curb located 25 feet from the centerline of the Borland Road right-of-way or an alternative centerline determined by Engineering staff. Centerline of the right-of-way shall be established by a registered survey.
 - c) Drainage facilities in conformance with *ZDO* section 1008 and *Clackamas County Roadway Standards* Chapter four.
 - d) A minimum seven-foot wide unobstructed sidewalk behind a minimum five-foot wide landscape strip with street trees. The applicant shall relocate mailboxes, fire hydrants, utility poles, etc, when they are located within the limits of the sidewalk or construct an eyebrow so that the full width of the sidewalk is provided around the obstruction. Mailboxes shall be relocated or replaced in accordance with standards established by the local Post Office. Additional easement, as necessary, shall be granted to provide for any sidewalk eyebrows. Sidewalks at transit stops shall be a minimum of eight feet in width and curb tight.
 - e) Two minimum 28-foot wide (throat width) driveway approaches in conformance with *Roadway Standards* Drawing D650 at the existing Borland Road approach locations. Minimum throat lengths, measured from the backs of the new separated sidewalk, shall be 50 feet.
 - f) Appropriate pavement tapers. Tapers shall be in accordance with *Roadway Standards* Subsection 250.6.4 for transitions.
- 6) All curbs shall typically be type "C", or curb and gutter if curb line slope is less than one percent, if they carry, direct or channel surface water.
- 7) The applicant shall design and construct curb/sidewalk ramps at the easterly driveway approach which is opposite a new street associated with the new subdivision to the south.

- 8) The applicant shall provide a copy of the Clean Water Services approved drainage study, surface water management plan, and Engineer's detention calculations to DTD Engineering, Deana Mulder.
- 9) The applicant shall provide and maintain adequate intersection sight distances and adequate stopping sight distances at the driveway approach intersections with Borland Road in accordance with Roadway Standards section 240 requirements. Adequate intersection sight distance for drivers turning left into the site shall also be provided and maintained. In addition, no plantings at maturity, retaining walls, embankments, fences or any other objects shall be allowed to obstruct minimum sight distance requirements. **Plans submitted in anticipation of issuance of a Development Permit shall include an exhibit illustrating sight lines for the site driveways intersections with Borland Road to insure sight lines are not obstructed by street trees or any other objects along the subject property frontage.**
- 10) Applicant shall comply with County Roadway Standards clear zone requirements in accordance with Roadway Standards section 245 along the Borland Road frontage.
- 11) The applicant shall install and maintain 30-inch "STOP" signs, behind the separated sidewalk, with the bottom of the signs positioned seven feet above the surface of the sidewalk, at the driveway approach intersections with Borland Road. (Manual on Uniform Traffic Control Devices)
- 12) All traffic control devices on private property, located where private driveways intersect County facilities shall be installed and maintained by the applicant, and shall meet standards set forth in the *Manual on Uniform Traffic Control Devices* and relevant Oregon supplements.
- 13) Prior to the issuance of a building permit, the applicant shall submit to Clackamas County Engineering Office:
 - a) Written approval from the local Fire District for the planned access, circulation, fire lanes and water source supply. The approval shall be in the form of site and utility plans stamped and signed by the Fire Marshal.
 - b) Written approval from Clean Water Services for surface water management facilities, surface water detention facilities, and erosion control measures.
 - c) A set of street improvement and site frontage improvement construction plans, including a striping and signing plan for Borland Road and the Borland Road frontage, for review, in conformance with *Clackamas County Roadway Standards* Section 140, to Deana Mulder in Clackamas County's Engineering Office and obtain written approval, in the form of a Development Permit.
 - i) The permit will be for road, driveway approach, driveway throat, curb, sidewalk, drainage, and other road and frontage improvements.
 - ii) The fee for the Development Permit will be calculated in accordance with the current fee schedule.

- iii) The applicant shall have an Engineer, registered in the state of Oregon, design and stamp construction plans for all required improvements, or provide alternative plans acceptable to the Engineering Division.
- 14) Before the County issues a Development Permit, the applicant shall submit a construction vehicle management and staging plan for review and approval by the County DTD, Construction and Development Section, if the public road right-of-way is planned to be used for construction staging. That plan shall show that construction vehicles and materials will not be staged or queued-up on public streets and shoulders without specific authority from DTD. If the public road right-of-way is not planned to be used for construction staging, a note shall be added to the construction drawings stating that the public road right-of-way will not be used for construction staging.
- 15) Prior to certificate of occupancy, the applicant shall provide an Engineer's cost estimate to Clackamas County Engineering for any unfinished improvements required by conditions of approval. The estimate shall be submitted for review and approval of quantities of asphalt concrete, aggregates, curbs, sidewalks and any other required improvements and associated construction costs.

Map 72-1: Natural Resources Protection Overlay District (NRPO) and Greenway Locations





Legacy Health Meridian Park Patient Care Expansion

ARCHITECTURAL REVIEW BOARD HEARING
FEBRUARY 6, 2017



HEARING AGENDA

- Staff Presentation
- Applicant Presentation
- Public Comment
- ARB Deliberation and Decision



HEARING PURPOSE

- **Architectural Review:** application for a 163,203 sq ft expansion of the existing Meridian Park Hospital
- Elements of review limited to Architectural Features:
 - Architecture
 - Parking Lot
 - Bike Parking
 - Landscaping
 - Tree Mitigation
 - Lighting
 - Trash Plan and Enclosures



ARCHITECTURAL REVIEW BOARD

- **Why is this proposal subject to ARB review?**
 - ARB reviews and comments on all commercial development over 50,000 sq ft per TDC 73.030
- **Scope of ARB:**
 - The ARB shall make the final decision on this AR proposal. Decision may be appealed to the City Council



REVIEW OF APPLICATION

- **Important Dates**
 - Submitted application on July 28, 2016
 - Deemed complete on November 14, 2016
 - 120-day period ends on March 14, 2017
- **Analysis and Findings**
 - Staff finds that the proposed architectural features shown on the site plan are consistent with the Tualatin Development Code, when subject to the recommended conditions of approval



FEBRUARY 6, 2017





PROPOSED ELEVATIONS



WEST ELEVATION
1/16" = 1'-0"



SOUTH ELEVATION
1/16" = 1'-0"

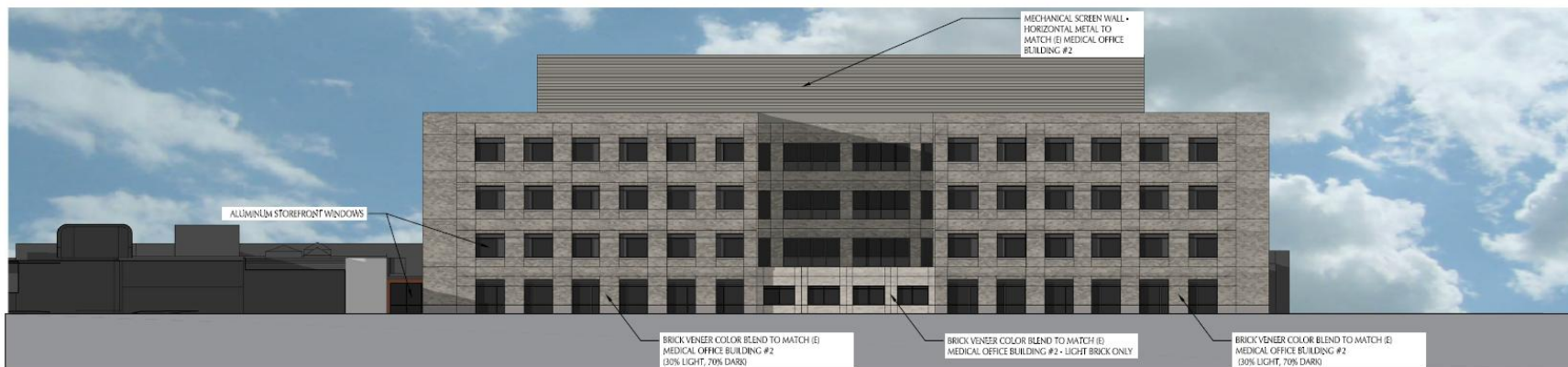


PROPOSED ELEVATIONS



EAST ELEVATION

1/16" = 1'-0"



NORTH ELEVATION

1/16" = 1'-0"



RECOMMENDED CONDITIONS

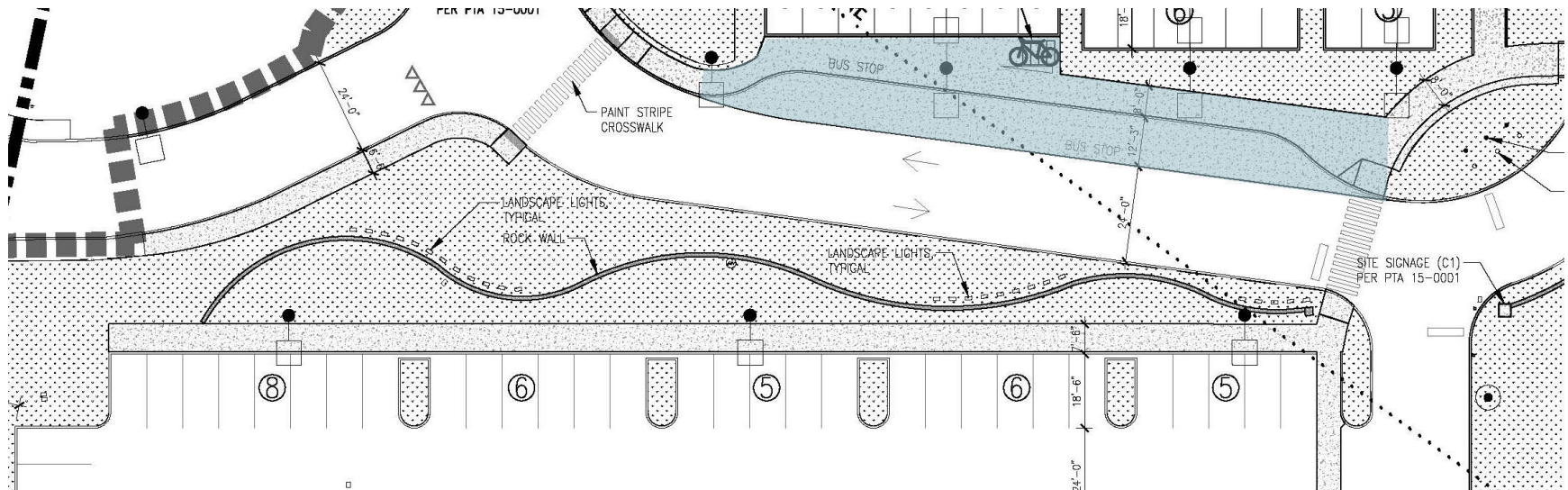
- **Pedestrian Circulation**
 - Conditions: AR-3, 4, 5, 6
- **Architectural Feature Review Elements:**
 - 6 ft wide walkways
 - Visibly raised walkways through parking area
 - Curb ramps
 - 8 ft wide accessway to connect walkways to abutting Arterial streets





RECOMMENDED CONDITIONS

- **Public Transit**
 - Condition: AR-8
- **Architectural Feature Review Elements:**
 - TriMet acceptance of transit pad design



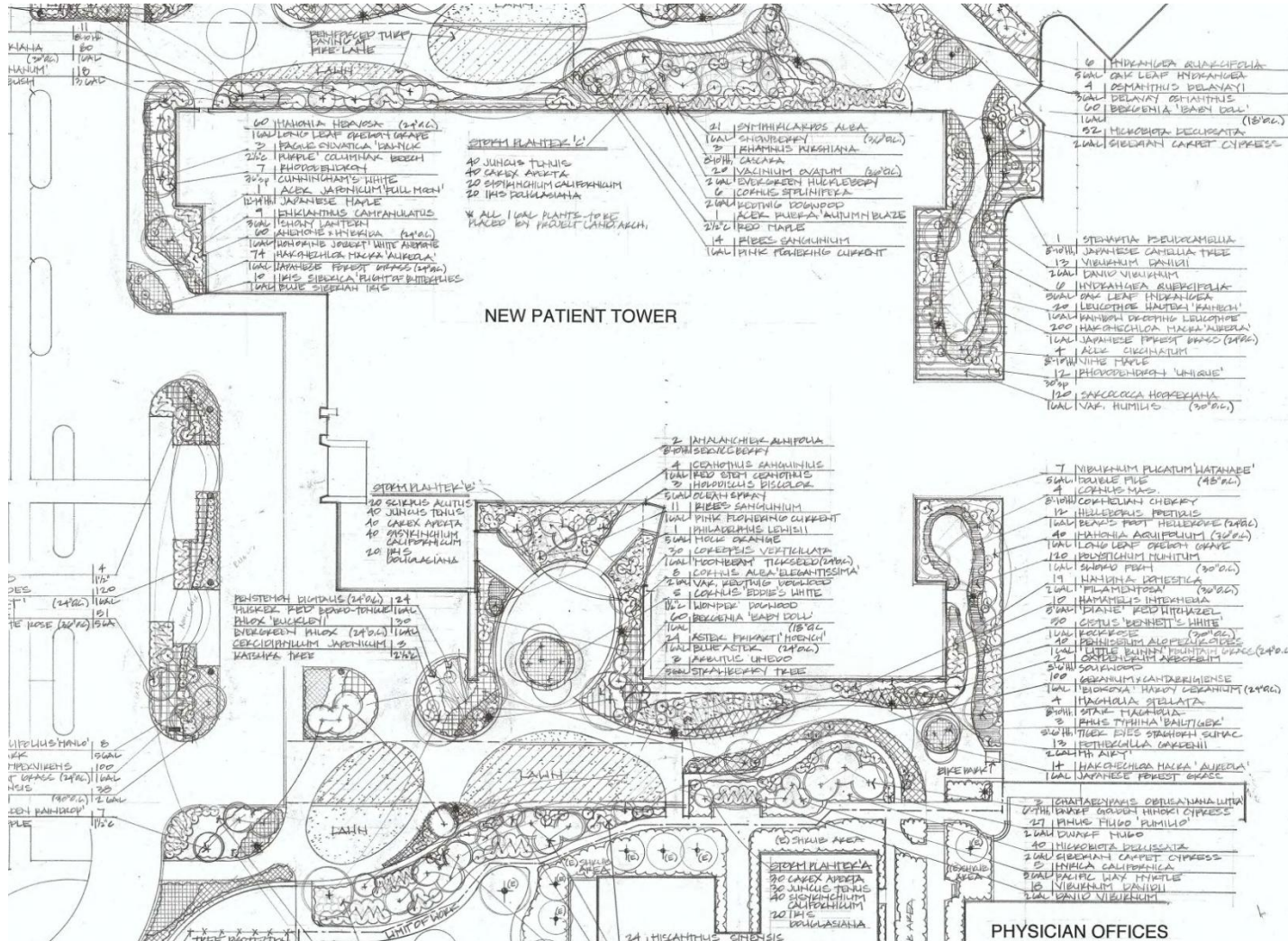


RECOMMENDED CONDITIONS

- **Landscaping**
 - Conditions: AR-10, 11, 12
- **Architectural Feature Review Elements:**
 - Proposed trees to meet minimum size requirements
 - Deciduous: 1.5” caliper
 - Coniferous: 5’ in height



REVIEW ELEMENTS: LANDSCAPING



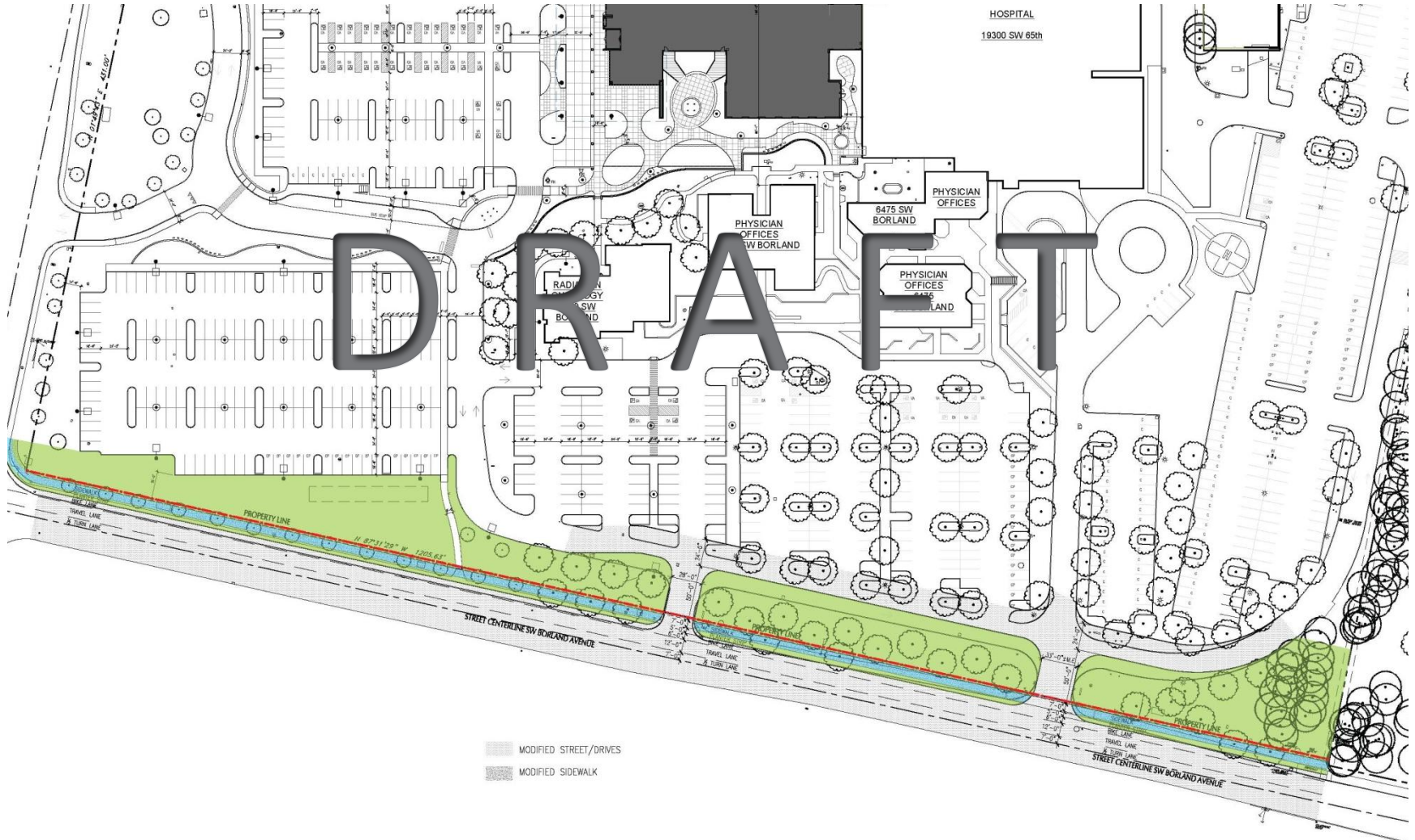


RECOMMENDED CONDITIONS

- **Site Revisions**
 - Condition: AR-13
- **Public Facilities Review Condition:**
 - Borland Road improvements to Clackamas Co. standards
- **Architectural Feature Review Elements:**
 - Potential impacts to: parking lot, landscaping, and tree mitigation



REVIEW ELEMENTS: BORLAND ROAD





RECOMMENDED CONDITIONS

- **Bicycle Parking**
 - Condition: AR-14
- **Architectural Feature Review Elements:**
 - MUTCD signage
- **Architectural Review Board Consideration:**
 - Reduced bike parking requirements



REVIEW ELEMENTS: BIKE PARKING

- **613 bike stalls required by Code**
 - TDC 73.370(1)(n) allows applicant to demonstrate why fewer bike stalls are sufficient
- **Applicant Response:**
 - 27 bicycle stalls are available for staff and patient use
 - Unlikely that patients would arrive by bike
 - 2014 Trimet employee commute survey: 2 trips reported; adjusted to 5
 - Current bike parking used: 7-19%
 - Proposed additional: 32% increase in floor area
 - If commuters tripled: 15 stalls needed
- **Staff recommends approval of reduced bike parking requirement**

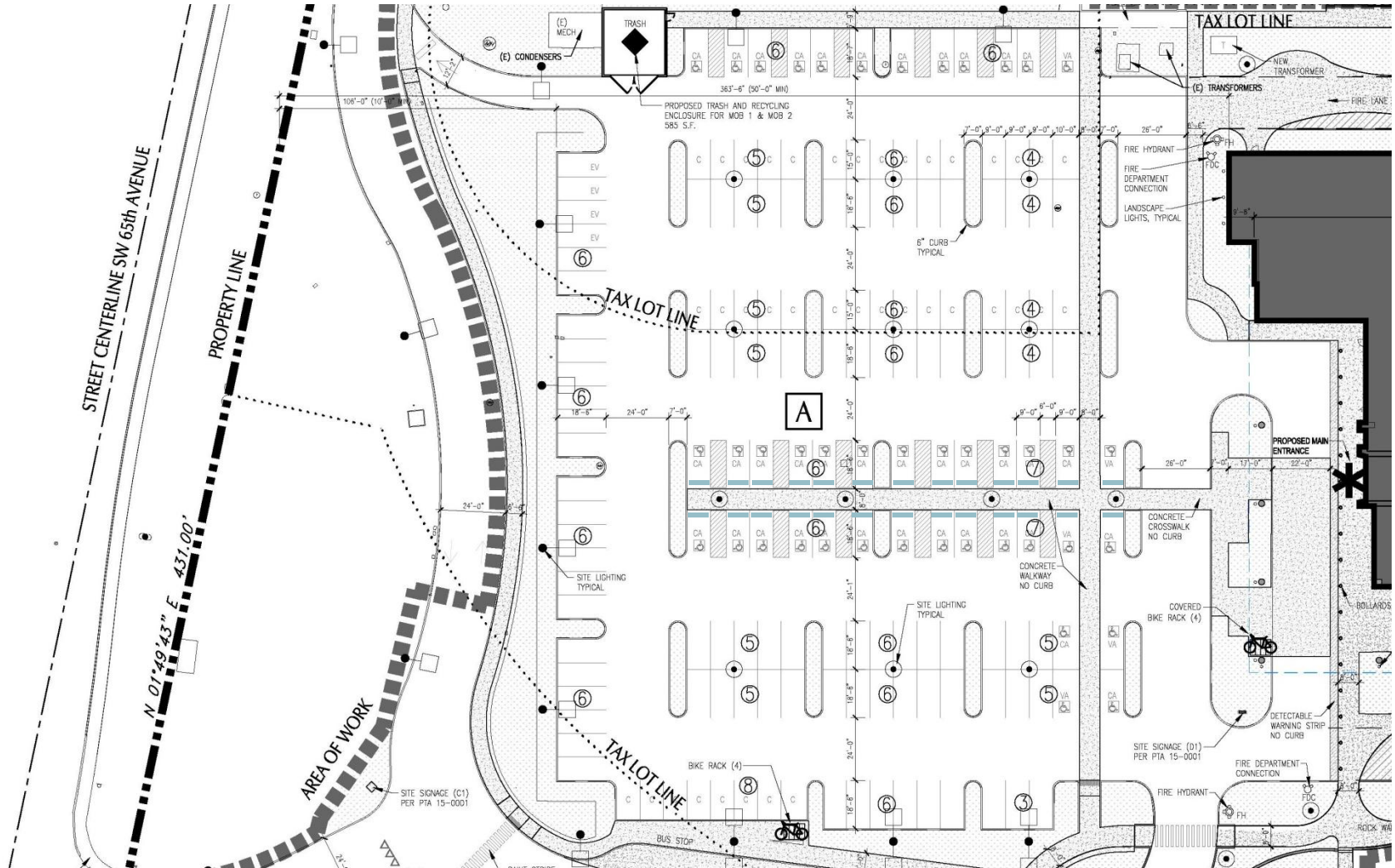


RECOMMENDED CONDITIONS

- **Parking Design**
 - Conditions: AR-15, 16
- **Architectural Feature Review Elements:**
 - Wheel stops
 - 24 ft aisle width between parking stall lines



REVIEW ELEMENTS: WHEEL STOPS





RECOMMENDATION OPTIONS

- Approve with staff recommended findings and conditions of approval
- Approve with amended findings and conditions of approval
- Continue the Hearing
- Deny the application