



City of Tualatin

Attachment G:
Public Engagement



Basalt Creek
concept Plan



**Concept Plan
Open House**

April 28, 2016

Tonight's Agenda

**Project
Update –
Draft
Concept
Plan**

1

**Interactive
Polling –
Give your
feedback!**

2

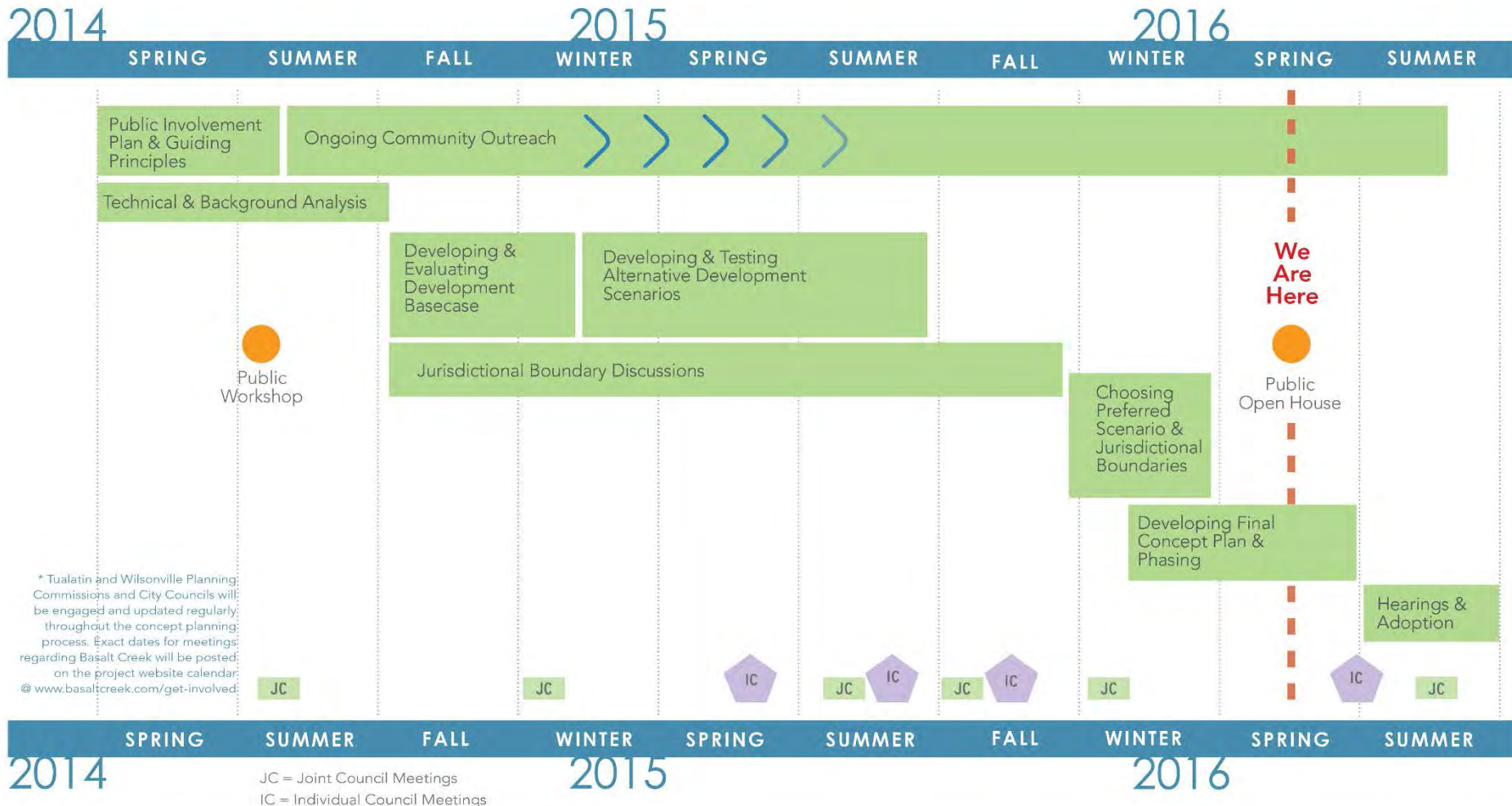
**Question
&
Answer**

3

**Poster
Session
with
Project Staff**

4

Project Status Update



Where we've been...

- Land Suitability
- Guiding Principles
- Base Case
- Utility Design
- Evaluations
- Four Options and Base Case studied

Public Involvement



Mapping workshop (June 2014)

- 40 attended
- Wide range of ideas
- Housing to the north, industrial to south
- Protect existing neighborhoods
- Open to a range of employment and commercial uses
- Appropriate transitions between land uses

Public Involvement

Online survey

- 160 responses
- Less focus on housing compared to workshop participants
- Some support for retail in general, especially restaurants
- Less support for warehousing, industrial flex space
- Strong interest in public access to natural resources



Public Involvement

Focus groups and interviews

Developers

- Industrial development types changing
- Housing preferences changing
- Employers consider amenities
- Land assembly is a challenge

Property owners

- Desire for flexibility in land use
- Concerns about development impacts on quality of life



Public Involvement

Email, website and social media updates

- Monthly email and mailing updates
- 300 people on interested parties list



Themes from Joint Council

- Meet regional responsibility for jobs & housing
- Capitalize on area's assets
- Protect existing neighborhoods
- Maintain Cities' unique identities



City of Tualatin

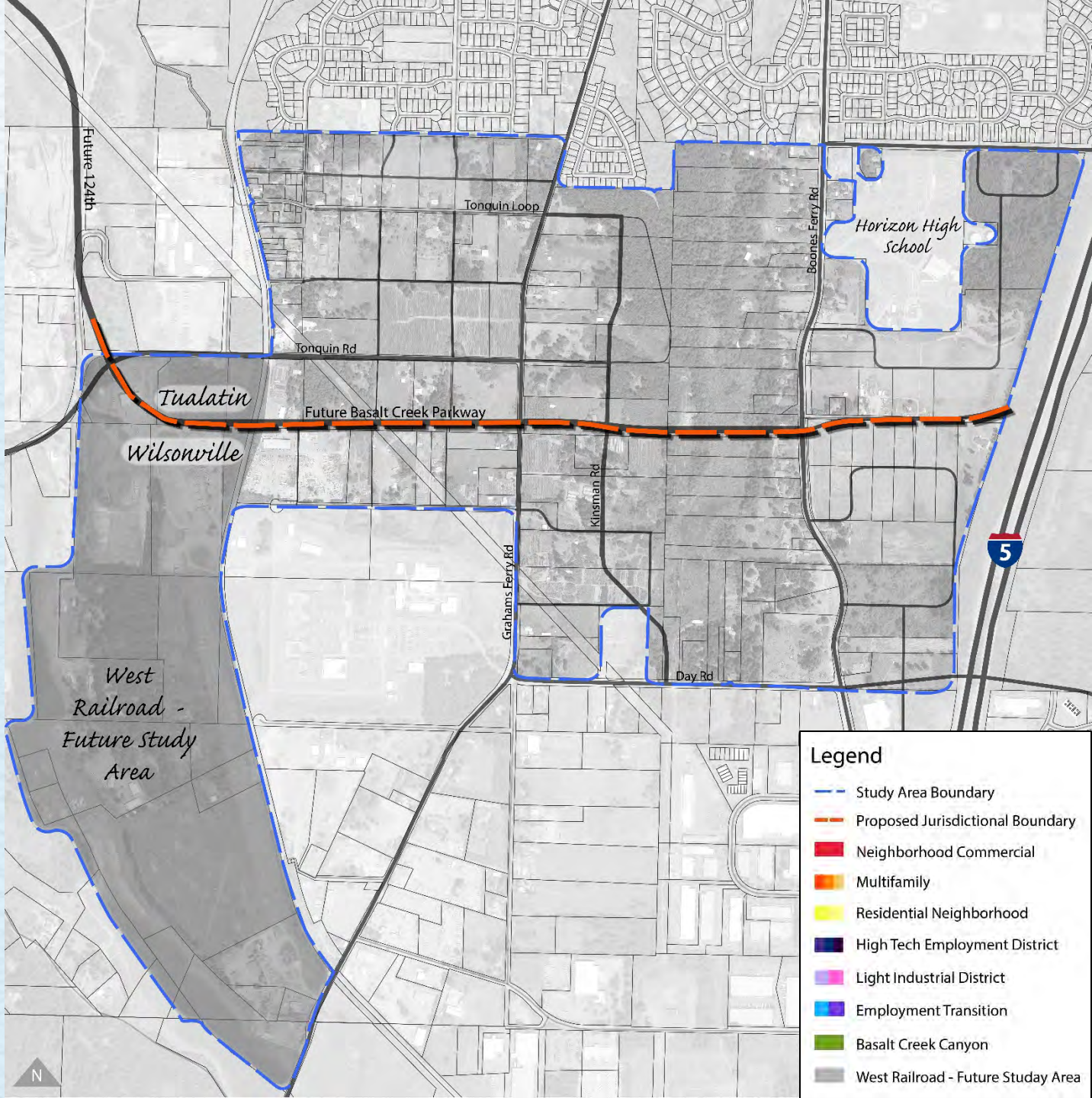
Themes from Joint Council

- Explore creative approaches, integration of employment and housing
- Ensure appropriate transitions between land uses
- High quality design and amenities for employment



City of Tualatin

Preferred Boundary

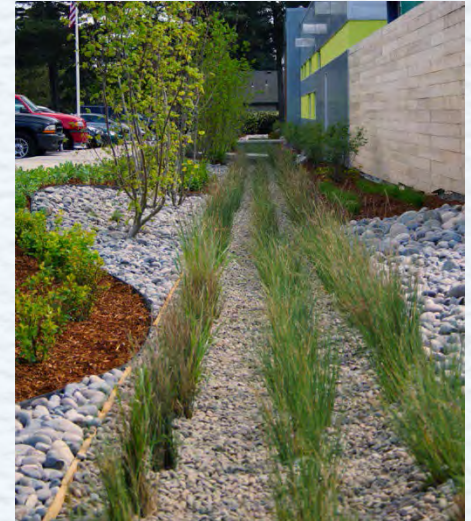


Preferred boundary subject to the Considerations for Success

- Legend**
- - - Study Area Boundary
 - - - Proposed Jurisdictional Boundary
 - Neighborhood Commercial
 - Multifamily
 - Residential Neighborhood
 - High Tech Employment District
 - Light Industrial District
 - Employment Transition
 - Basalt Creek Canyon
 - West Railroad - Future Study Area

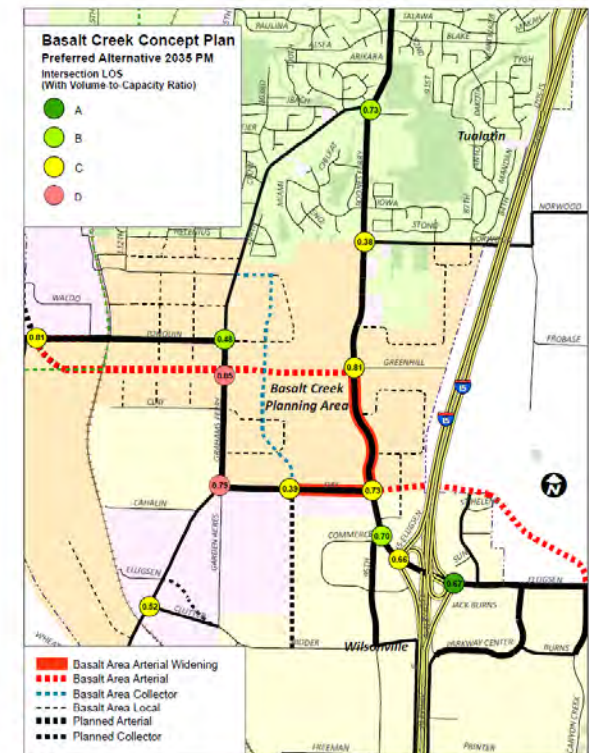
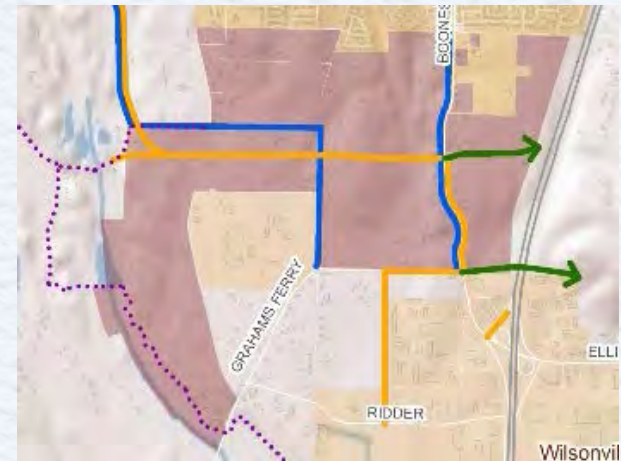
Ten Considerations for Success

1. **Sewer** – Cities serve own areas
2. **Stormwater** – Work jointly between the Cities and Clean Water Services to ensure requirements for each city's stormwater permits are met.
3. **Regional industrial land**– Employment land envisioned for the region benefits both cities



Ten Considerations for Success

- 4. Roadway improvements**– Work together with the region to assure needed upgrades are funded.
- 5. Traffic levels**– The Basalt Creek Transportation Refinement Plan modeled traffic totals in the area. Proposed new traffic loads would need to be evaluated.
- 6. Auto trips** – Ensure land uses support development that won't exceed transportation system capacity in each city.



Ten Considerations for Success

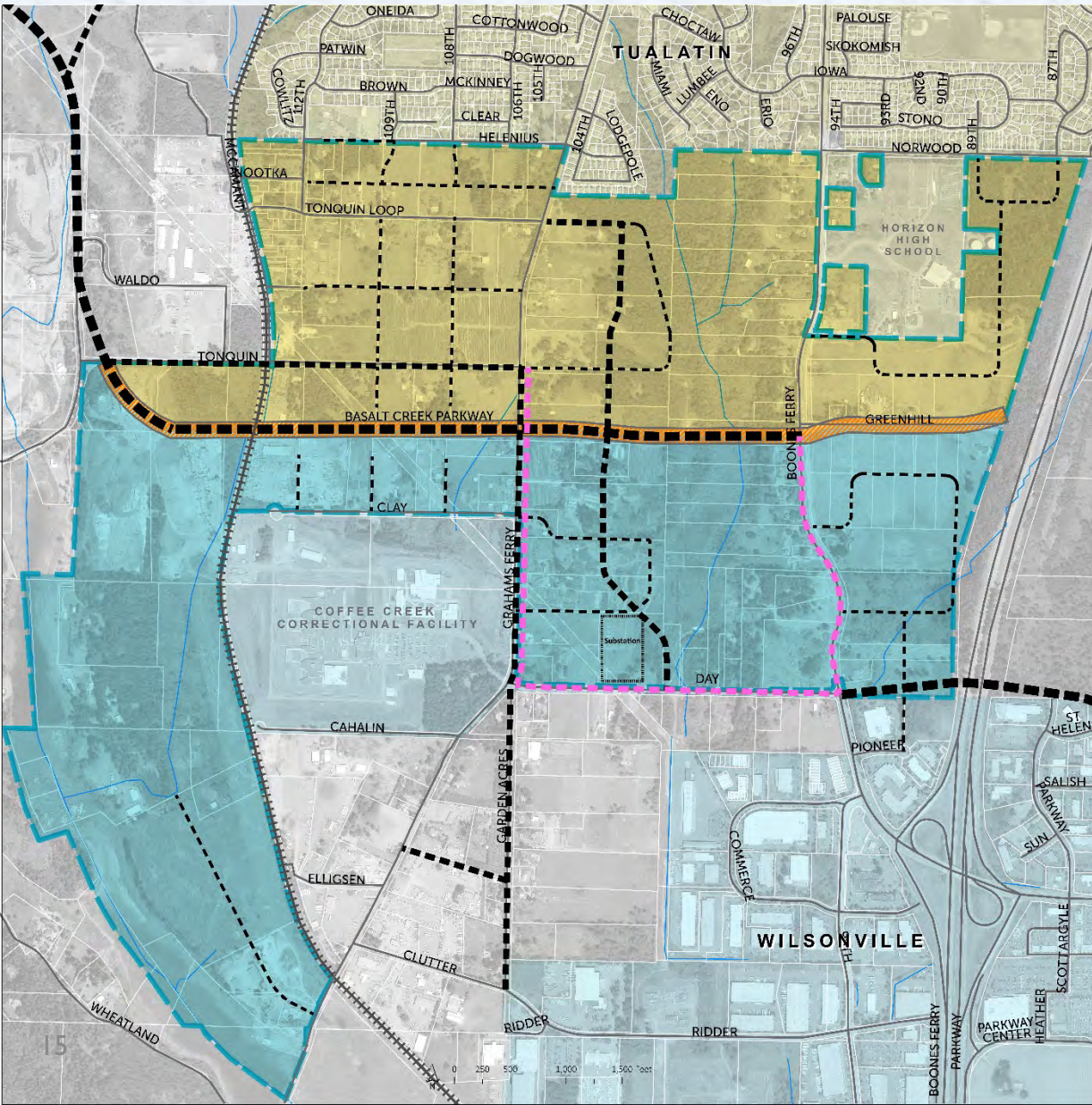
7. **I-5 Crossings** – Regional investment needed for more I-5 crossings
8. **Kinsman Rd.** – Cities will evaluate whether Kinsman Road extension north of Day Road is needed.
9. **Natural Area management** – Develop joint management practices for the Basalt Creek Canyon natural area.
10. **Transit service** – Determine how SMART and TriMet will provide most effective transit service to this area.



SMART
SOUTH METRO AREA REGIONAL TRANSIT

Basalt Creek
Concept Plan

Road Network Concept

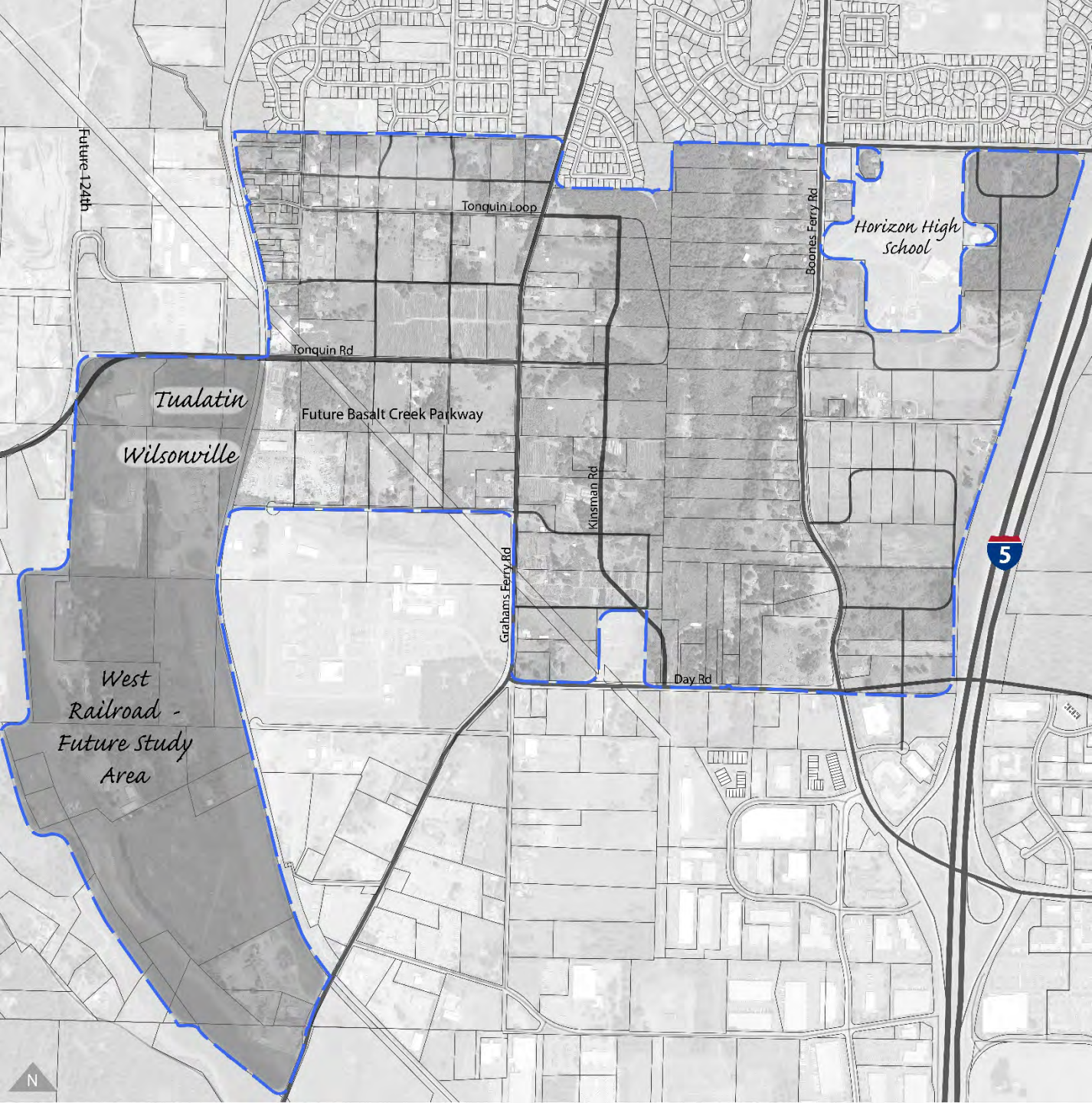


-  Planning Area
-  Jurisdictional Boundary*
-  Current Tualatin Boundary
-  Current Wilsonville Boundary
-  Future Tualatin Boundary
-  Future Wilsonville Boundary
-  Taxlot
-  Existing Roads
-  WES Commuter Rail
-  Arterial
-  Collector
-  Local
-  Collector Widening

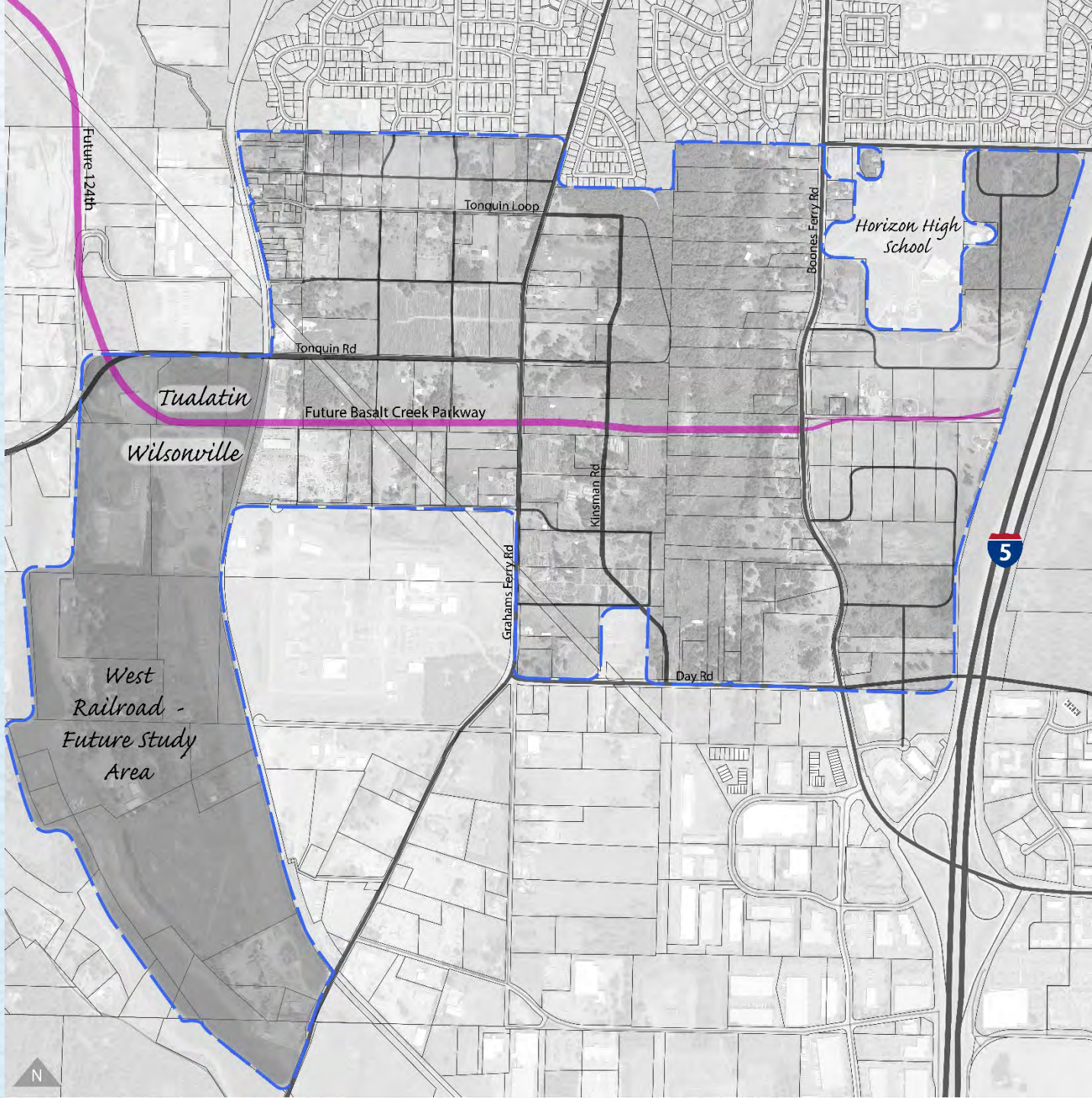
*Subject to Considerations for Success

Study Area Boundary

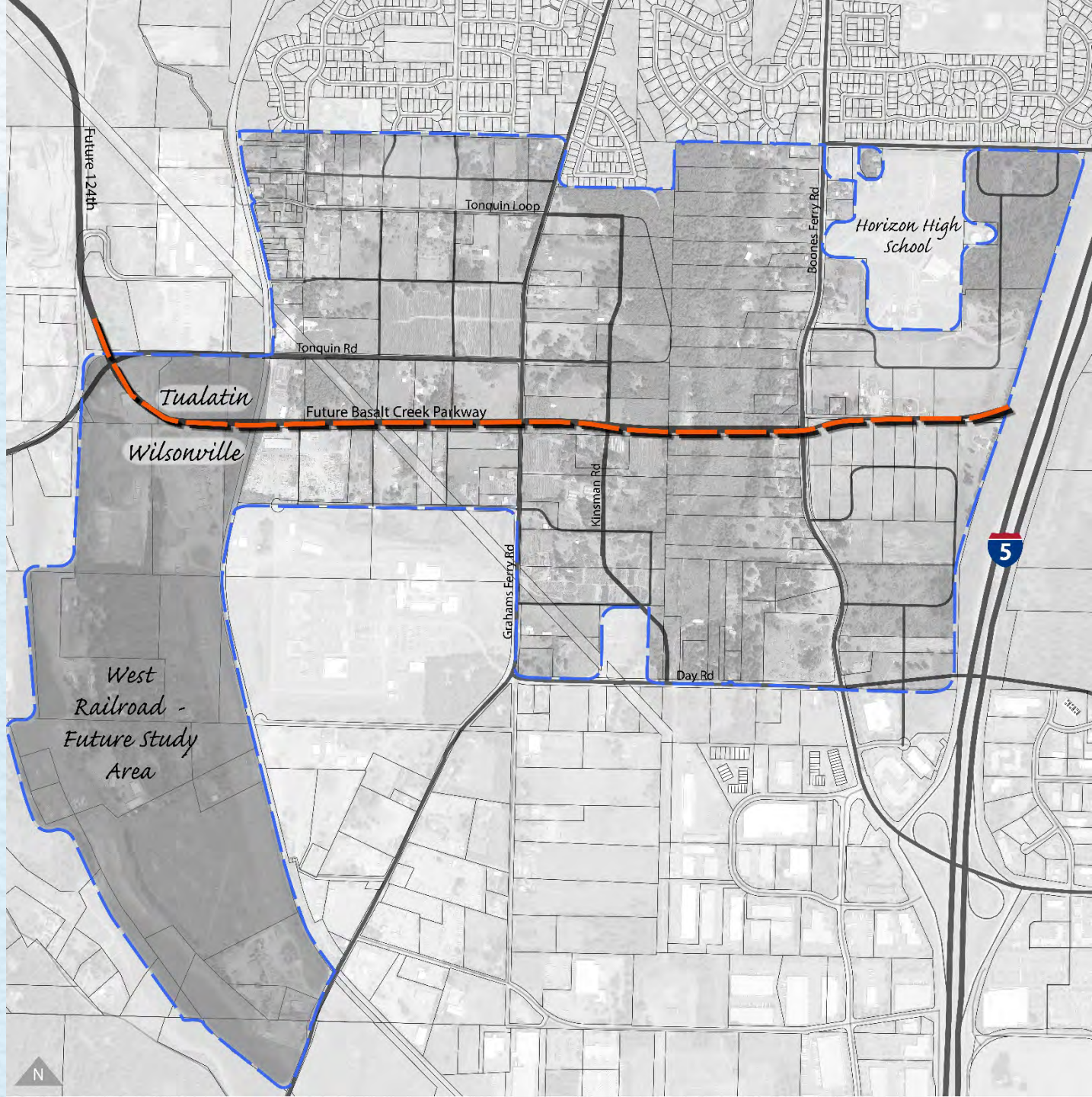
- Basalt Creek
- West Railroad Future Study Area



- **Basalt Creek Parkway:**
Tualatin north,
Wilsonville south

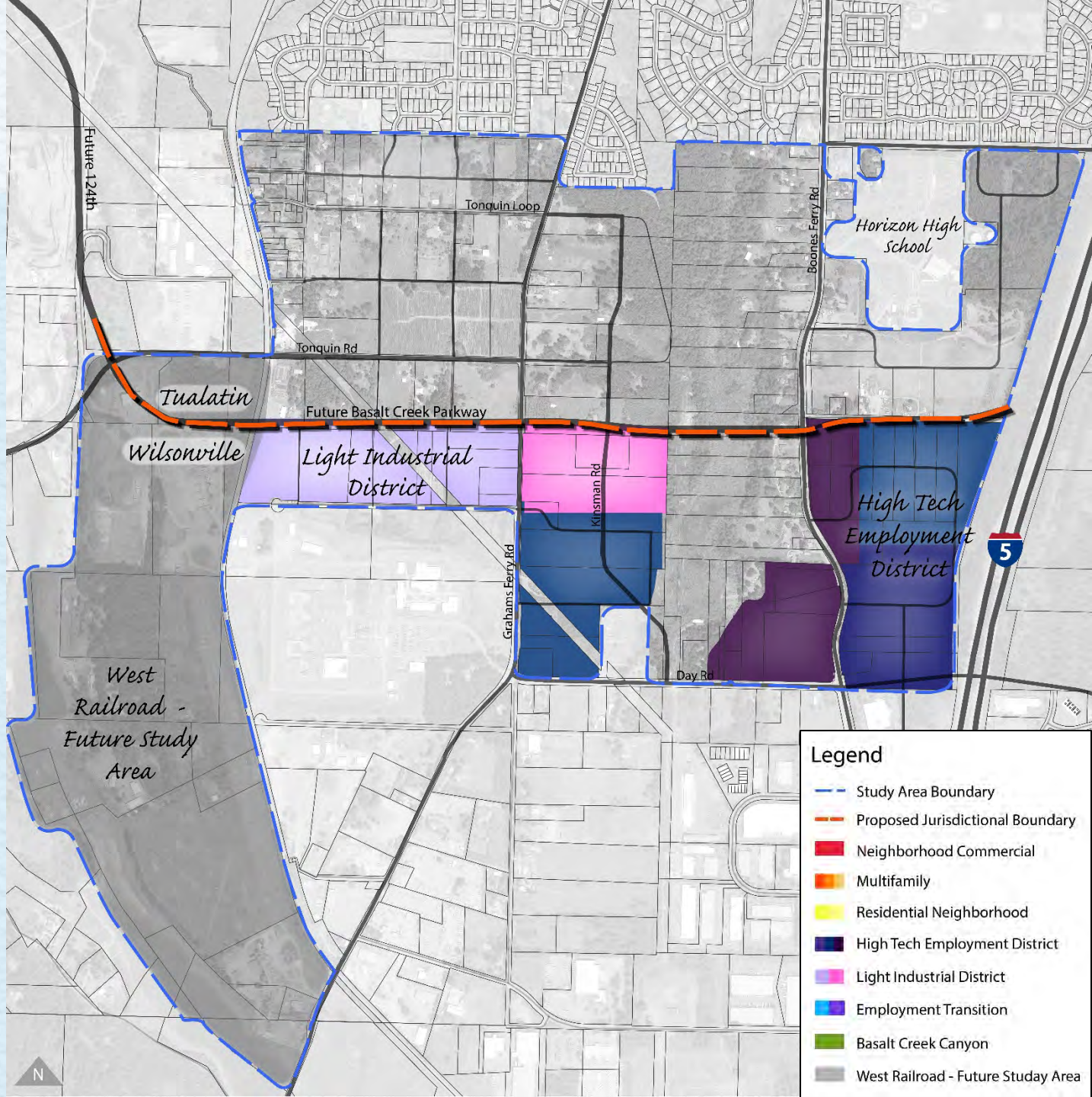


- Proposed Jurisdictional Boundary follows Basalt Creek Parkway



Wilsonville Land Uses

- Light Industrial
- High Tech Employment



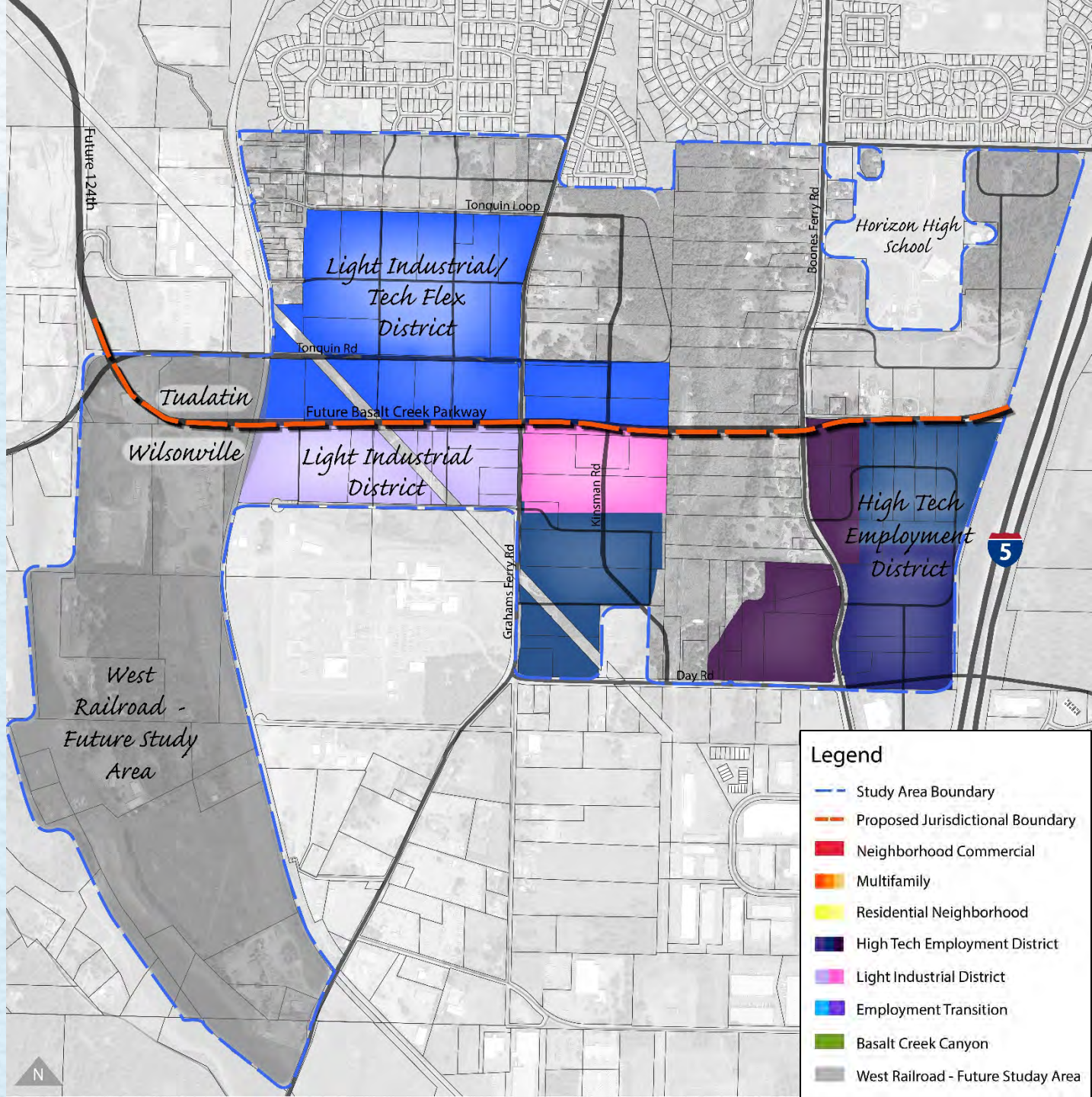
Tualatin Land Uses

- Light Industrial and Tech Flex

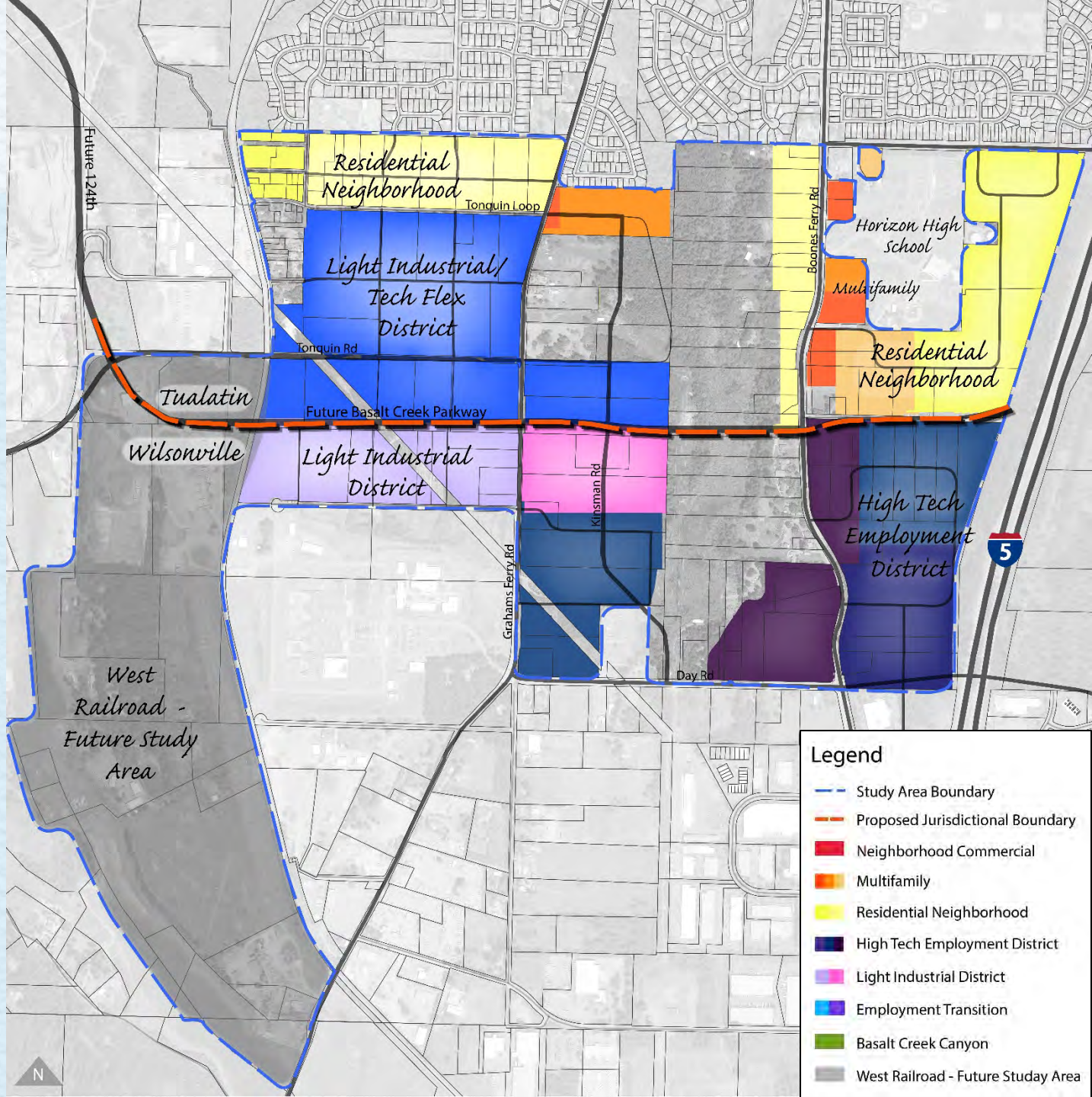
Wilsonville Land Uses

- Light Industrial
- High Tech Employment

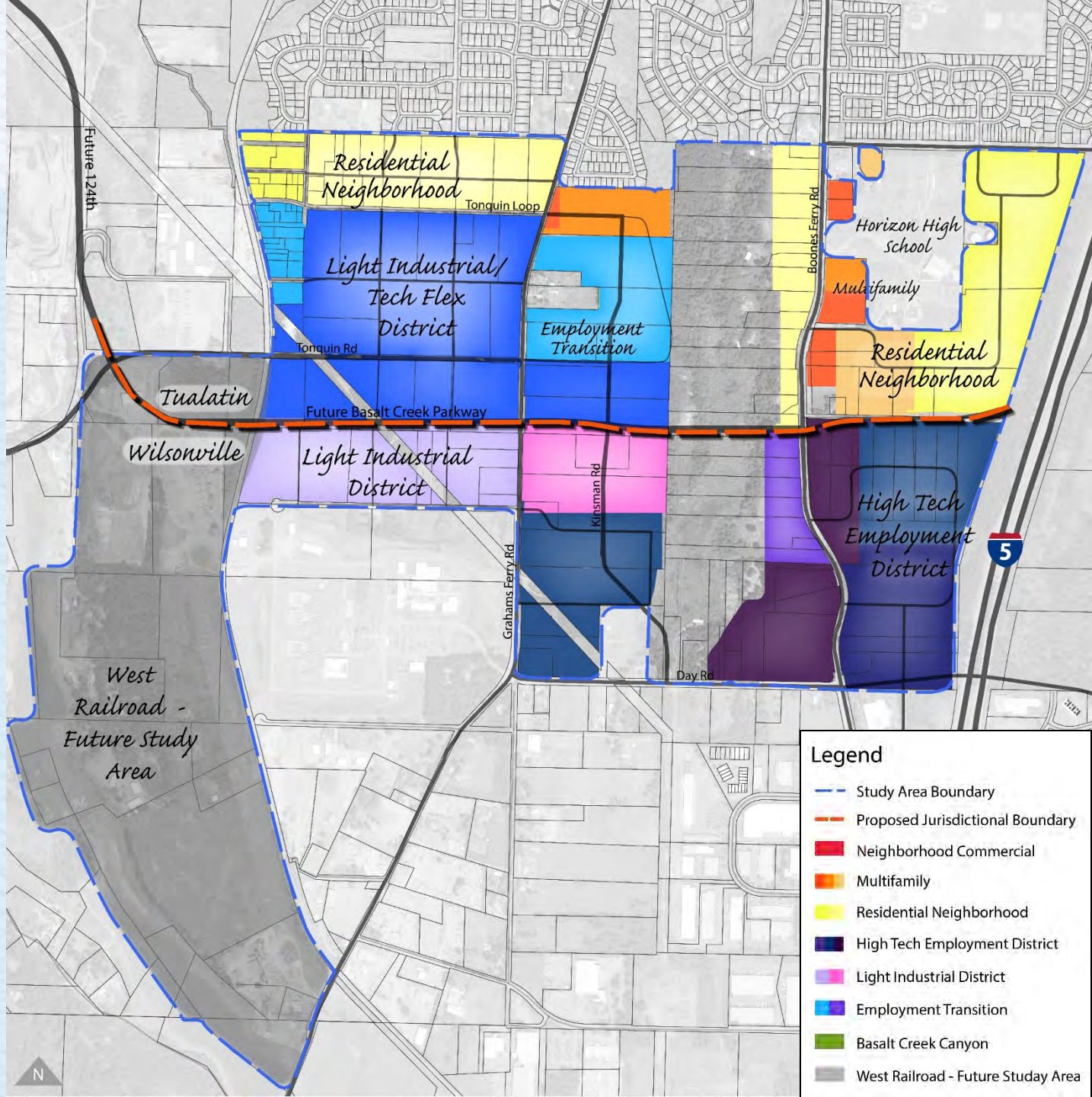
Placement based on land suitability



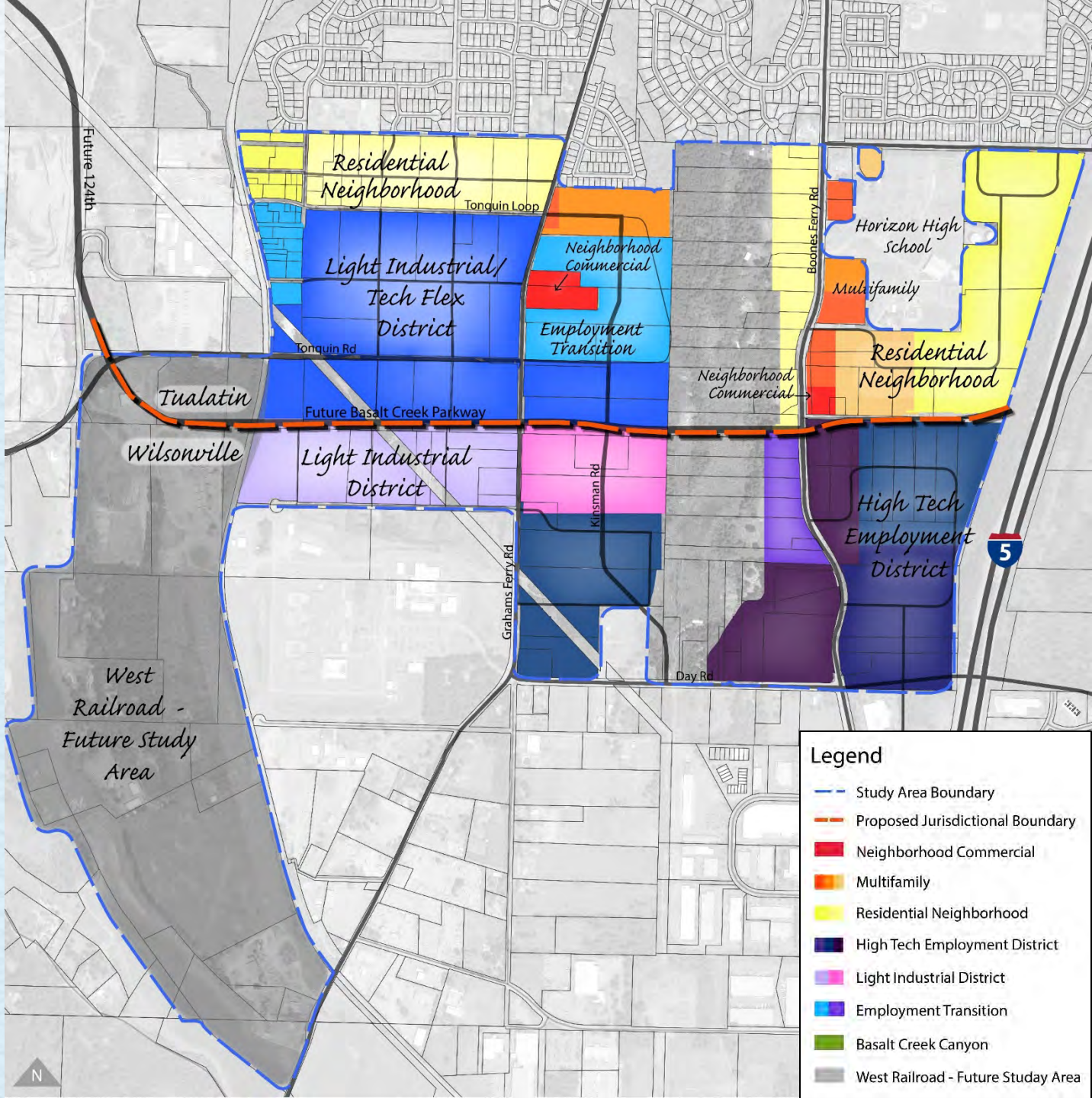
- **Housing to the north:** Buffers existing residential in the City of Tualatin



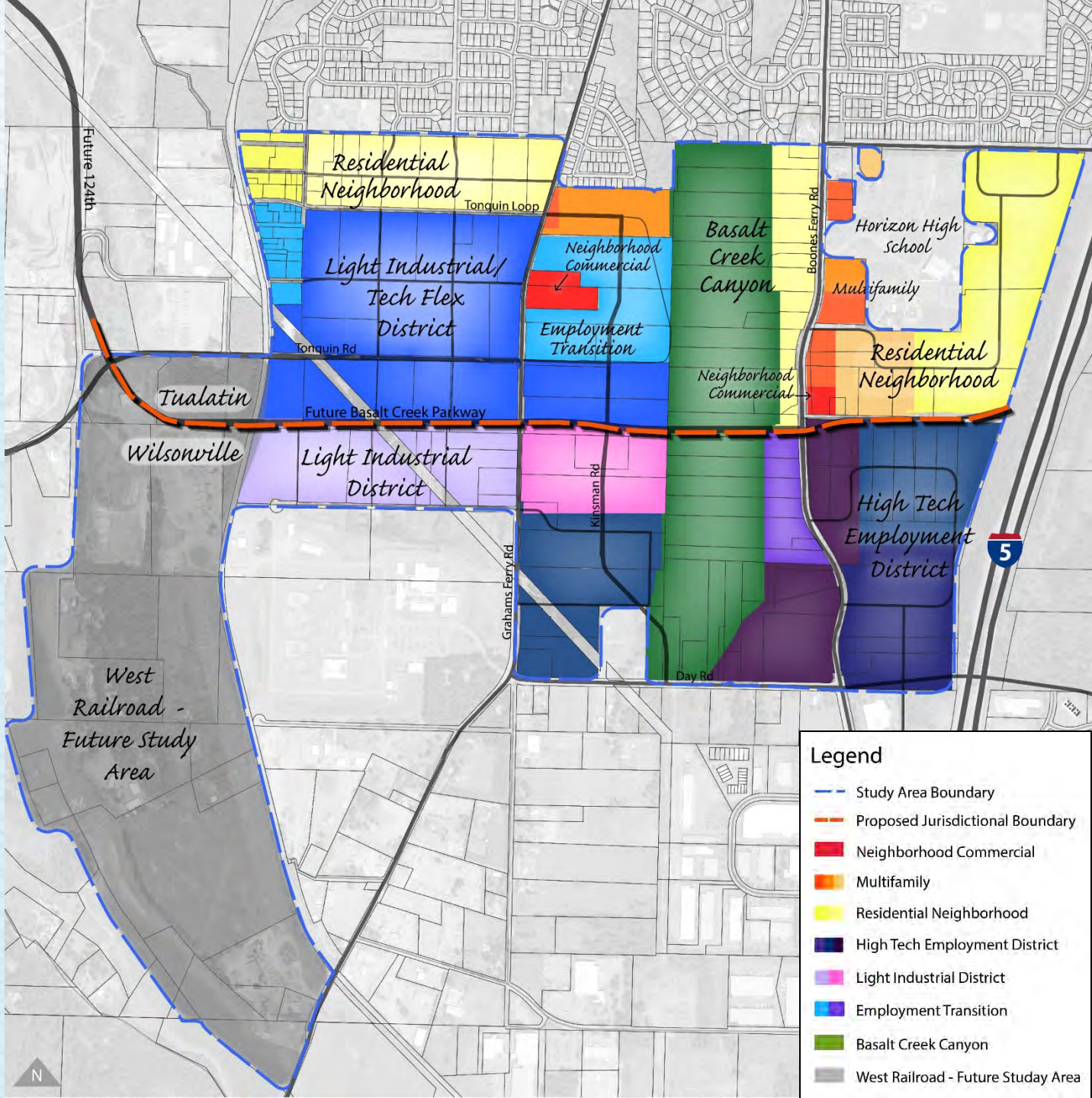
- **Employment Transition:**
Buffers new residential



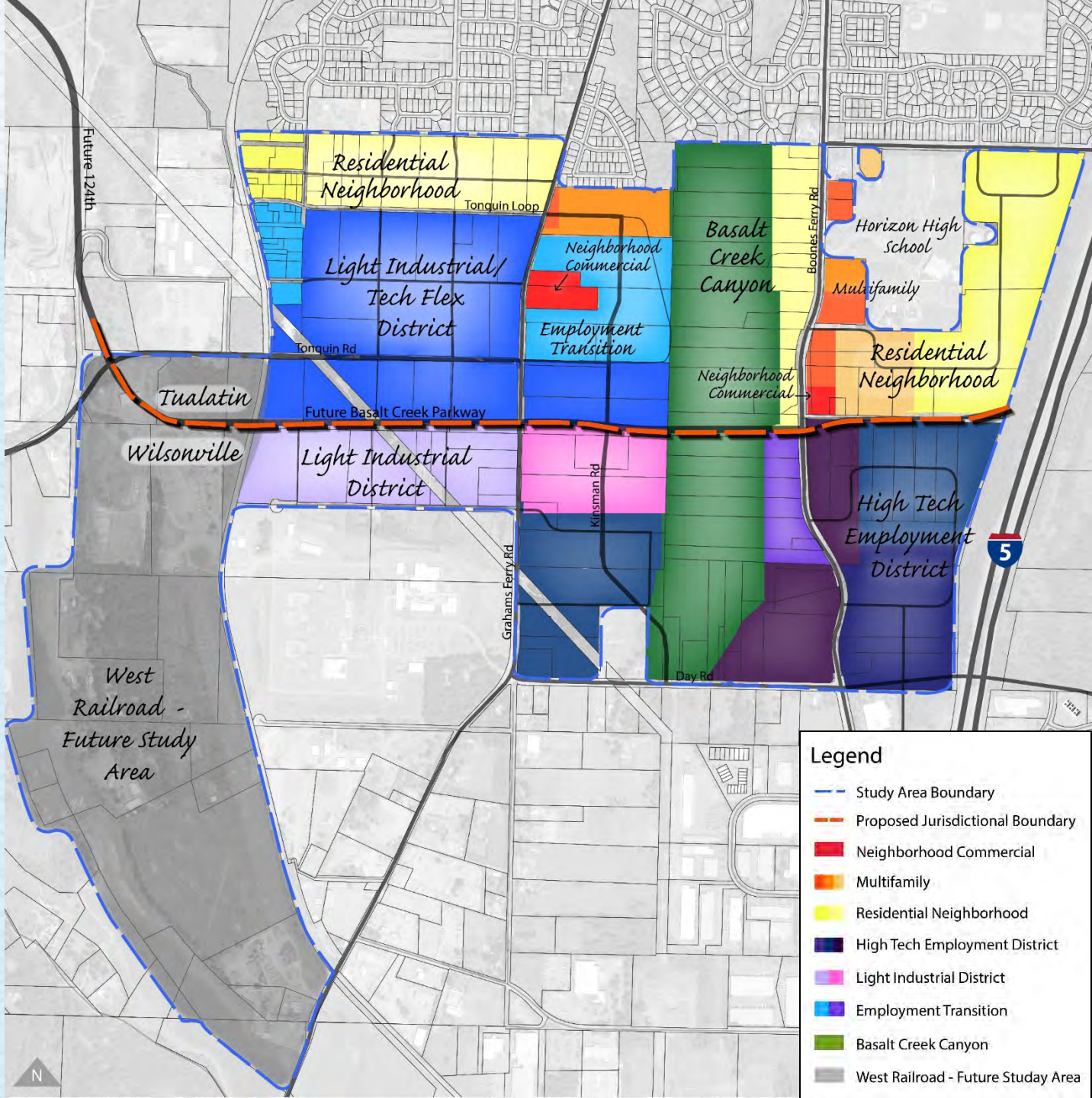
- **Small Retail Node:** Serves local residents & workers



- **Basalt Creek Canyon:** Open space spans both cities



Basalt Creek Land Use Concept Map

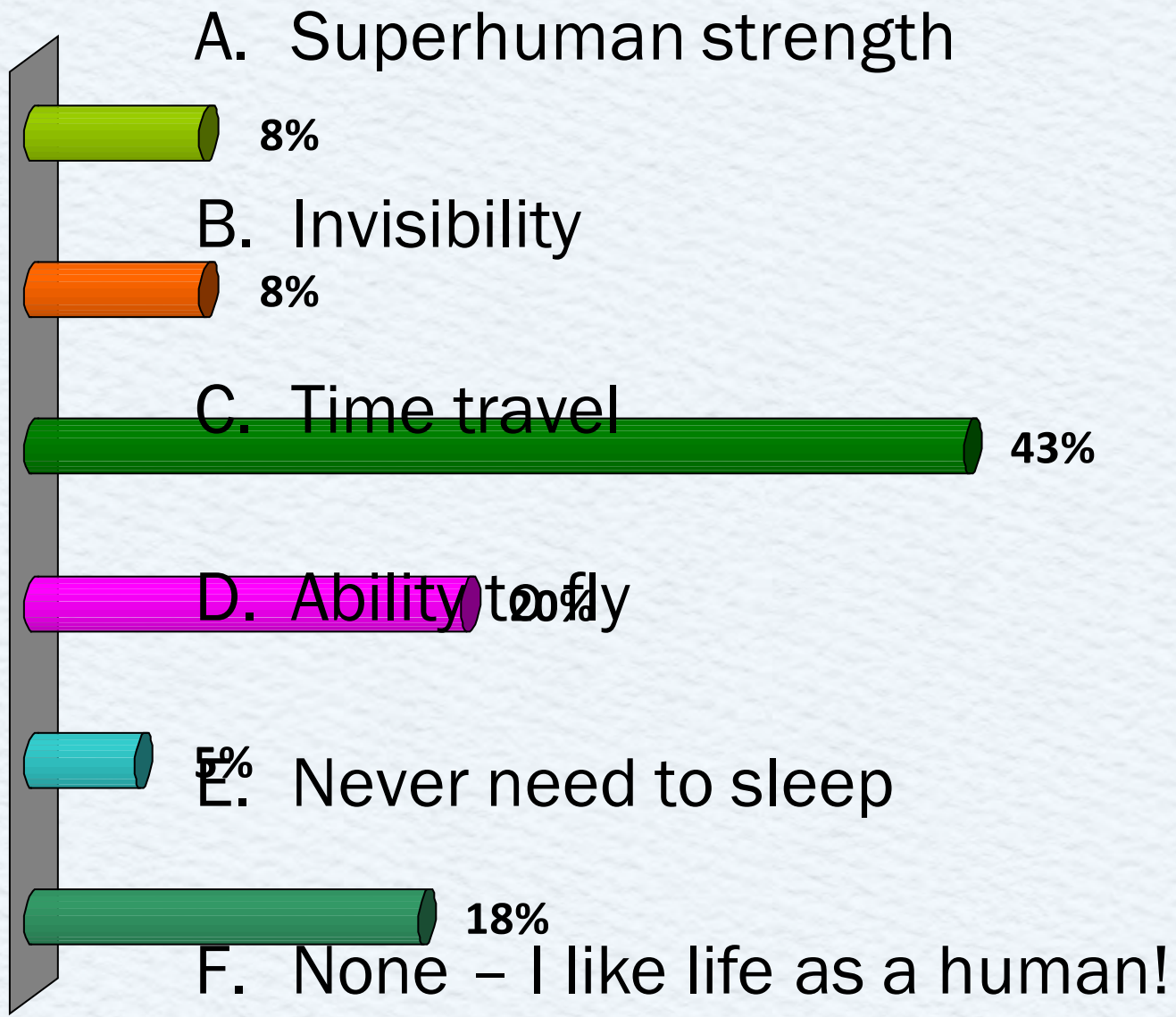


Instant Polling

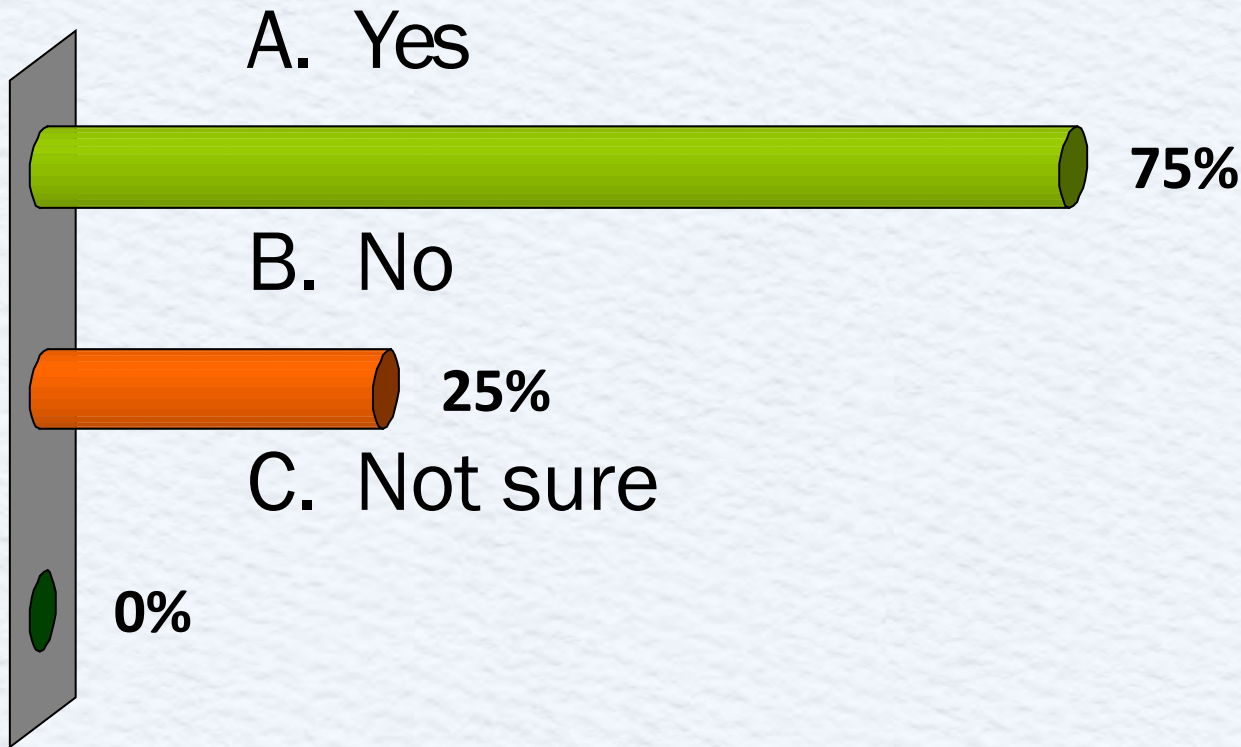
- Go with your gut!
- No right or wrong answers
- Responses are anonymous



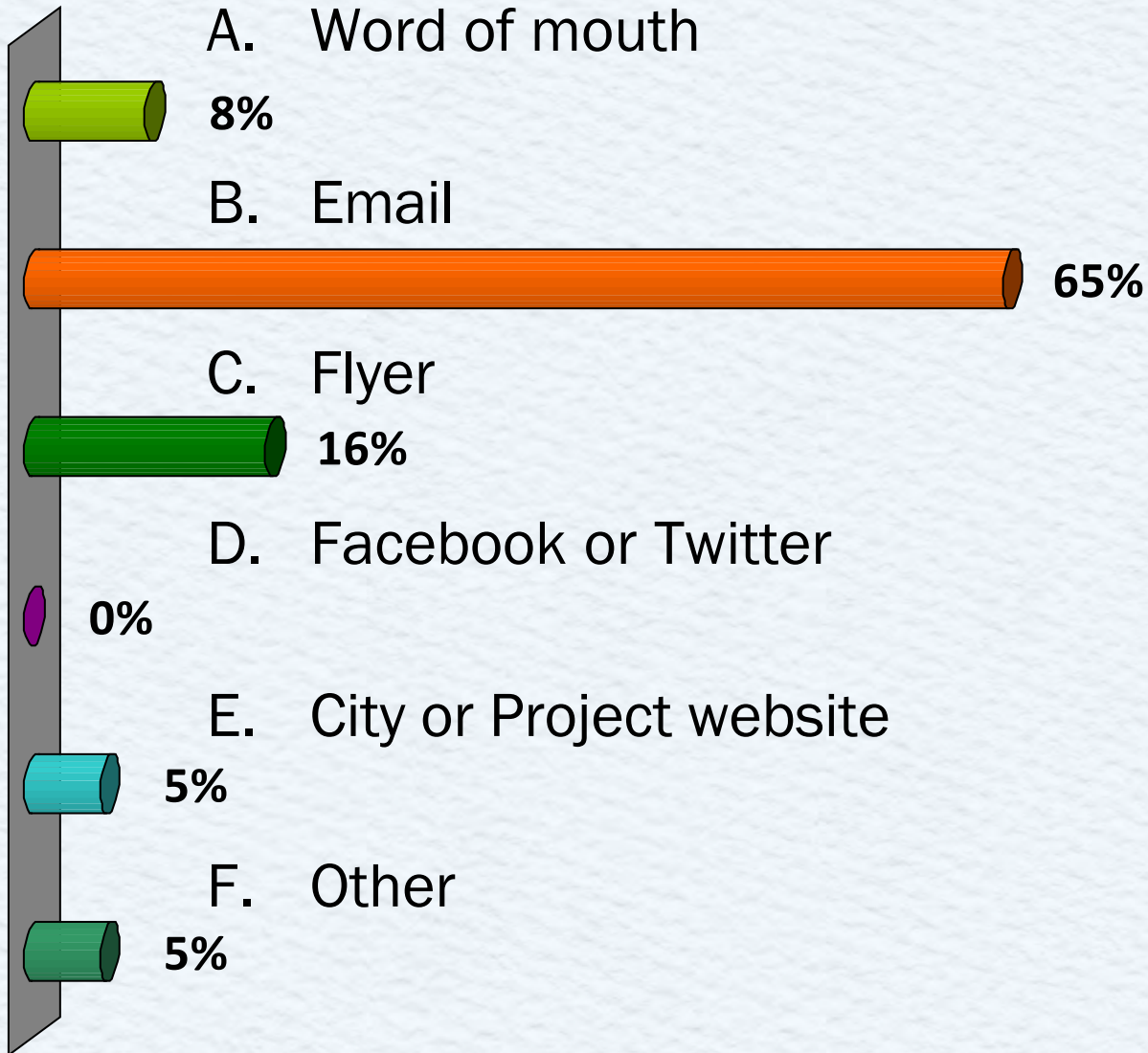
Which superpower would you rather have?



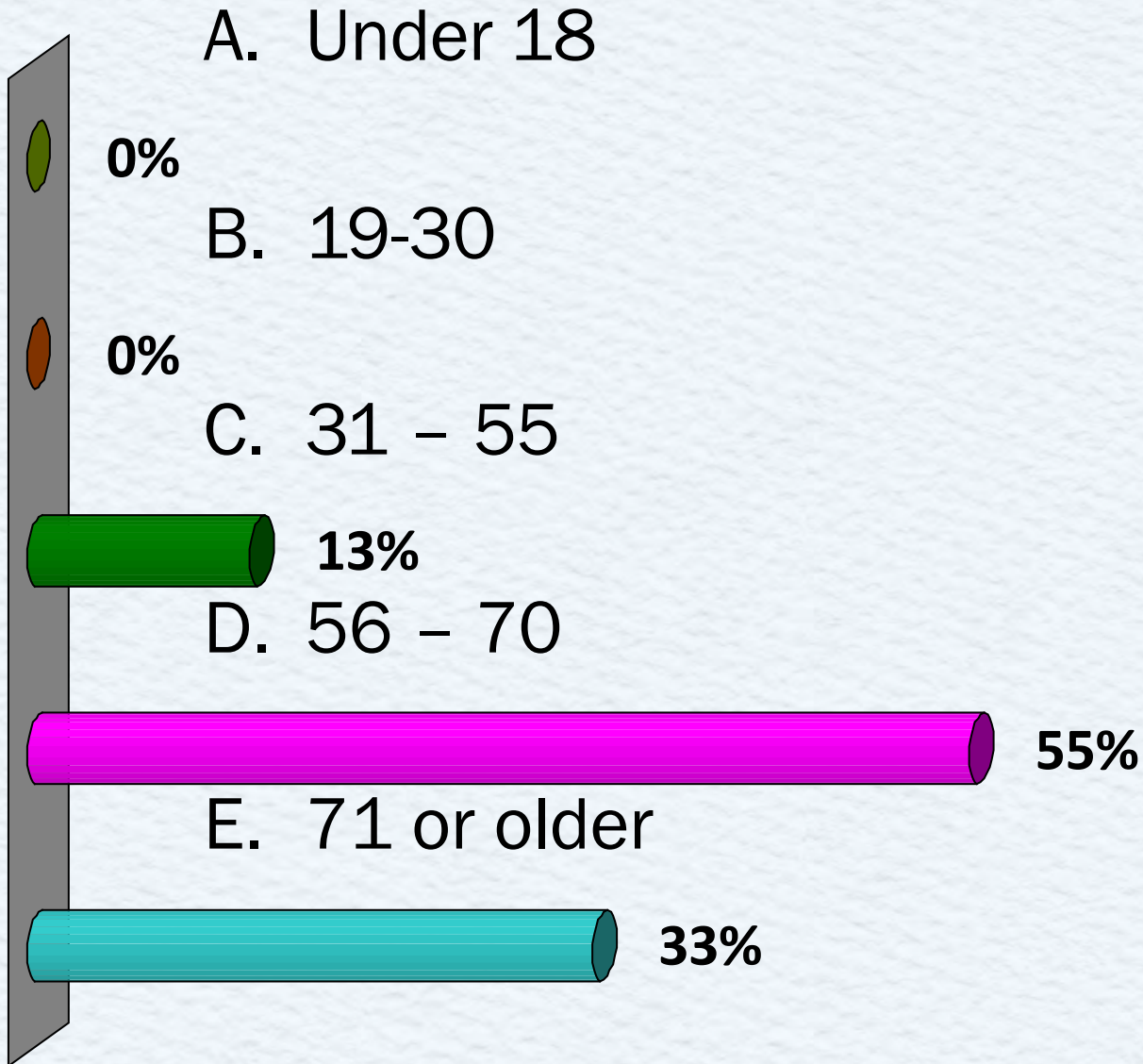
Have you been involved in the Basalt Creek project before today?



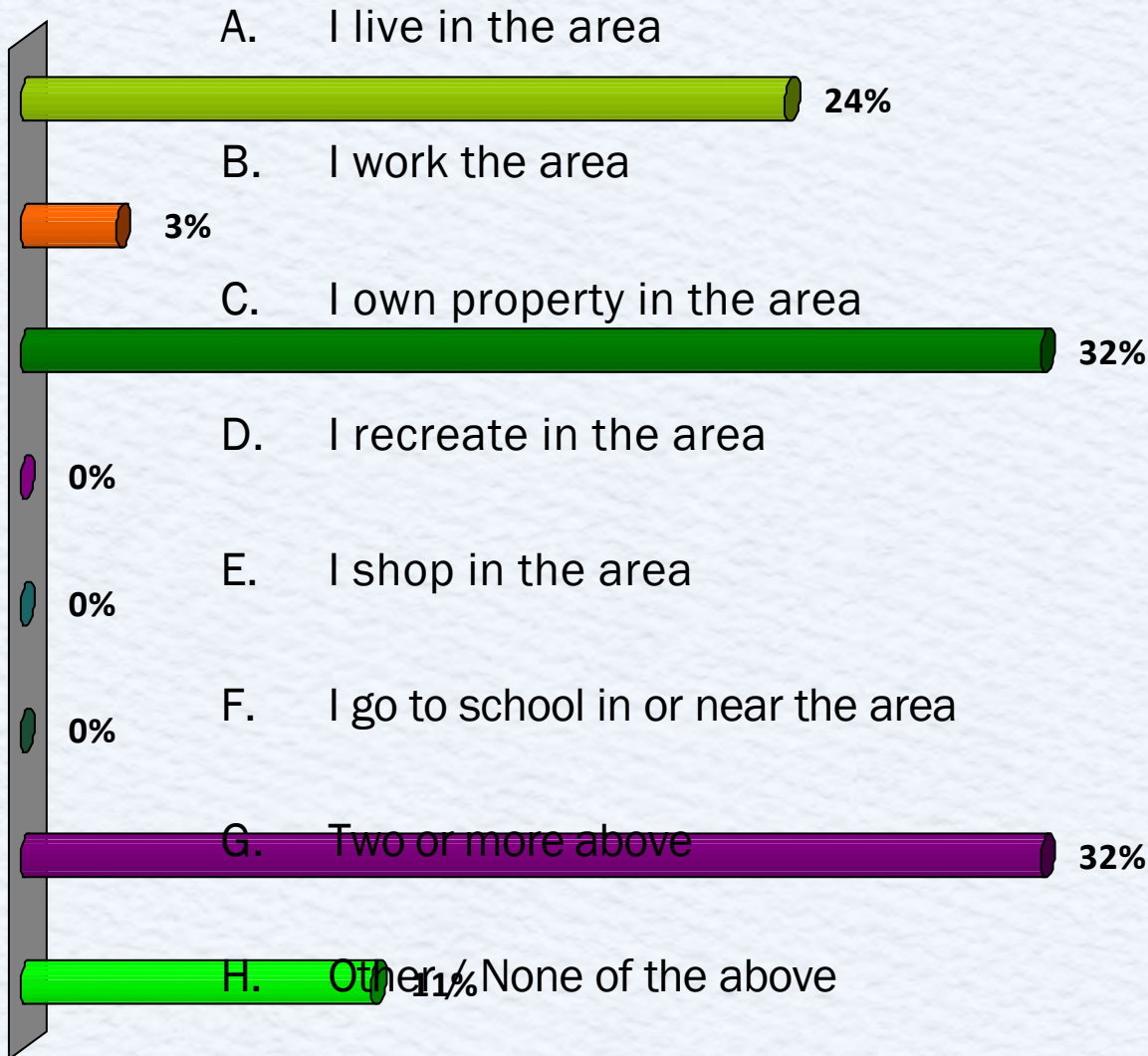
How did you hear about today's event?



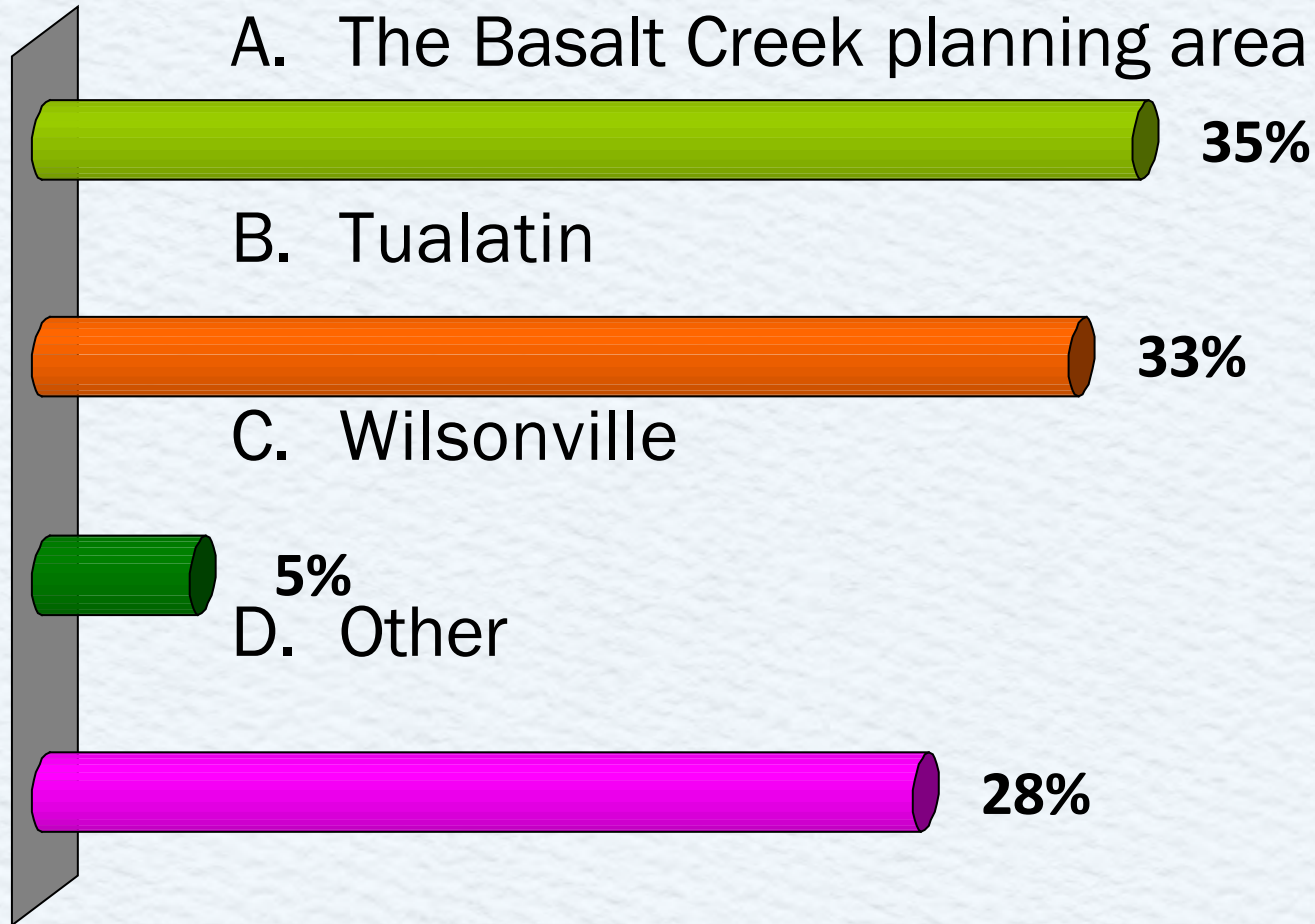
What is your age?



What is your primary connection to the Basalt Creek Planning Area?



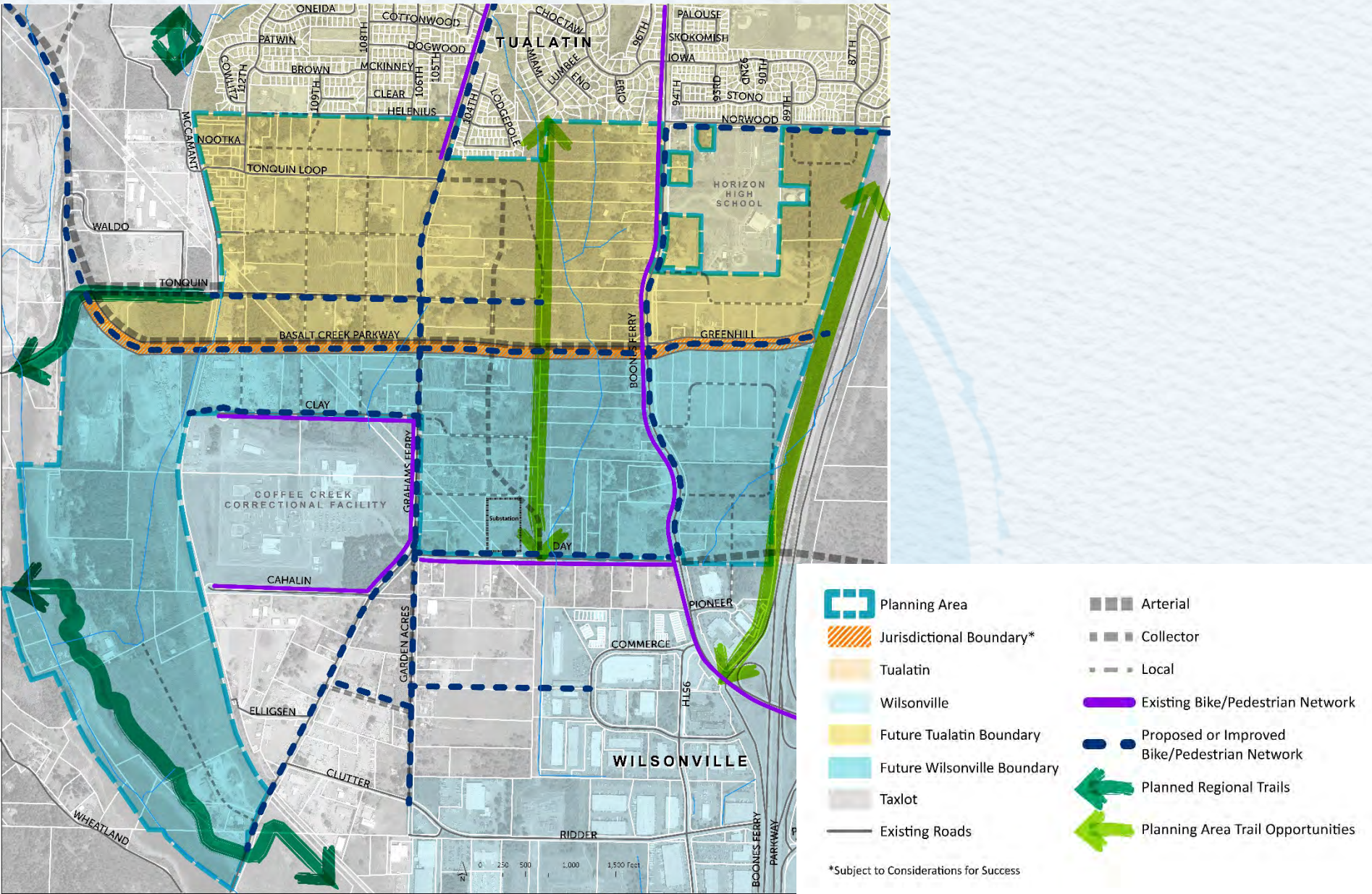
Where do you live?



Bike, Pedestrian and Trail Network



Bikes, Trails and Pedestrian Network



In the future, how might you walk or bike through the Basalt Creek area? (choose one)

- 5% A. Commuting to and from work
- 3% B. To get to transit (bus or WES)
- 3% C. To run errands
- 50% D. For exercise or recreation
- 21% E. To access recreation / natural areas
- 18% F. Other / Not sure

How often do you think you would walk or bike in Basalt Creek in the future?

15% A. Daily

29% B. At least once a week

17% C. At least once a month

15% D. Less than once a month

24% E. Rarely or never

How often do you currently walk or bike?

23% A. Daily

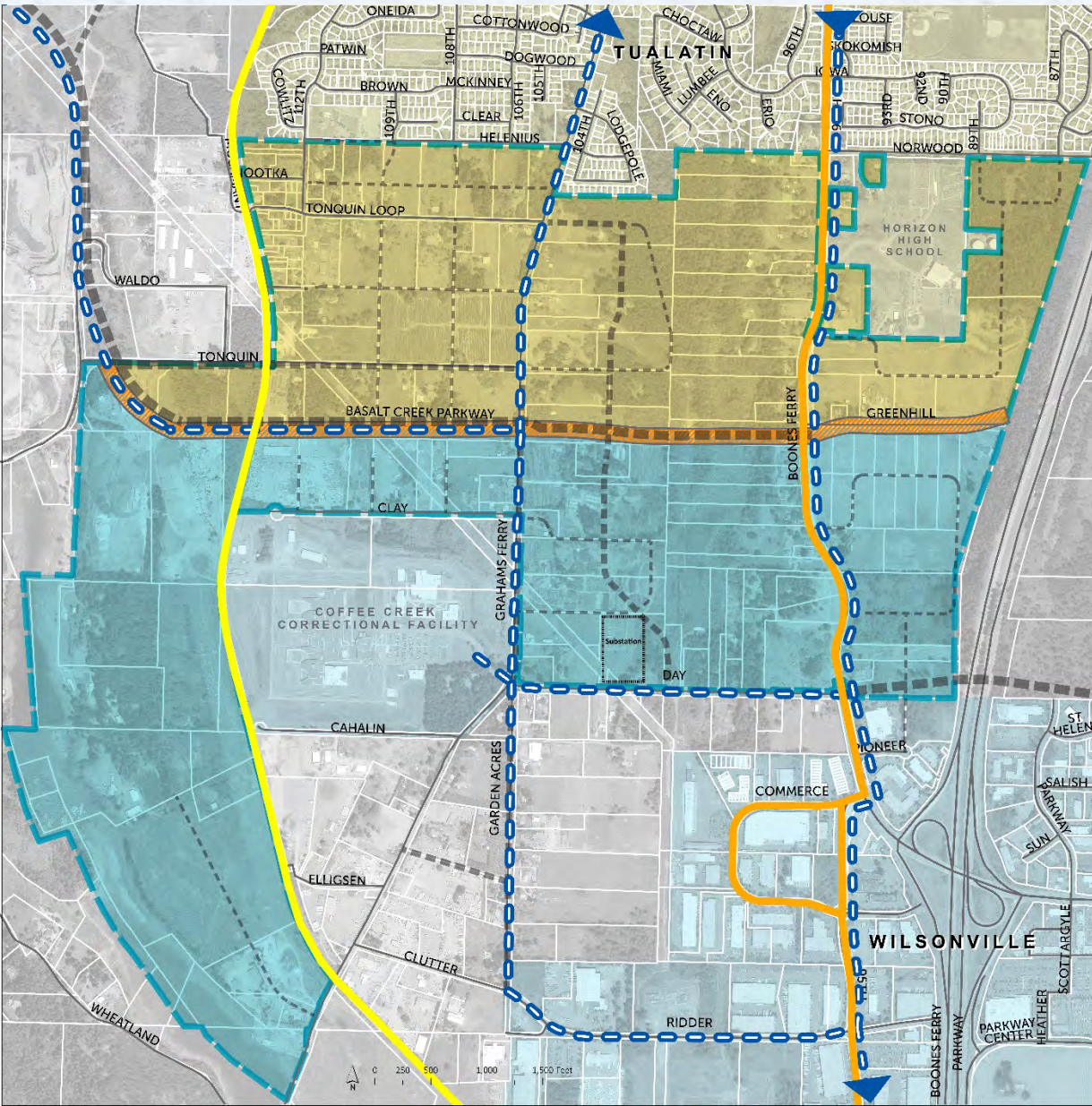
36% B. At least once a week















15% C. At least once a month

13% D. Less than once a month

13% E. Rarely or never

Transit Network



-  Planning Area
-  Jurisdictional Boundary*
-  Tualatin
-  Wilsonville
-  Future Tualatin Boundary
-  Future Wilsonville Boundary
-  Taxlot
-  Existing Roads
-  Arterial
-  Collector
-  Local
-  Existing Rail
-  Existing Bus Line
-  Proposed New Bus Line

*Subject to Considerations for Success

In the future, how might you use transit in the Basalt Creek area?

14%

A. To commute to and from work

8%

B. To run errands

3%

C. To visit Bridgeport Village

11%

D. To visit the Wilsonville Town Center

0%

E. To get kids to and from school

65%

F. Other / Not sure

How often do you think you would use transit in Basalt Creek in the future?

3%

A. Daily

17%

B. At least once a week

28%

C. At least once a month

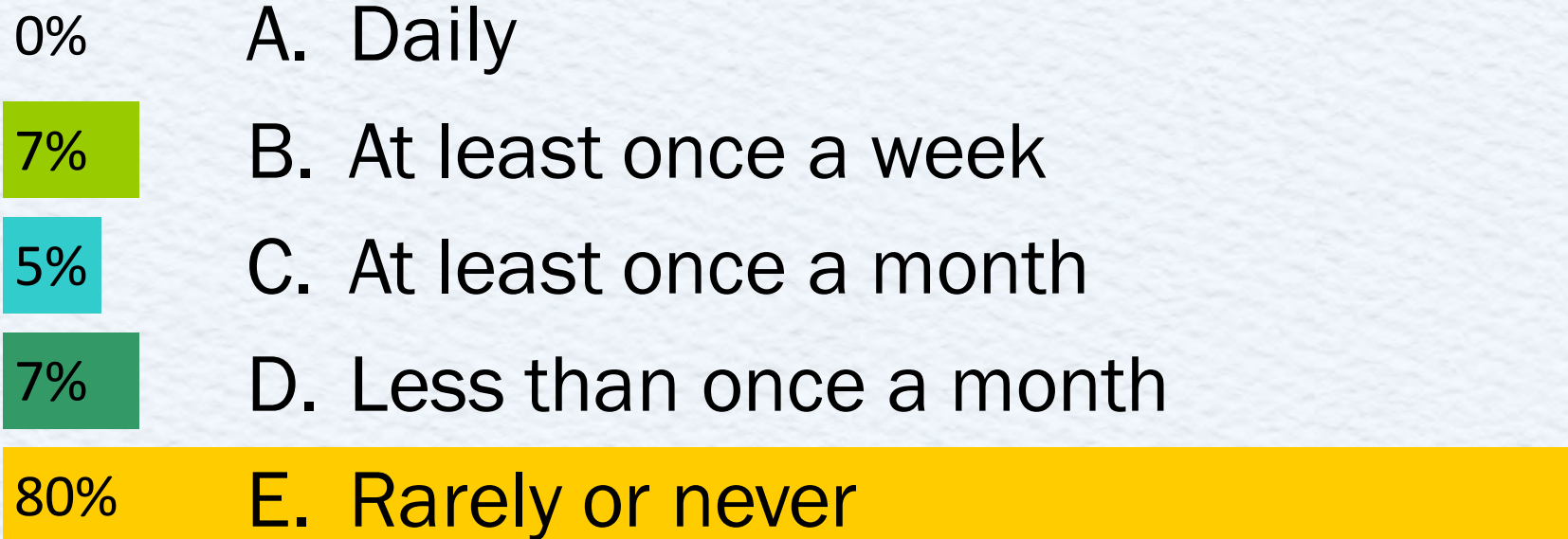
3%

D. Less than once a month

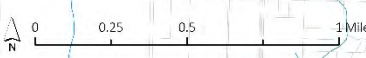
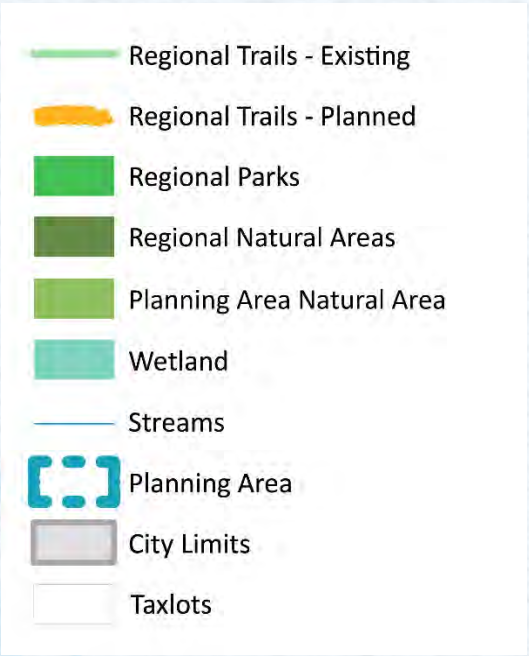
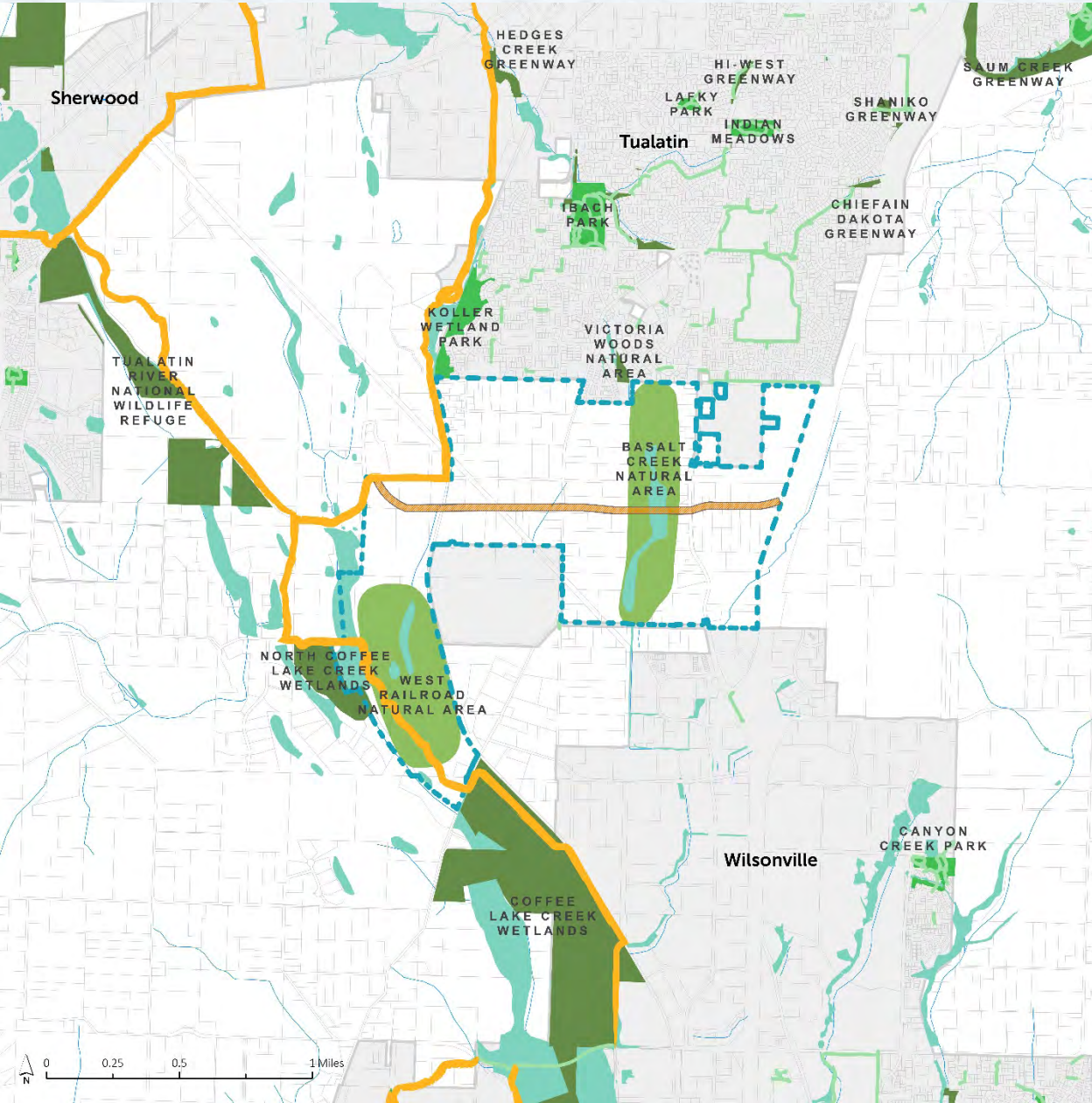
50%

E. Rarely or never

How often do you currently use transit?



Parks & Natural Areas



A variety of parks facilities and amenities are possible...



Neighborhood Park



15 to 20 acres

Pocket Parks



Less than an acre

Nature Interpretive Areas



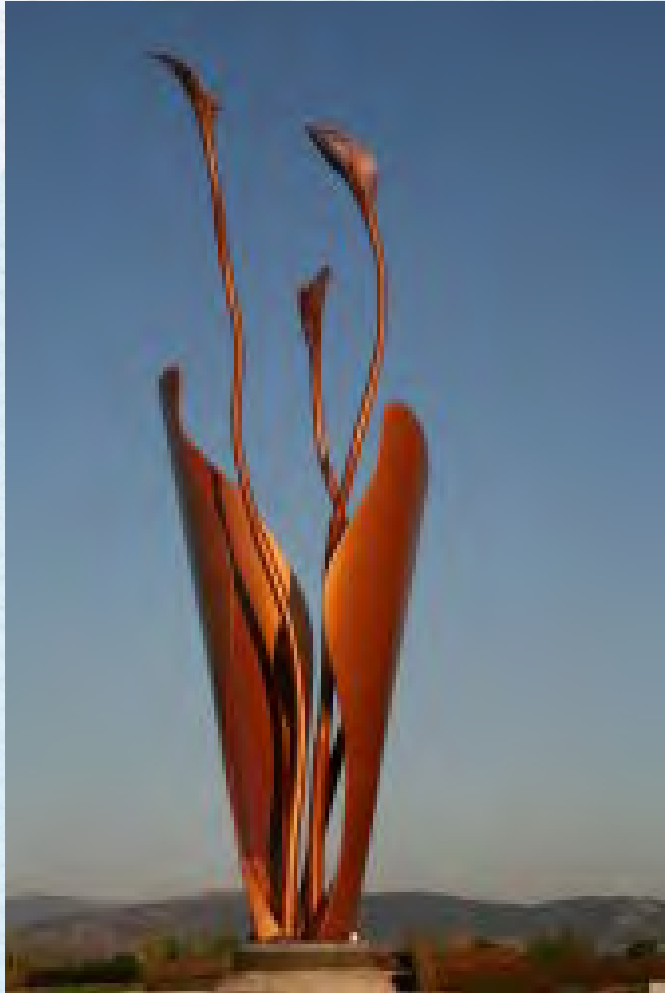
Conservation Areas



Outdoor Education



Public Art



Which type of amenity would you like best in Basalt Creek?

31% A. Neighborhood Park

5% B. Pocket Parks

26% C. Nature Interpretive Areas

18% D. Conservation Areas

3% E. Outdoor Education

3% F. Public Art

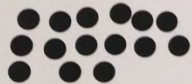
15% G. Other / Not sure

WHAT TYPES OF PARK FACILITIES & AMENITIES WOULD YOU LIKE TO SEE IN THE BASALT CREEK AREA?

PLACE A DOT STICKER BELOW THE FACILITY AND/OR AMENITY YOU PREFER!



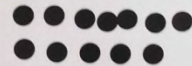
NEIGHBORHOOD PARK
15-20 ACRES



15



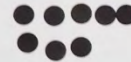
POCKET PARKS
< 1 ACRE



12



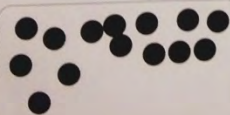
NATURE INTERPRETIVE
AREAS (E.G. CENTERS,
KIOSKS, OVERLOOKS)



8



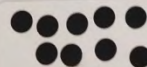
CONSERVATION AREAS



14



OUTDOOR EDUCATION



9

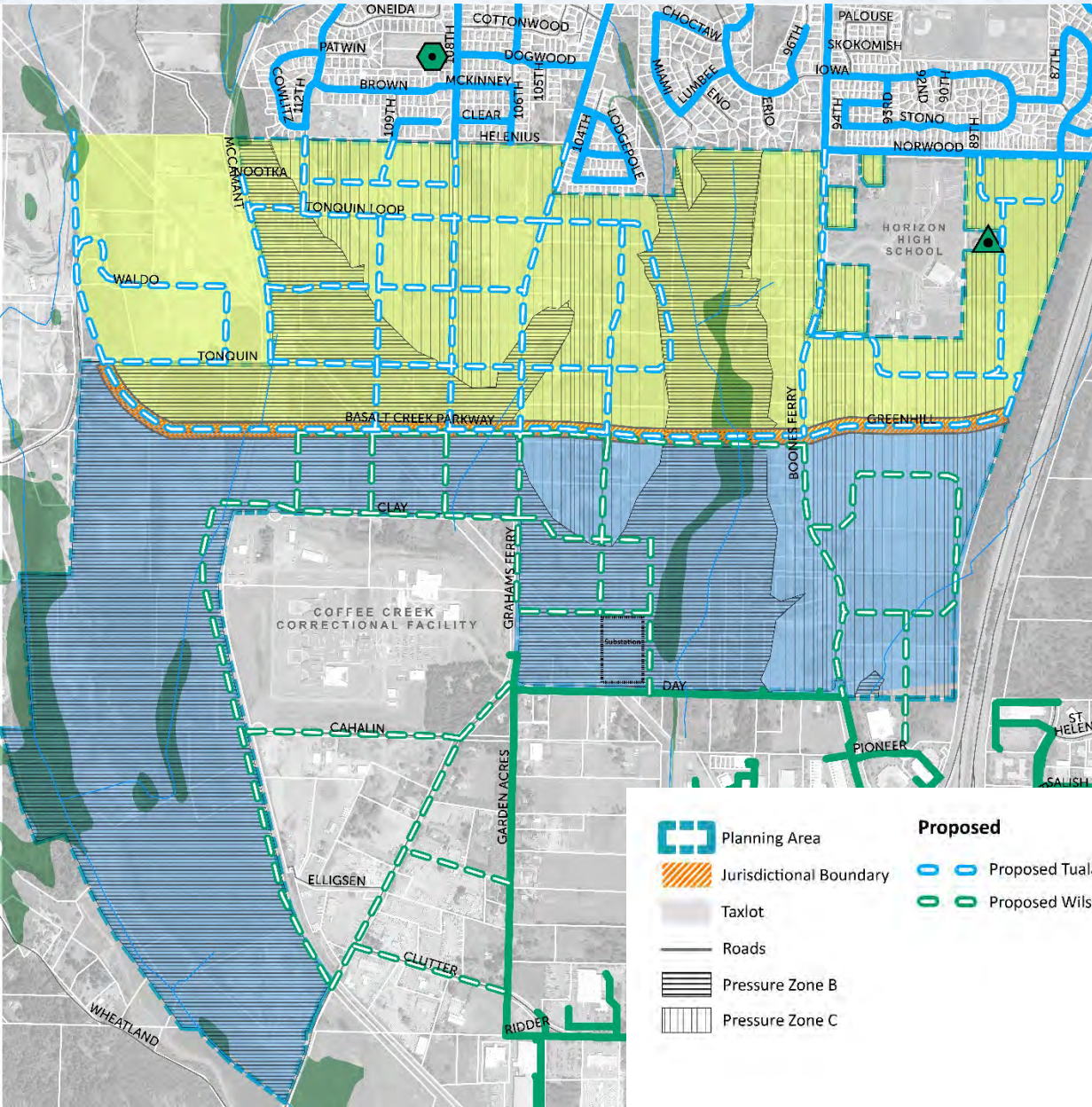


PUBLIC ART




4

Infrastructure: Water





 Planning Area

 Jurisdictional Boundary


 Taxlot


 Roads

 Pressure Zone B


 Pressure Zone C


Proposed


 Proposed Tualatin Water System


 Proposed Wilsonville Water System

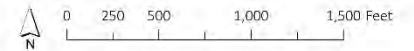
Existing

 Existing Tualatin Norwood PS and Reservoirs

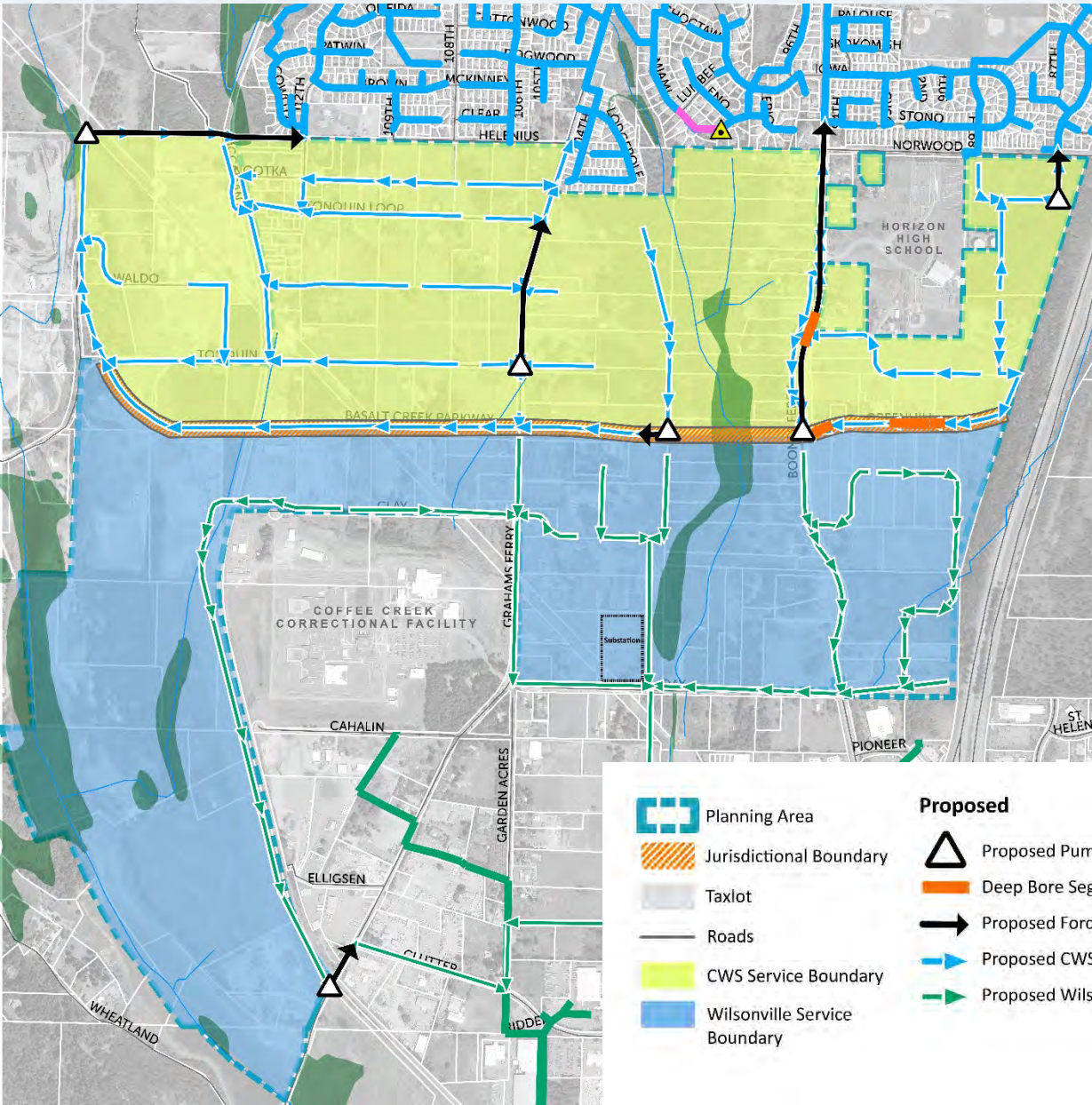
 Existing Tualatin ASR

 Existing Tualatin Water System






 Existing Wilsonville Water System







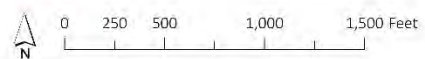
Infrastructure: Sanitary



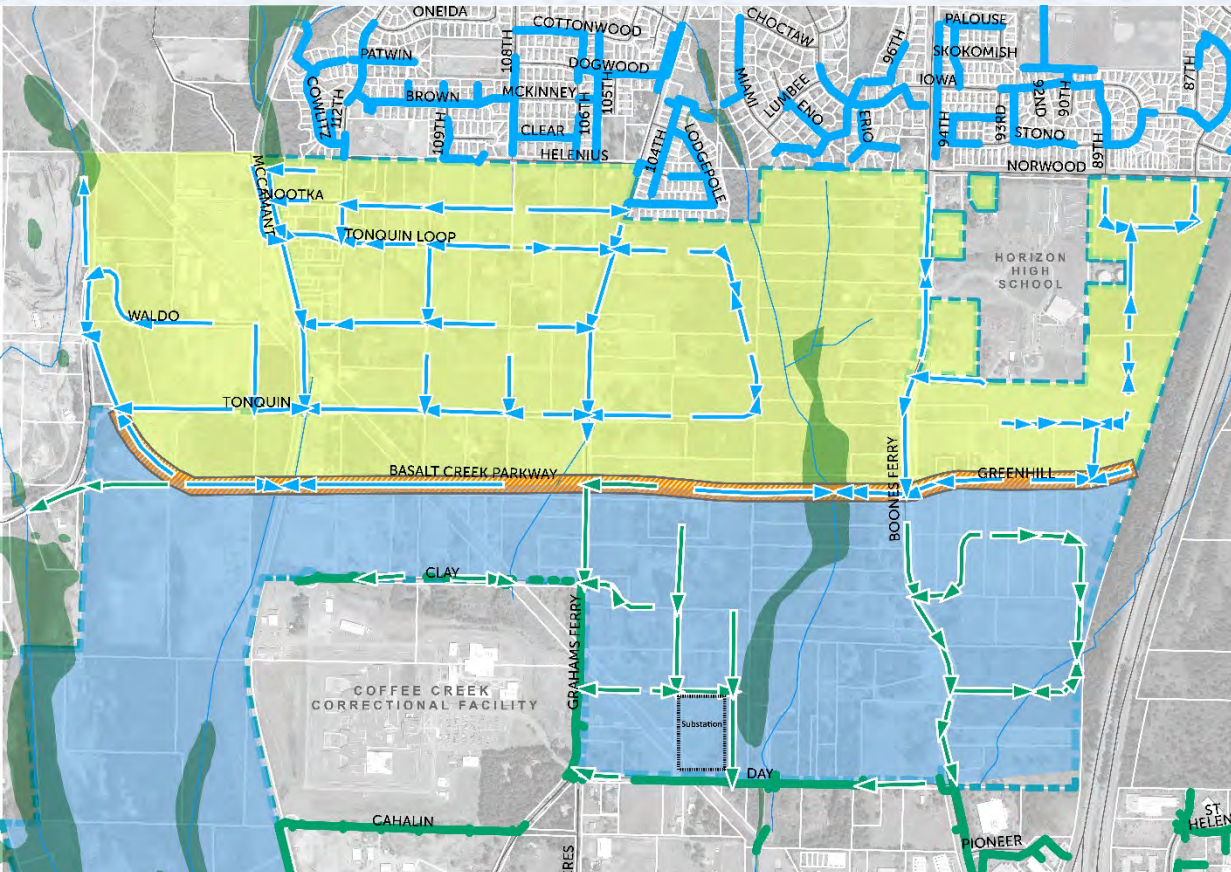
-  Planning Area
-  Jurisdictional Boundary
-  Taxlot
-  Roads
-  CWS Service Boundary
-  Wilsonville Service Boundary

- Proposed**
-  Proposed Pump Stations
 -  Deep Bore Segment (over 25' deep)
 -  Proposed Forcemains
 -  Proposed CWS Service System
 -  Proposed Wilsonville Service System



- Existing**
-  Existing CWS/Tualatin Pump Stations
 -  Existing CWS/Tualatin Gravity System
 -  Existing CWS/Tualatin Forcemains
 -  Existing Wilsonville System





Infrastructure: Stormwater



-  Planning Area
-  Jurisdictional Boundary
-  Taxlot
-  Roads
-  CWS Service Boundary
-  Wilsonville Service Boundary
-  Wetland
-  Streams

- Proposed**
-  Proposed Tualatin Stormwater System
 -  Proposed Wilsonville Stormwater System

- Existing**
-  Existing Tualatin Stormwater System
 -  Existing Wilsonville Stormwater Mainlines



What will be in the Concept Plan?

- The planning process
- Considerations for success
- Land use plan
- Service plan for water, stormwater and sewer
- Transportation plan
- Implementation strategies

Next Steps

- Team is working on the Draft Concept Plan
- Present Draft for Council Feedback
- Finalize Concept Plan

- Additional Documents that will be developed:
 - Metro Regional Framework Plan Memo
 - Draft Comprehensive Plan Amendment for Wilsonville
 - Draft Comprehensive Plan Amendment for Tualatin
 - Intergovernmental Agreements for a variety of topics

Thank You!

Further questions or comments:

www.basaltcreek.com

Aquilla Hurd-Ravich, project manager

ahurd-ravich@ci.tualatin.or.us

503-691-3028

Miranda Bateschell, project manager

bateschell@ci.wilsonville.or.us

503-570-1581

NAME

EMAIL OR ADDRESS

JERRY SHARROW SHARP

JERRYSHARP@FRONTIER.CO

Mi Decker

meggiid@comcast.net

ERIC ROUSE

erouse@SummitDevelopment
Group.com

Kelsey Lewis

klesw@ci.tualatim.or.us

Brent Beebe

brent.beebe@gmail.com

Kevin Ferrasi Omalley

kferrasiomalley@gmail.com

Kaum Peil Fox

kper18@gmail.com

Kristin Akervall

kristin.akervall@gmail.com

ROBERT KELLOGG

robertekellogg@yahoo.com

JACKIE ROO

jackie_roo@hotmail.com

Basalt Creek Concept Plan



Open House

You are invited to a Basalt Creek Open House.

Come learn about the preferred land use plan, parks and open space, pedestrian, bike and transit networks. The project team will be available to answer questions.

**We invite the public to attend an
Open House:**

**Thursday, April 28, 2016
5:30-7:30 pm**

**Juanita Pohl Center
8513 SW Tualatin Rd.
Tualatin, Oregon**



City of Tualatin

The Basalt Creek Concept Planning area is generally located between the southern edge of Tualatin and the northern boundary of Wilsonville. Because concept planning is a joint effort between Tualatin and Wilsonville, the Cities maintain an independent website for the project at www.basaltcreek.com.



City of Tualatin's project contact is **Aquilla Hurd-Ravich**, Planning Manager, 503.691.3028.

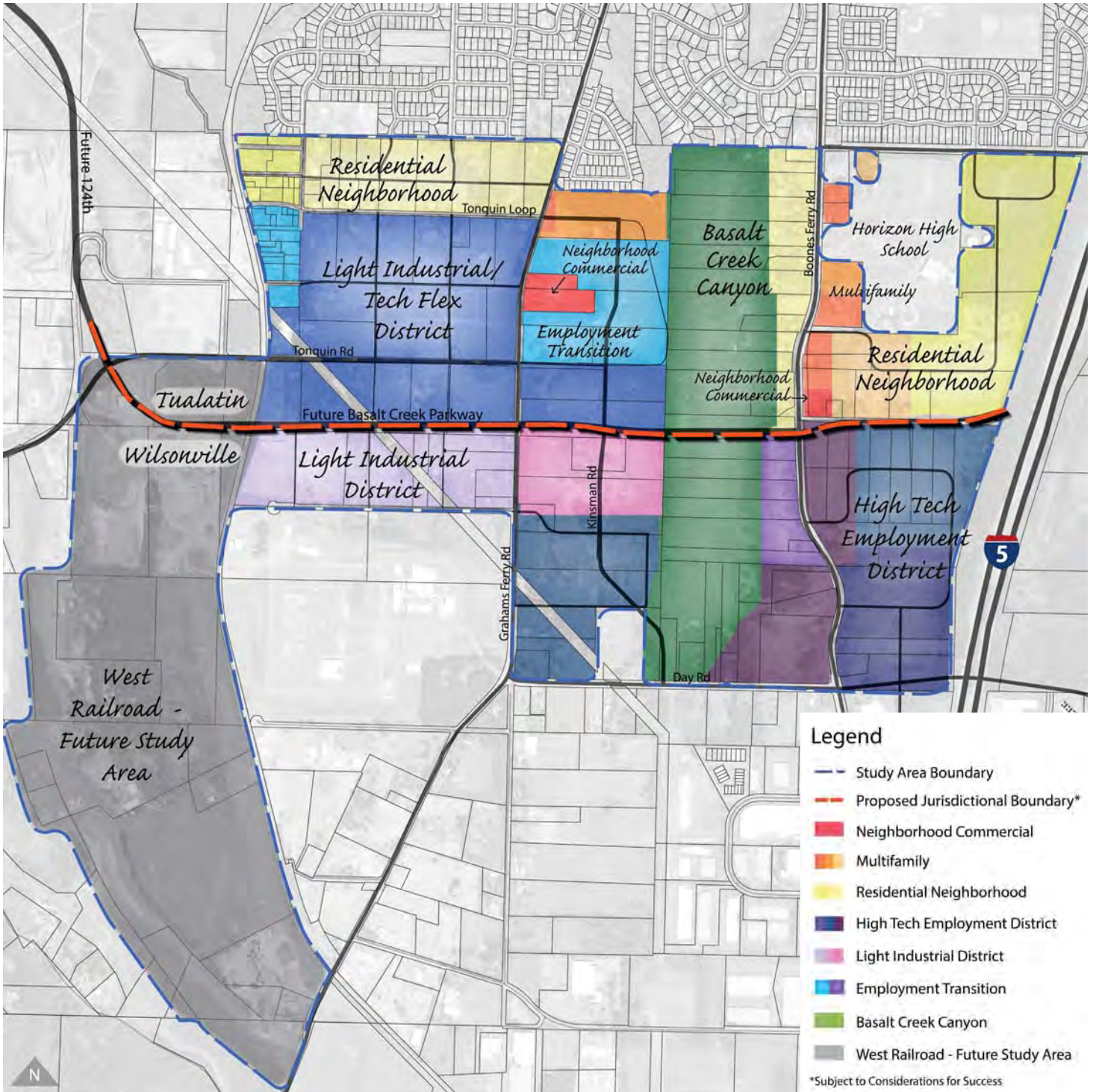
City of Wilsonville's project contact is **Miranda Bateschell**, Long-Range Planning Manager, 503.570.1581.



www.basaltcreek.com

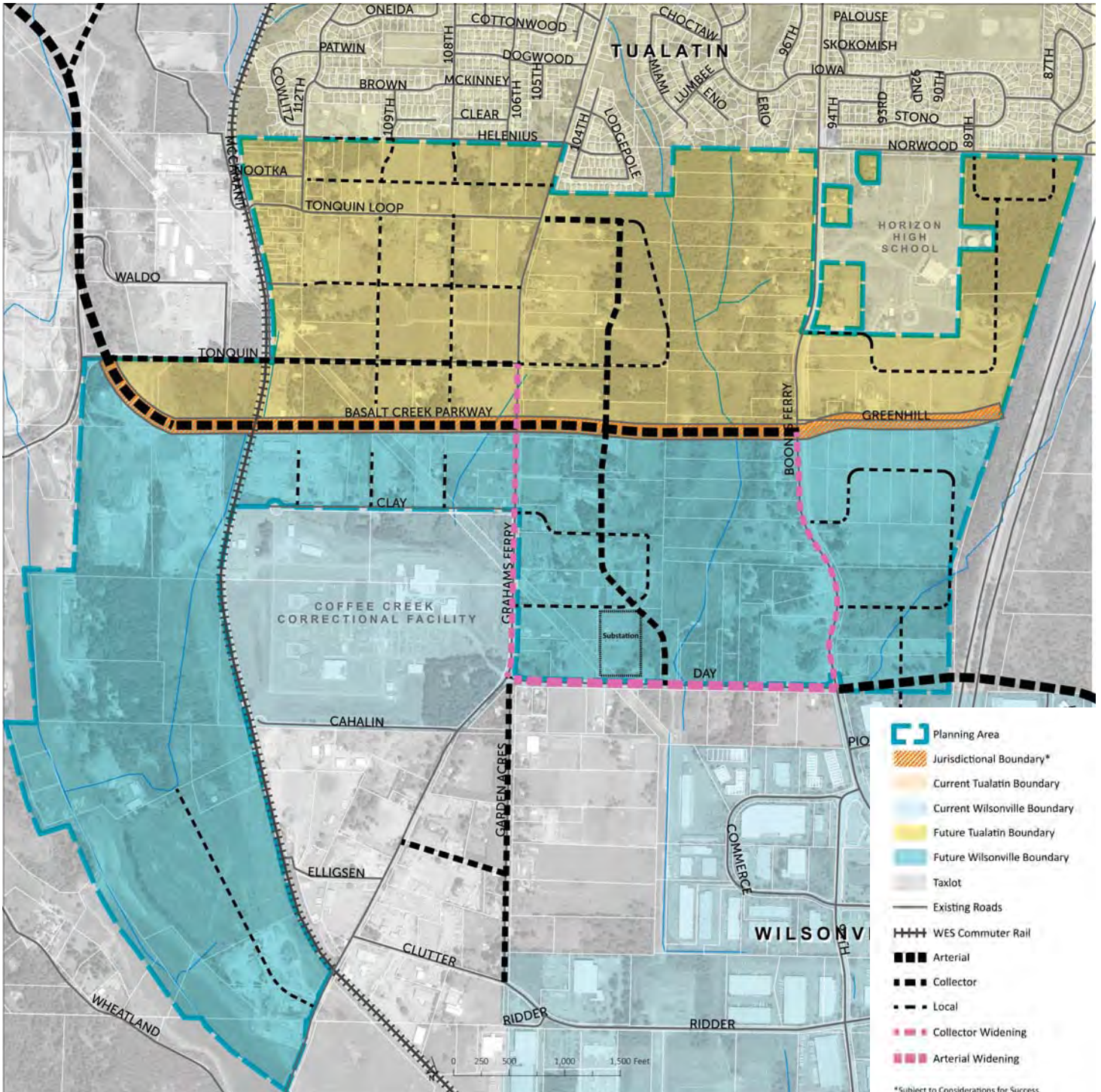
Basalt Creek Concept Plan

THE LAND USE CONCEPT



Basalt Creek concept Plan

THE ROAD NETWORK CONCEPT

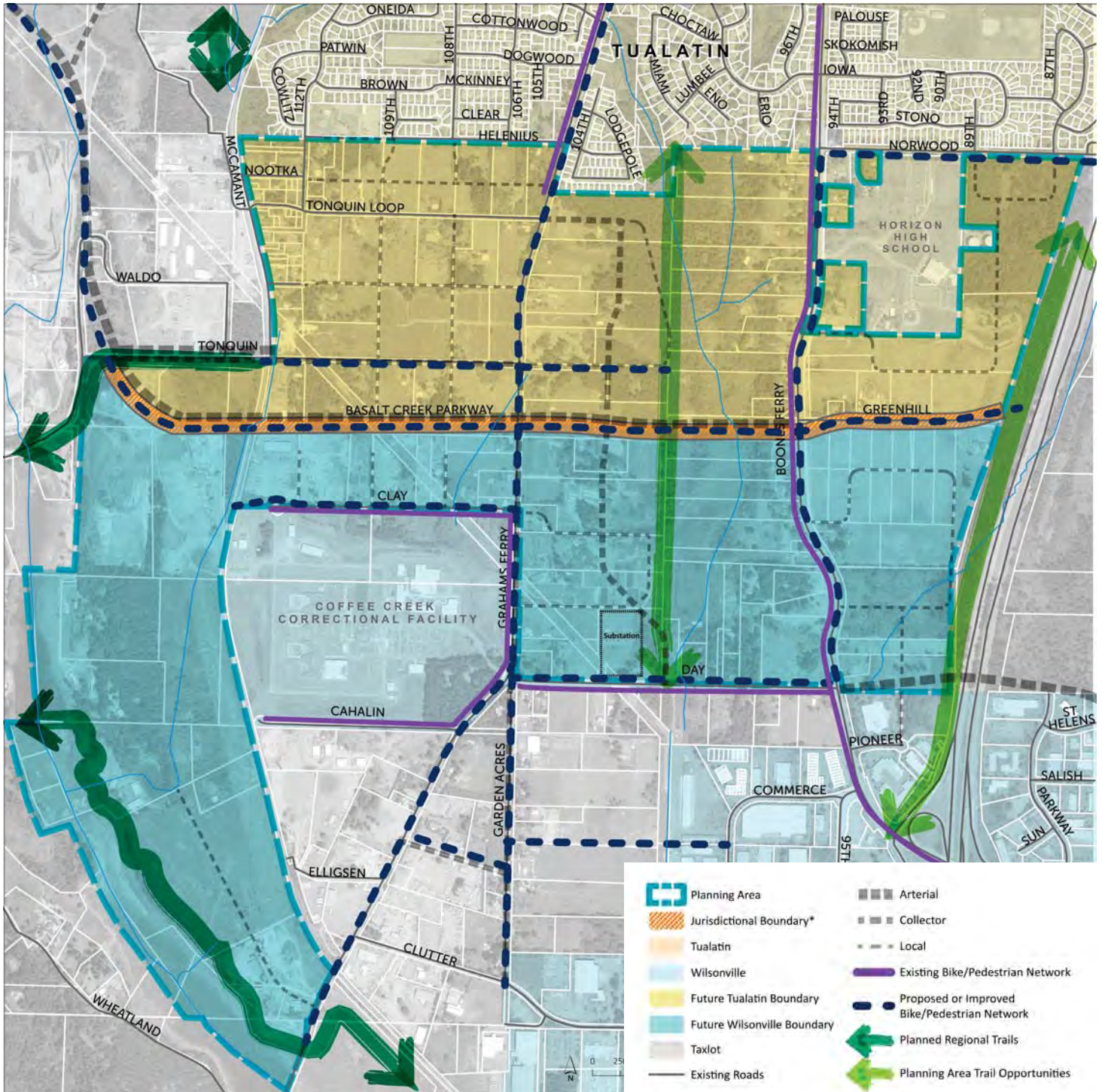


- Planning Area
- Jurisdictional Boundary*
- Current Tualatin Boundary
- Current Wilsonville Boundary
- Future Tualatin Boundary
- Future Wilsonville Boundary
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- WES Commuter Rail
- Arterial
- Collector
- Local
- Collector Widening
- Arterial Widening

*Subject to Considerations for Success

Basalt Creek Concept Plan

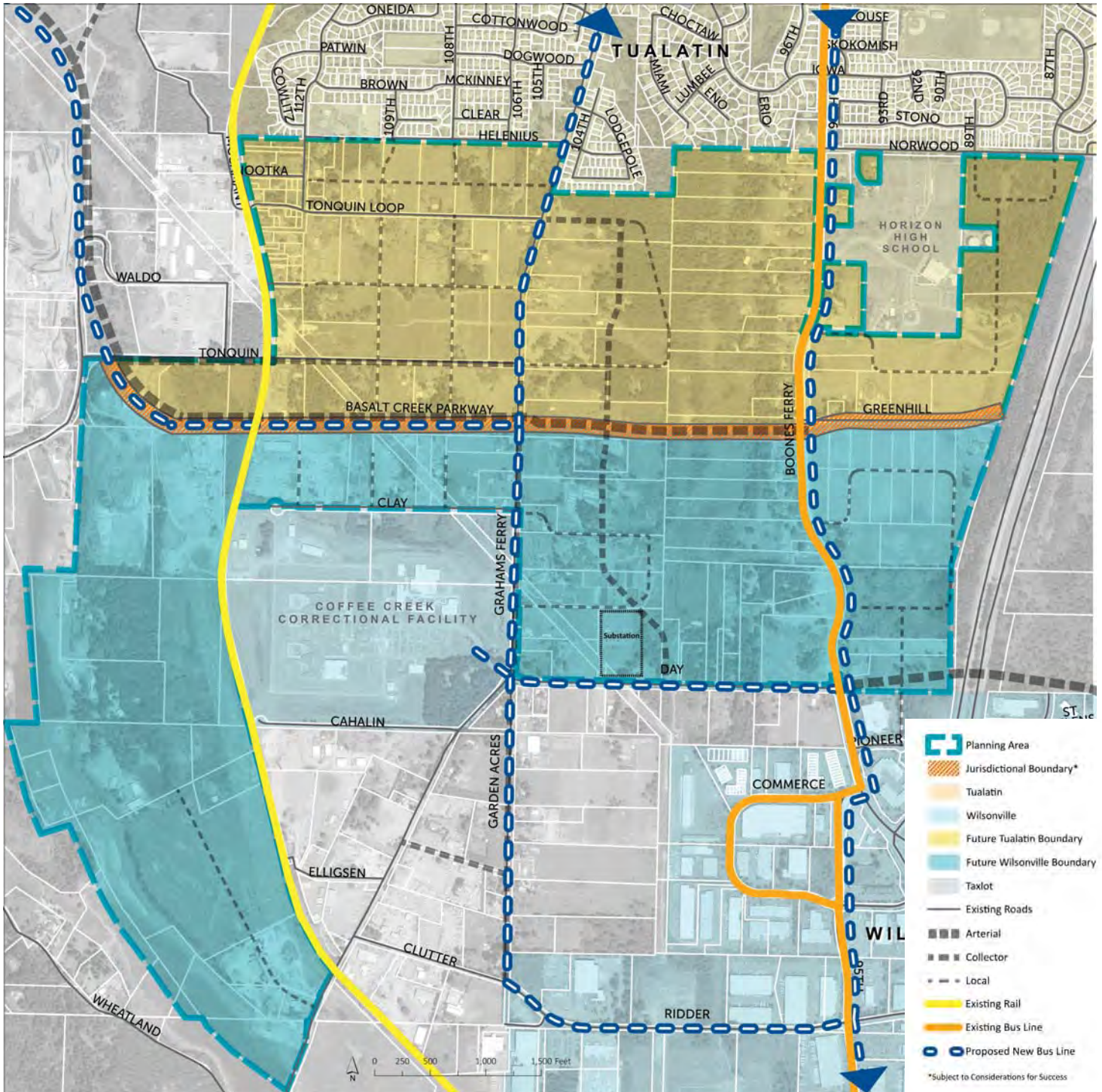
CONCEPTUAL BIKE, PEDESTRIAN & TRAIL NETWORK



*Subject to Considerations for Success

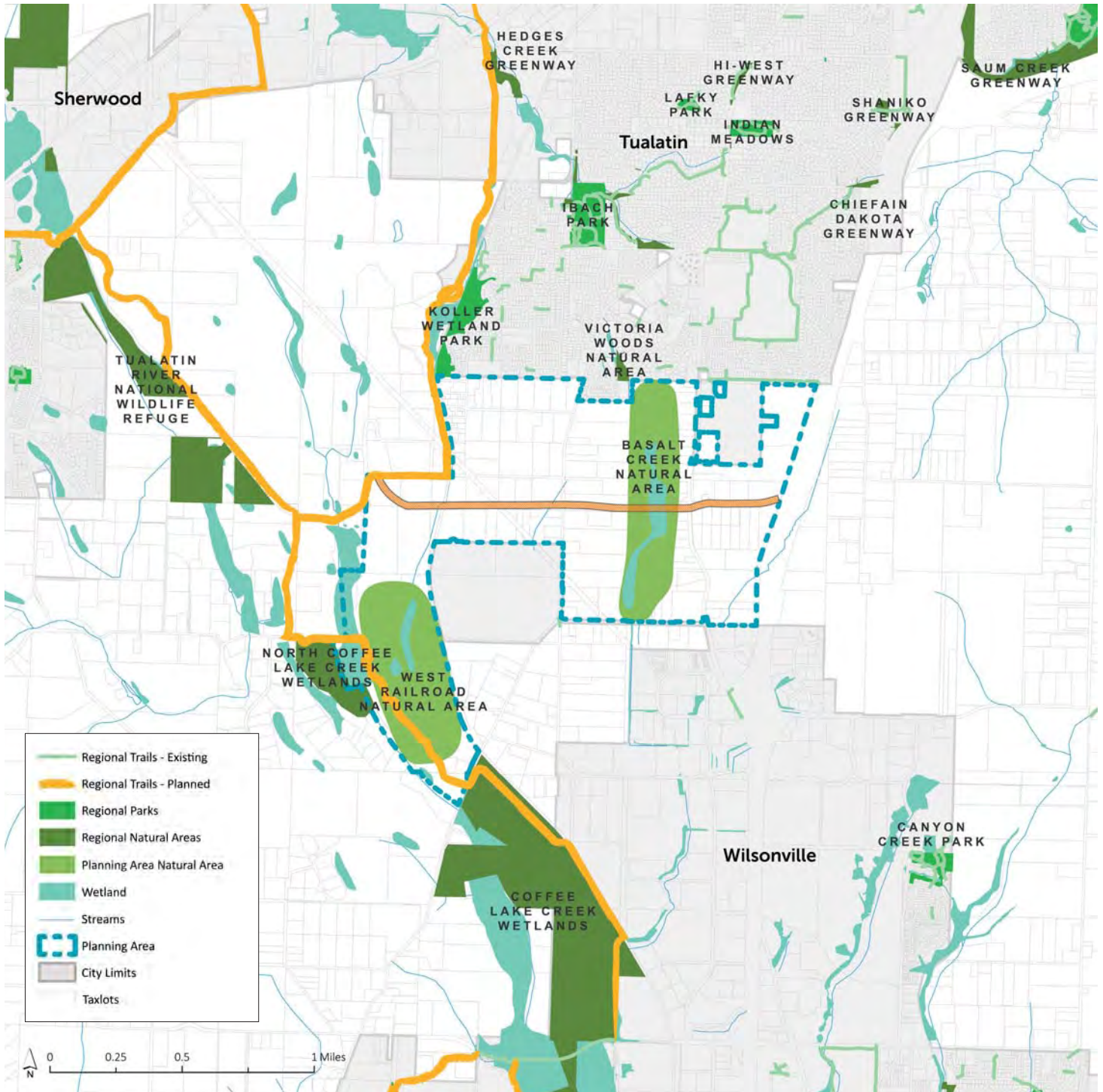
Basalt Creek Concept Plan

CONCEPTUAL TRANSIT NETWORK



Basalt Creek Concept Plan

EXISTING REGIONAL PARKS & NATURAL AREAS



WHAT TYPES OF PARK FACILITIES & AMENITIES WOULD YOU LIKE TO SEE IN THE BASALT CREEK AREA?

PLACE A DOT STICKER BELOW THE FACILITY AND/OR AMENITY YOU PREFER!



**NEIGHBORHOOD PARK
15-20 ACRES**



**POCKET PARKS
< 1 ACRE**



**NATURE INTERPRETIVE
AREAS (E.G. CENTERS,
KIOSKS, OVERLOOKS)**



CONSERVATION AREAS



OUTDOOR EDUCATION



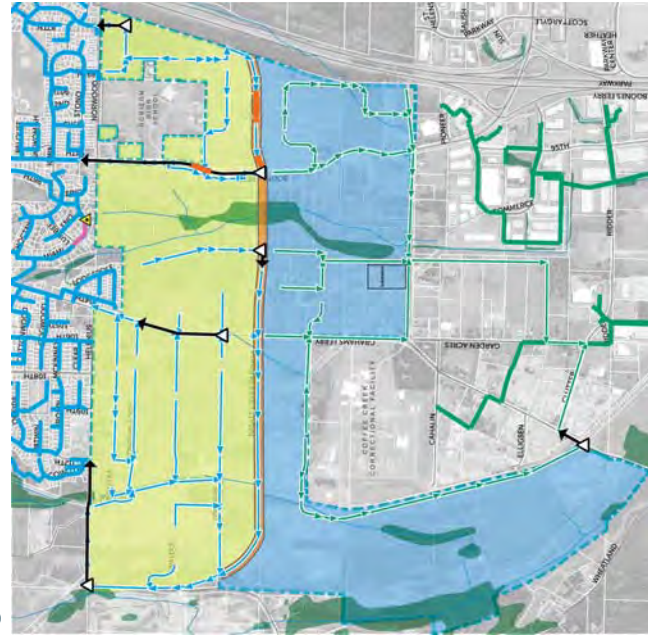
PUBLIC ART

Basalt Creek Concept Plan

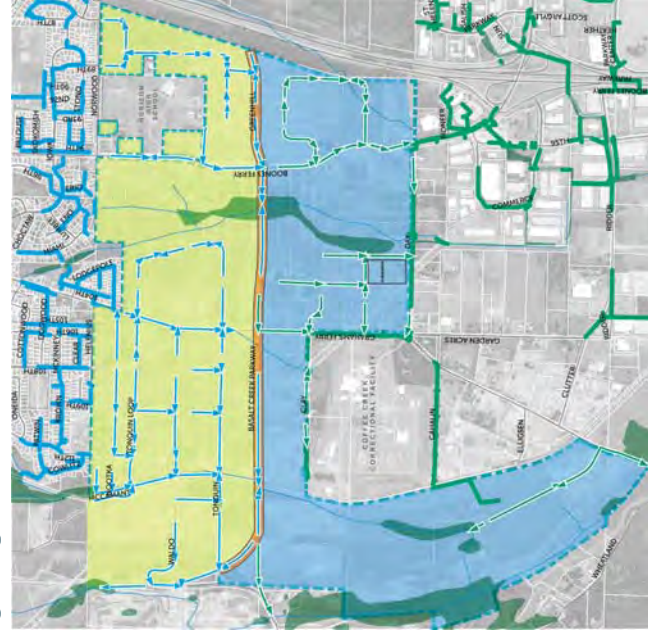
INFRASTRUCTURE SANITARY, STORMWATER & WATER



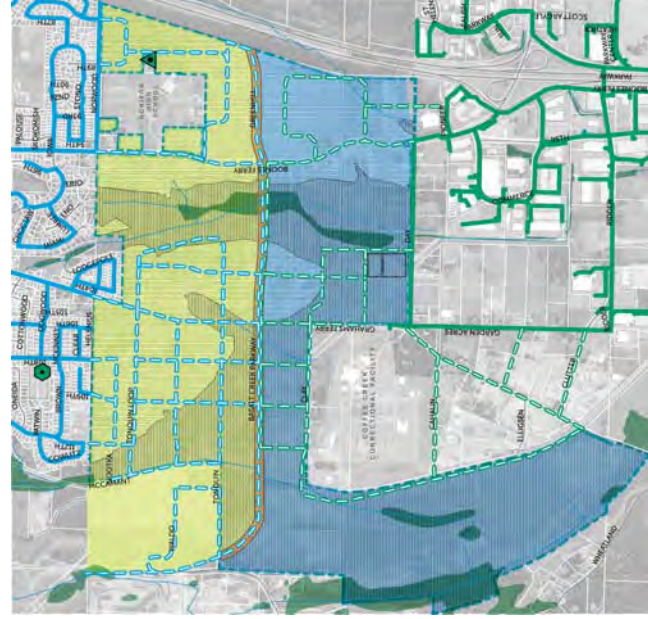
SANITARY



STORMWATER



WATER



MEMORANDUM

TO: BASALT CREEK PROJECT MANAGEMENT TEAM (PMT)
FROM: LEILA AMAN, FREGONESE ASSOCIATES
SUBJECT: APPROACH FOR JUNE 17TH BASALT CREEK PUBLIC WORKSHOP
DATE: JUNE 12TH, 2014

The purpose of this memo is to outline proposed activities and presentation content for the June 17th Basalt Creek Concept Plan public workshop in order to solicit feedback from the PMT. Content includes:

- I. Workshop Agenda
 - II. Detailed Workshop Agenda
 - III. Facilitator and Note-Taker Roles
-

I. Workshop Agenda

The June 17th Workshop will last for 2.5 hours, and will consist of the following activities (with expected duration noted):

Goals and Objectives

1. Orient stakeholders to project - tell the story of the Basalt Creek Area and describe the planning framework
 - a. Role of participants
 - b. Constraints on different land uses
 - c. Planning boundaries and assumptions
2. Engage the public in a dynamic dialogue about planning for the Basalt Creek Area
3. Understand the individuals' goals and desires for the area so they can be incorporated into the planning process and communicated to elected officials
4. Increase future participation by the public in the Basalt Creek planning process

Schedule

5:45 – 6:10 *Welcome/sign-in [25 min]*
6:10 – 6:15 *Introductions [5 min]*
6:15 – 6:35 *Background presentation [20 min]*
6:35 – 7:00 *Instant polling [25 min]*
7:00 – 8:00 *Mapping Exercise [60 min]*
8:00 – 8:30 *Wrap-up/Questions & Comments [20 min]*

II. Detailed Workshop Agenda

Set-up [5:00 – 5:45] (All)

- Set up sign-in table with agendas, nametags, pens, sign-in sheets, comment cards, and “where do you live/work/own property” map for people to put stickers on (Erica + 1)
- Arrange and distribute materials to tables. Each table will be set-up with instant polling devices and map exercise materials for up to 10 participants (Erica +1)
- Set out refreshments (PMT)
- Set up and test projector/PowerPoint presentation (FA) (Leila)
- Post directional signage to help participants find the site / room (PMT)
- Facilitator and note-taker briefing 5:15 – 5:30 (John and all facilitators/note takers)

Welcome/Sign-in [5:45 – 6:10] – any volunteers to staff the sign-in table? Having 2 people on hand would be great.

- Upon arrival, participants will be asked to sign in and wear a nametag
- We will have a map for people to put a dot on where they live/ work?
- Participants will be asked to select a seat so as to have at least six people per table (rather than spreading out and leaving groups of 2-3 at each table)
- Encourage sitting with some “new” friends.
- Refreshments will be available for attendees to serve themselves and bring to their table (provided by the PMT)

Introductions [6:10 - 6:15]

- Introductions of consultant team and public agency staff (John)

Background Presentation [6:15 – 6:35] (John)

- Presentation will be brief and straightforward, using minimal text with supporting graphics to tell the story of the Basalt Creek area and objectives of the planning process (including project timeline, public involvement plan, planning context, and existing conditions).
- Opportunities for public involvement will be highlighted
- Outline assumptions that are “set” and issues that are up for discussion/the focus of this process. It will be important for attendees to hear that this workshop is not revisiting the E-W connector alignment, the UGB boundary, whether the area will urbanize over time etc.
- Present guiding principles. We are asking workshop participants to help solve a problem (as defined by our councils)

Instant Polling [6:35 – 6:55] (John)

- At end of presentation, PowerPoint slides will transition into instructions for instant polling
- Set of approximately 20 multiple choice questions will ask stakeholders to rank project priorities (sourced from Guiding Principles)
- Questions and responses are integrated into the PowerPoint using TurningPoint software

- Polling results are collected using clickers – remote devices that send instant polling results to the computer of the presenter
- Tallied results will be displayed immediately on the screen
- The general flow of questions will be as such:
 - Icebreaker questions
 - Demographic questions
 - General questions about types of housing and employment desirable in area
 - Questions to measure level of agreement with Guiding Principles

Map Exercise [6:55 – 7:55] (John, Facilitators and Note-takers)

At the conclusion of instant polling, slides will be presented explaining the map exercise, including the exercise goals, parameters, limits and expectations for the area. Table-sized maps of the planning area will function as mechanisms for workshop participants to communicate their ideas about what kind of changes they would like to see occur. At least one facilitator and one note-taker (project team staff) will be present at each table.

Materials

At each table, there will be:

- Table number (1x6)
- Map Exercise Instructions for each participant (10x6)
- A map “atlas” for with reference maps of the area containing contextual information. (3x6)
These maps will include:
 - Development Context
 - Transportation
 - Trails, Natural Areas and Open Space
- One table-sized basemap of the planning area, including alignment of future roadways based on the Basalt Creek TRP (1x6)
- Markers and pens (several per table)
- Development chip sets (3x6)
- Scissors and glue (a few per table)

Map Exercise Description

This workshop will use large, table-sized maps that accurately represent the current physical features of the Basalt Creek Planning Area. The maps are the mechanism for stakeholders to communicate their ideas in the context of existing constraints and project commitments. While discussing goals for the area among small groups, participants may write, label, draw, sketch, and stick any ideas on the map or in the margins.

The following steps are guidelines to make sure groups have enough time to communicate ideas to the project team. Groups may spend more or less time on a task if needed; the facilitator and note-taker at the table will help pace the activity and keep track of time.

Step 1: Introductions (5 min)

In small groups of 6-10, individuals will have the opportunity to introduce themselves and share with the group what he or she hopes to accomplish with this exercise.

Step 2: Familiarize with the materials (5 min)

Base map, pens, reference maps and development chips.

Step 3: Identify the group's goals for the workshop map (15 min)

Goals can include anything that has to do with land use, employment, recreation, development, housing and transportation in the Basalt Creek area. Facilitators will help groups to understand which types of goals might be more salient than others, and to consider that suggestions for all-or-nothing-style approaches should be tempered with an appreciation of political realities. Groups might spend ten minutes talking and five minutes writing its goals on the map. Not everyone has to agree, but establishing common ground among group members can be encouraged by the facilitator.

Step 4: Place development chips on the map (15 min)

Groups may use the next fifteen minutes or so to place development chips on the map representing group members' desires for the Basalt Creek area. People should feel free to write questions or comments on the map as needed to clarify the concepts they are illustrating and the locations they pertain to.

Step 5: Draw in transportation infrastructure beyond the BCTRP- transit, local connector roads, trails, and bike lanes (10 min)

This can be done simultaneously with chip placement. Markers provided at the tables can be used by participants to draw in different types of transportation infrastructure, services and connections. People should feel free to write questions or comments on the map as needed to clarify the concepts they are illustrating and the locations they pertain to.

Step 6: Report Out (20 min)

Groups may spend a few minutes choosing a group member to present the map to the larger group. Presenters will spend five minutes in front of the larger group presenting their group's map. They will describe their group's main goals for the exercise, and briefly describe the strategy and resulting chip placement (and/or drawings) used to illustrate how those goals might be achieved in the Basalt Creek area.

Workshop Wrap-Up [7:55 – 8:15]Concluding Comments (John)

Staff from the project team may give a brief summary of some of the major themes and concepts identified by groups through the map exercise. Next steps and opportunities for engagement will be presented. Participants will be encouraged to approach project team staff with questions or comments about the project. Maps will be collected, and the instant polling session file will be saved to a jump drive for later analysis.

Break-Down (All)

- Make sure all maps are labeled by table/group number before they are collected
- Collect sign-in sheet and any participant contact information provided
- Make sure to save TurningPoint session before closing PowerPoint presentation. Save back-up of TurningPoint session in second location.

III. **Facilitator and Note-Taker Roles**

Facilitator Role

Project team staff will act as neutral facilitators (one per table) for the small group map exercise in the second portion of the workshop. They will help familiarize participants with the materials and the process, answer questions, assist in resolving conflicts, and guide the group toward completing the tasks within the allotted time. Facilitator responsibilities may include:

- *Acting as a “scribe” for the group.* The facilitator, along with the note-taker, may serve as a graphic hand, helping to make sure comments get recorded—especially those of quieter group members, or if groups are hesitant to mark up the map. Others in the group can and should draw and write on the map as well.
- *Factual clarification.* Facilitators may provide technical information as well as background information about different planning concepts. This may help the group to move through the tasks at hand, rather than spending time debating technical questions. The primary job of the facilitator will be to make sure the group is focusing on the difficult question of what is appropriate for the area over the long term.
- *Moving the discussion along.* With 6 to 10 people around a table, and a limited amount of time, it can be difficult to reach consensus. The facilitator will keep things rolling along when conversation gets bogged down.
- *Mediation.* The facilitator will work to ensure that side conversations or personality conflicts do not detract from the group’s ability to focus on the task at hand.
- *Timekeeper.* The facilitator will track the time and make sure that the group stays on schedule

Note-Taker Role

- Capture overarching themes and patterns of spoken comments on a large easel so group members can track the direction of the conversation throughout the exercise
- Record unique or novel comments and observations
- Note the extent to which particular concepts are supported or challenged by group members (both quantitatively and qualitatively)
- Ask for participants to repeat or clarify statements, when necessary, to accurately capture their sentiments

Basalt Creek concept Plan



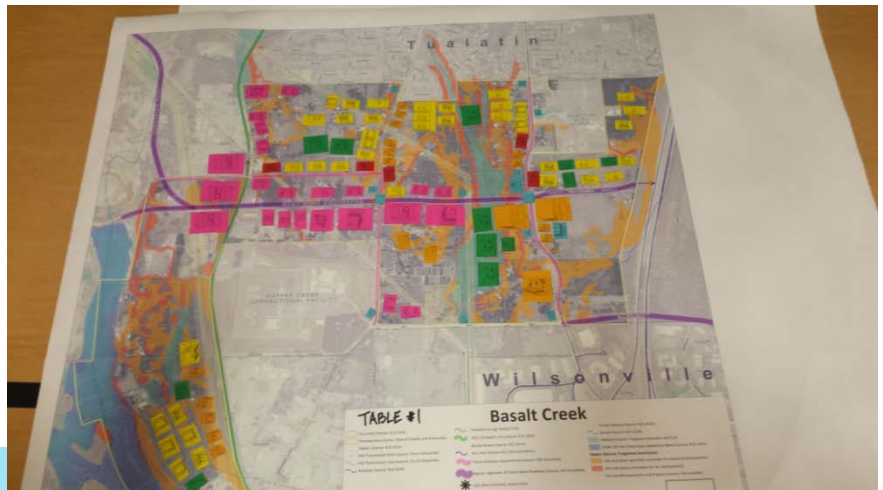
June 17th Public Workshop Results



Workshoppers in Action



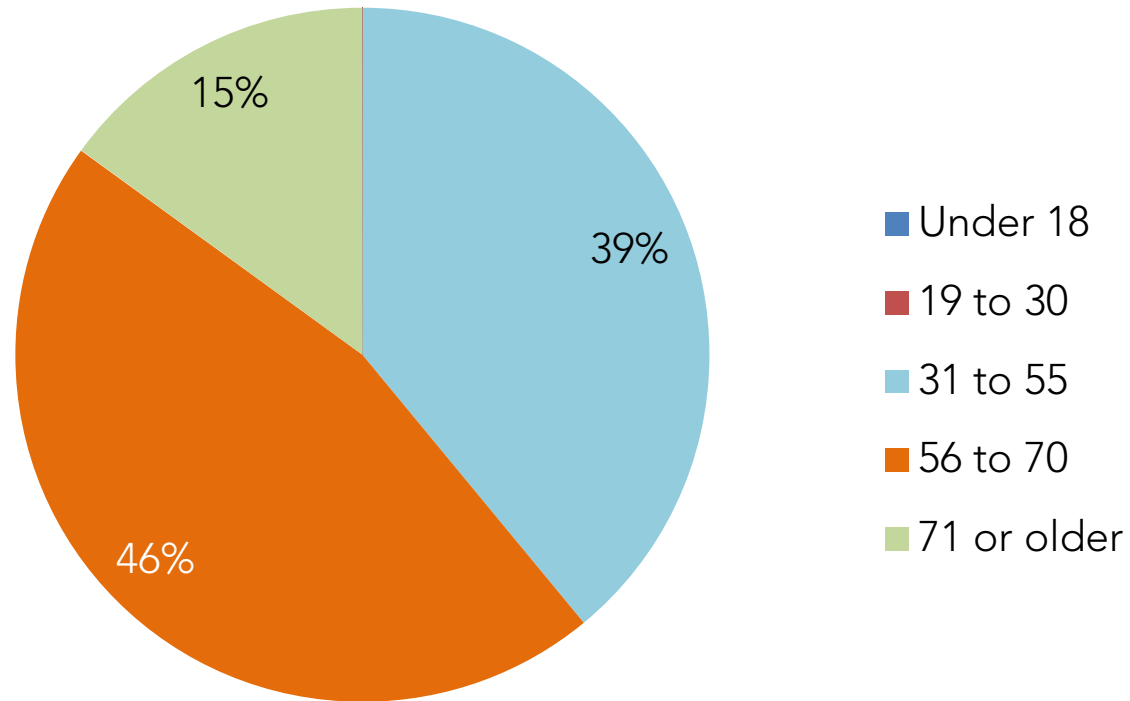
Workshoppers in Action



Instant Polling Results

Workshop Results

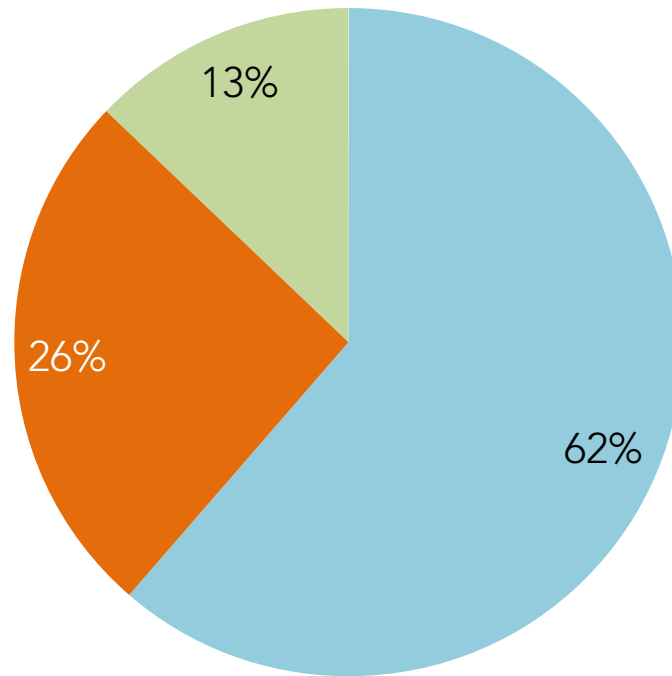
How old are you?



N = 42

Workshop Results

Have you participated in a public workshop before?



N = 39

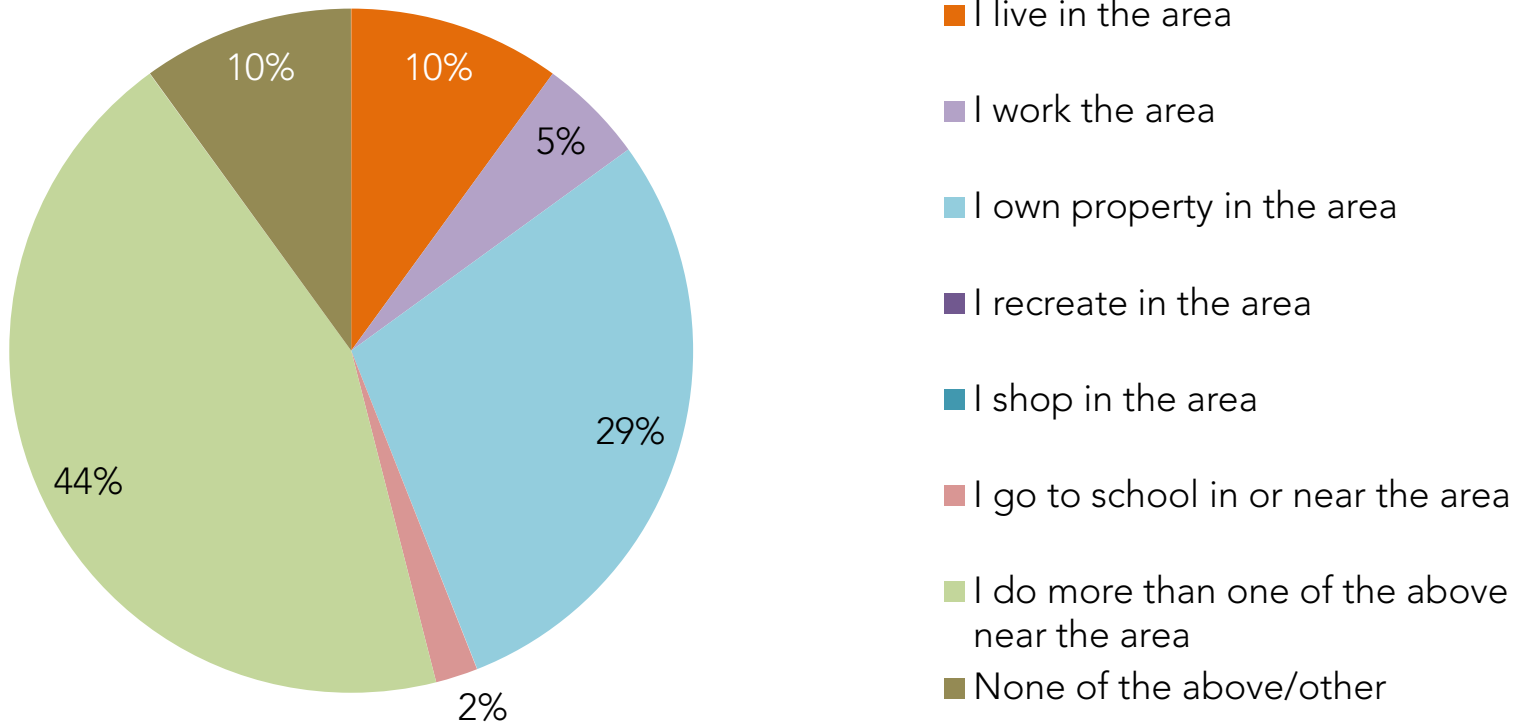
■ Yes - I love these things!

■ Nope, this is my first time

■ Not sure

Workshop Results

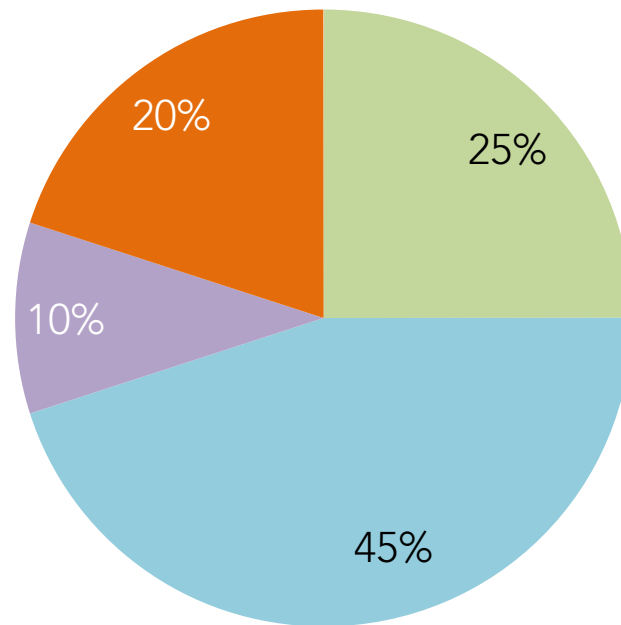
What is your primary connection to the Basalt Creek Planning Area?



N = 41

Workshop Results

Where do you live?

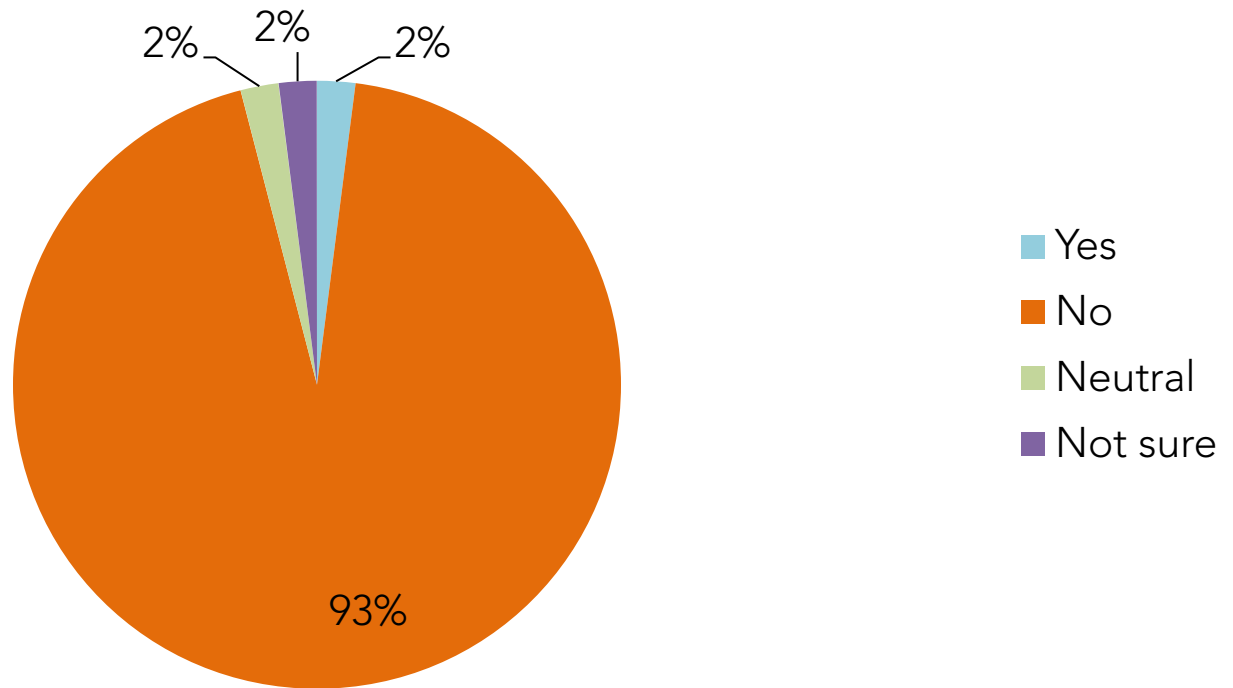


N = 40

■ The Basalt Creek planning area ■ Tualatin ■ Wilsonville ■ Other

Workshop Results

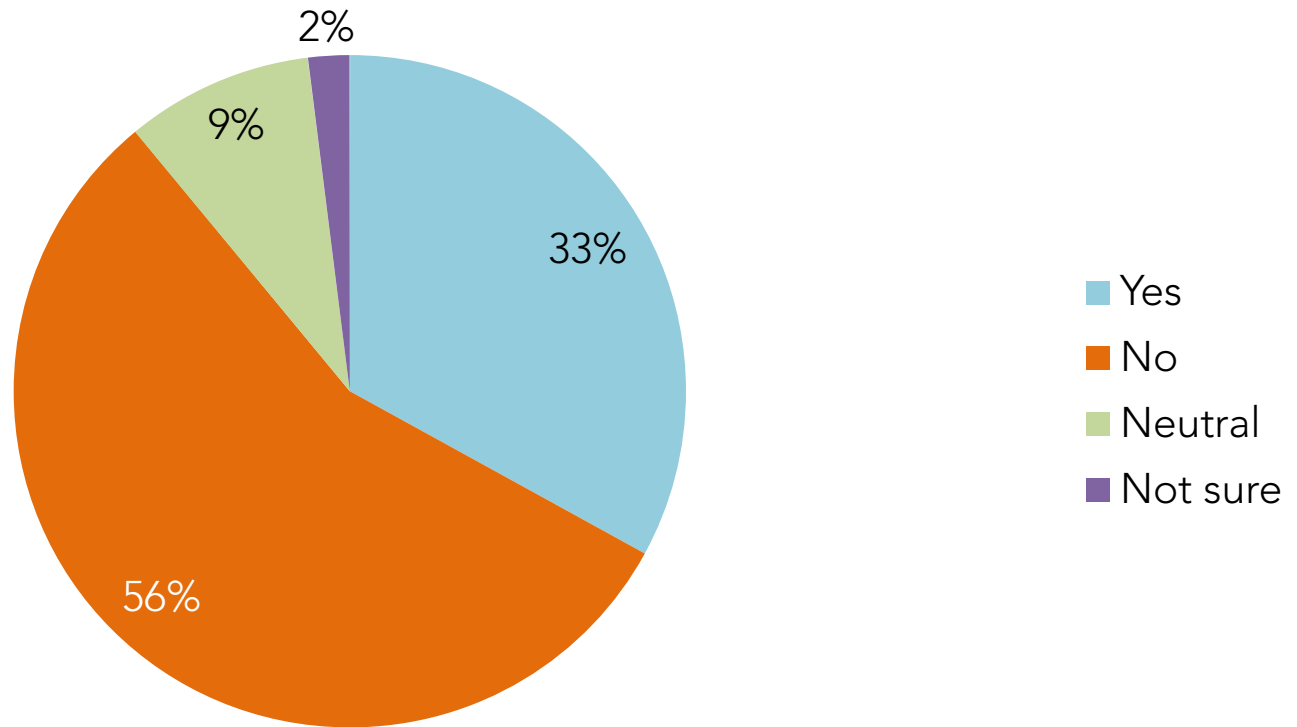
Appropriate for Basalt Creek?
Box retail (large-scale/auto-oriented)



N = 41

Workshop Results

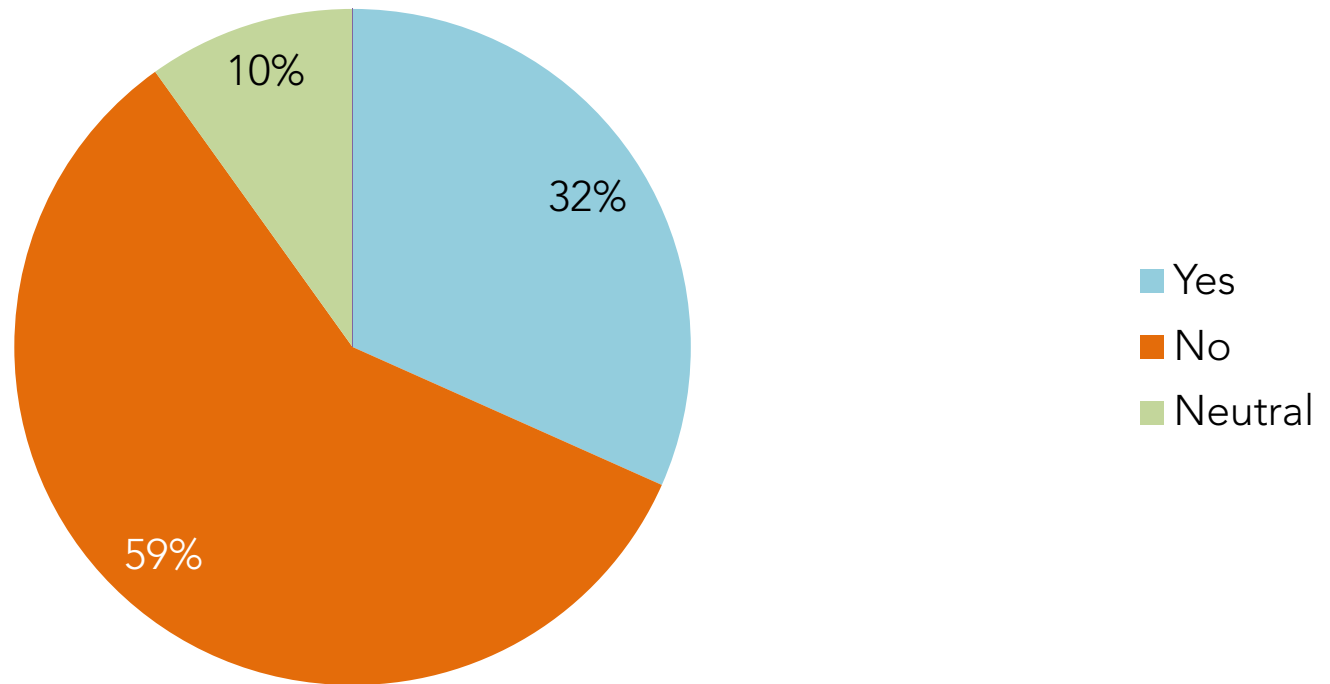
Appropriate for Basalt Creek?
Contractor's Supplies and Services



N = 43

Workshop Results

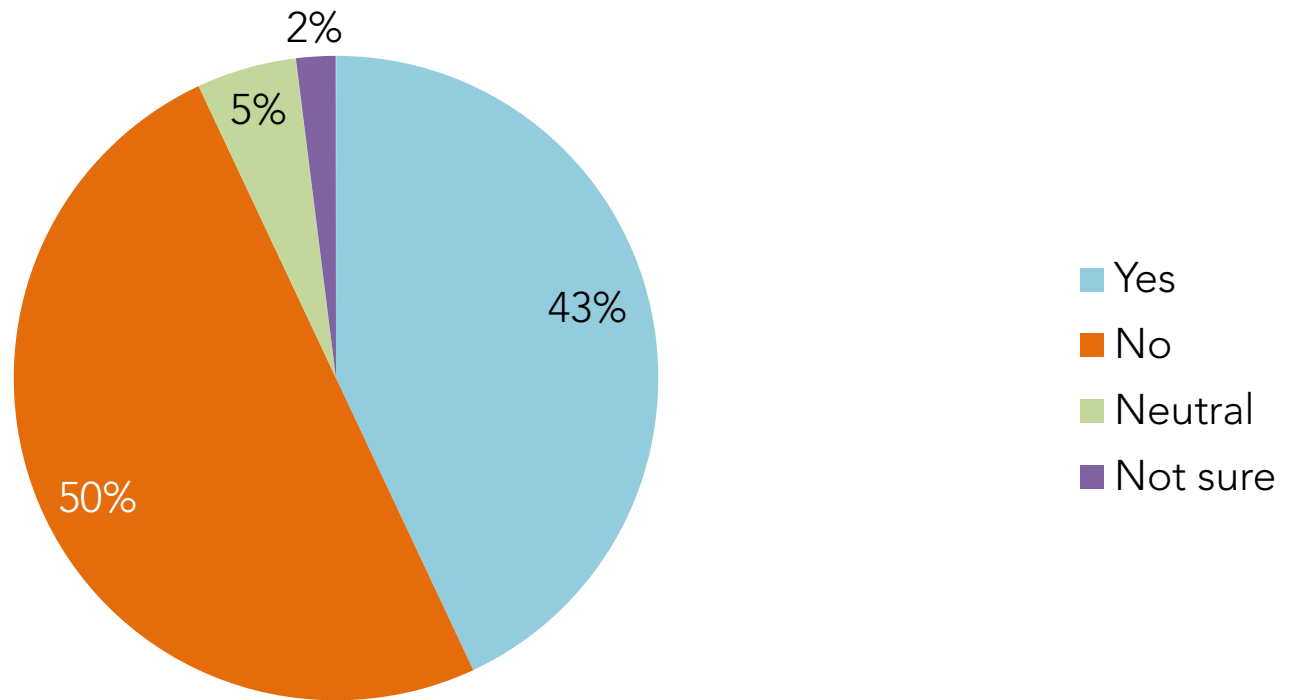
Appropriate for Basalt Creek?
Corner/convenience retail (single-use corner store)



N = 41

Workshop Results

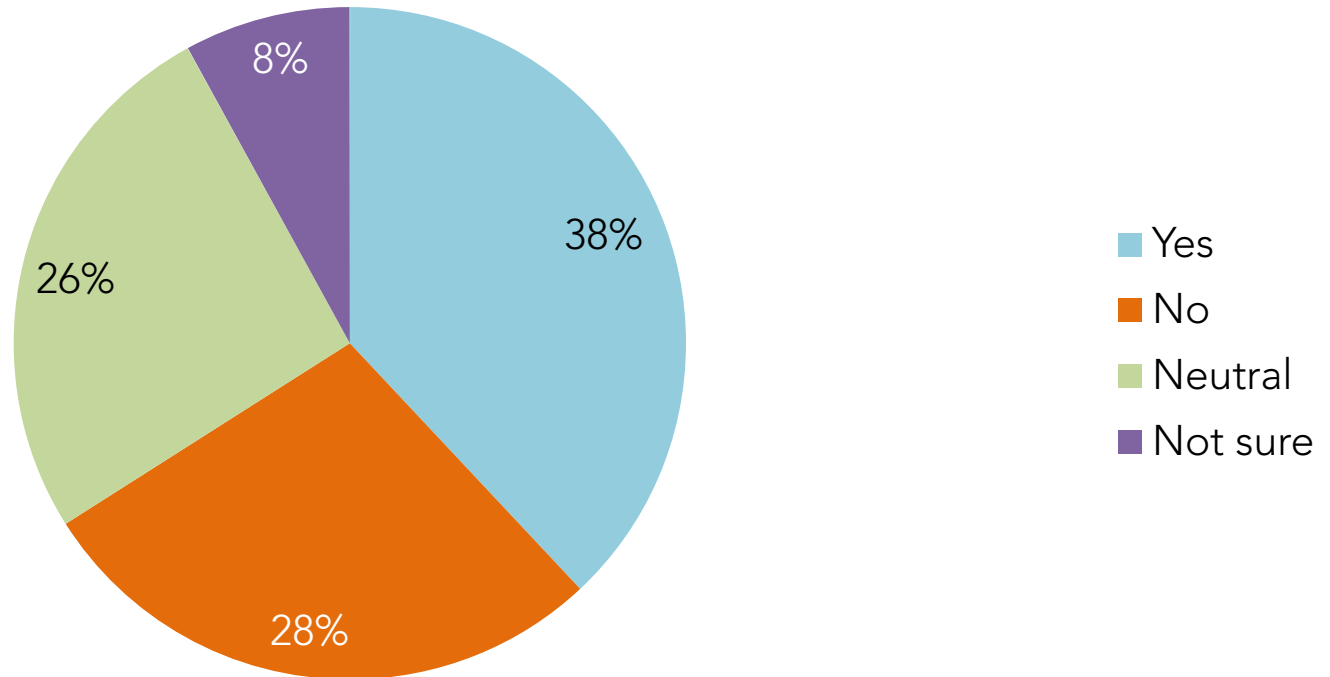
Appropriate for Basalt Creek?
Supermarket



N = 44

Workshop Results

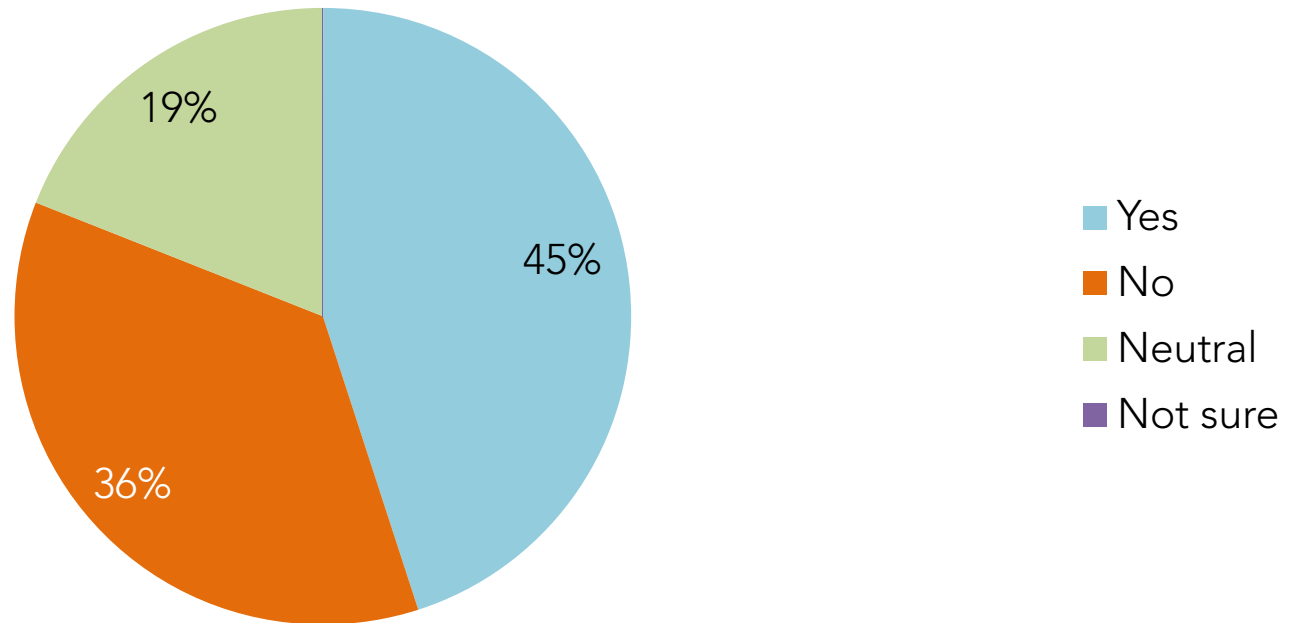
Appropriate for Basalt Creek?
Strip Retail (smaller scale/auto-oriented)



N = 39

Workshop Results

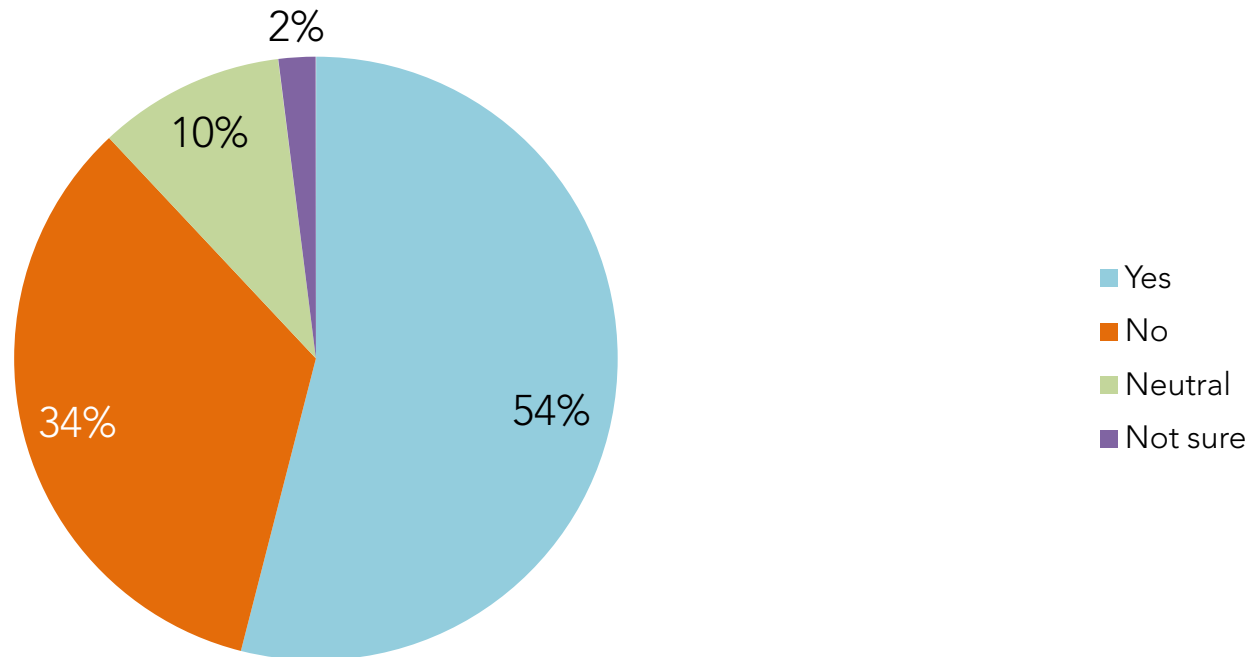
Appropriate for Basalt Creek?
Medical Services



N = 42

Workshop Results

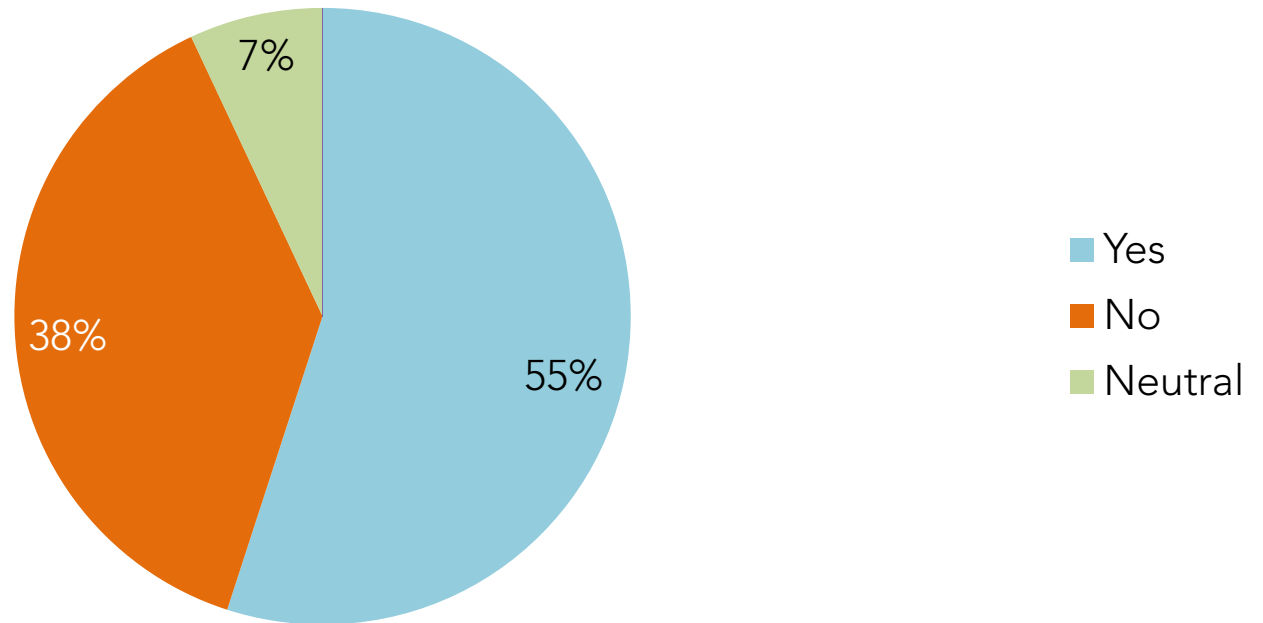
Appropriate for Basalt Creek?
Mixed-Use (ground floor retail with office or
housing above)



N = 41

Workshop Results

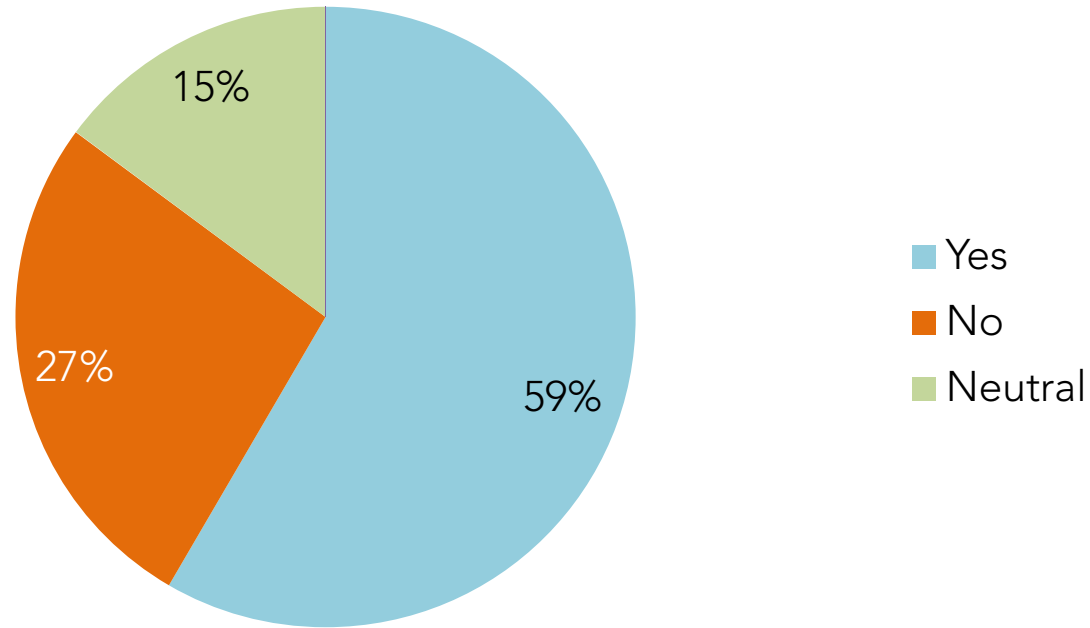
Appropriate for Basalt Creek?
Restaurants



N = 42

Workshop Results

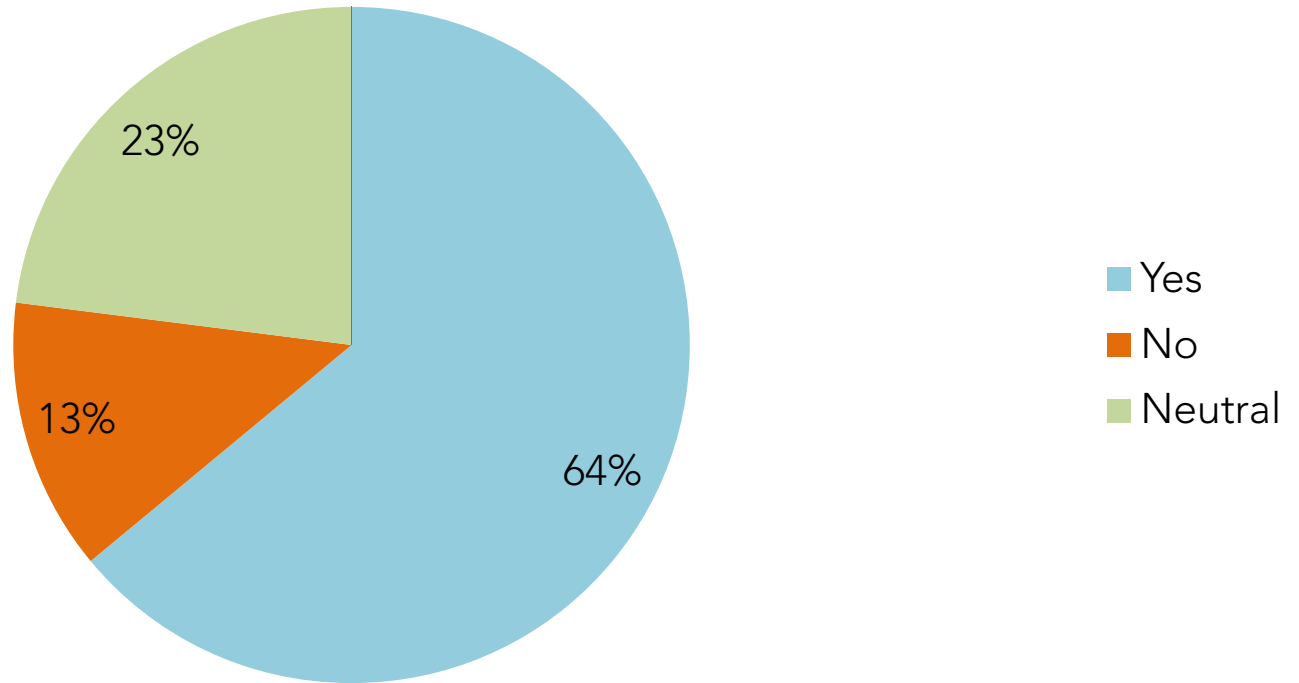
Appropriate for Basalt Creek?
Neighborhood Retail (small-scale/oriented to
pedestrians, transit and cars)



N = 41

Workshop Results

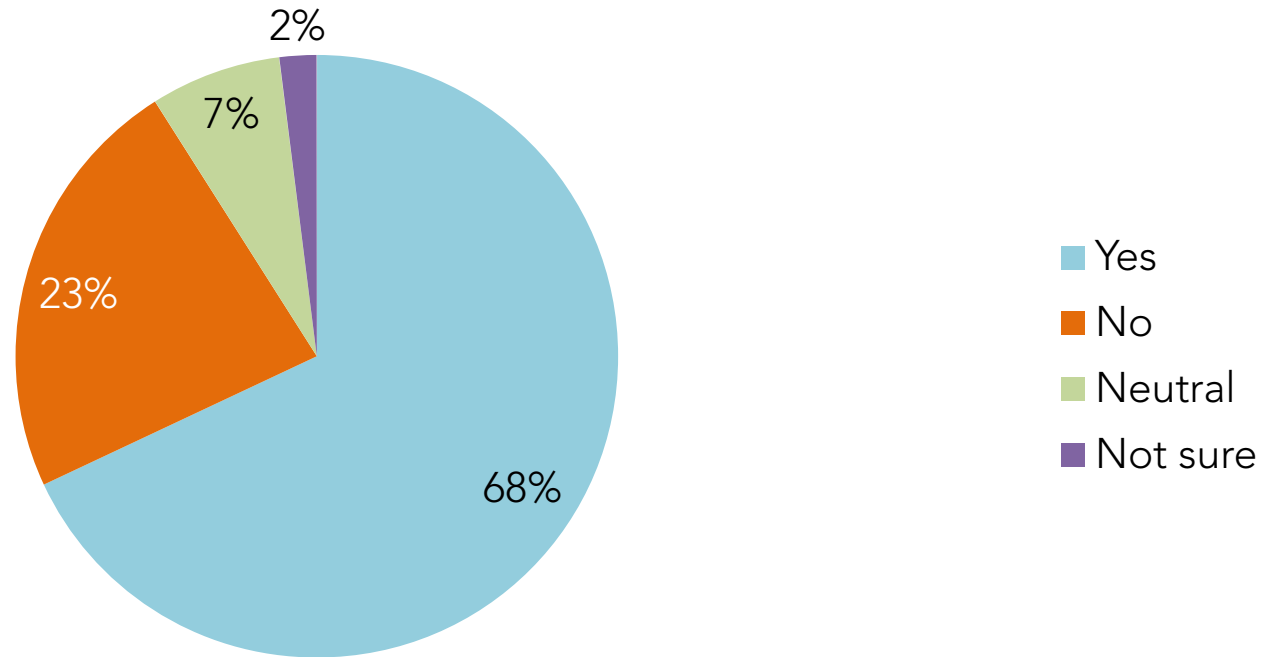
Appropriate for Basalt Creek?
Retail internally oriented to employees on a
campus



N = 39

Workshop Results

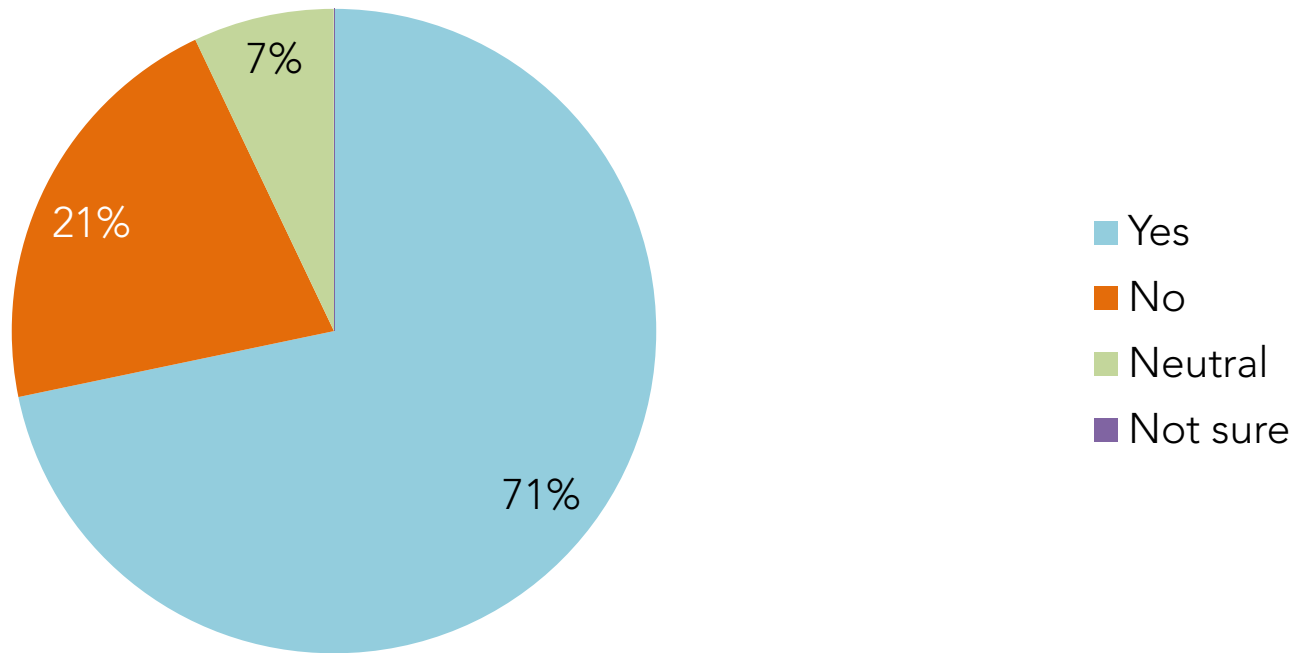
Appropriate for Basalt Creek?
Farm Stands



N = 44

Workshop Results

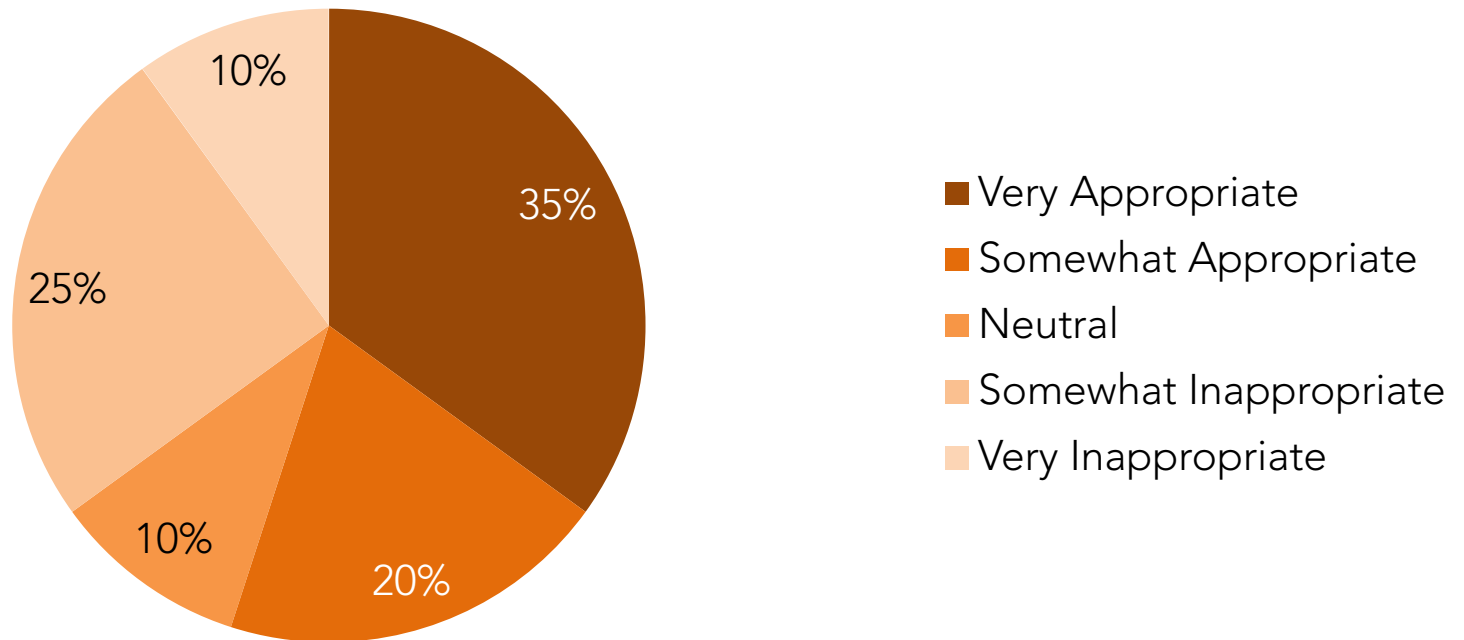
Appropriate for Basalt Creek?
Coffee Shops



N = 42

Workshop Results

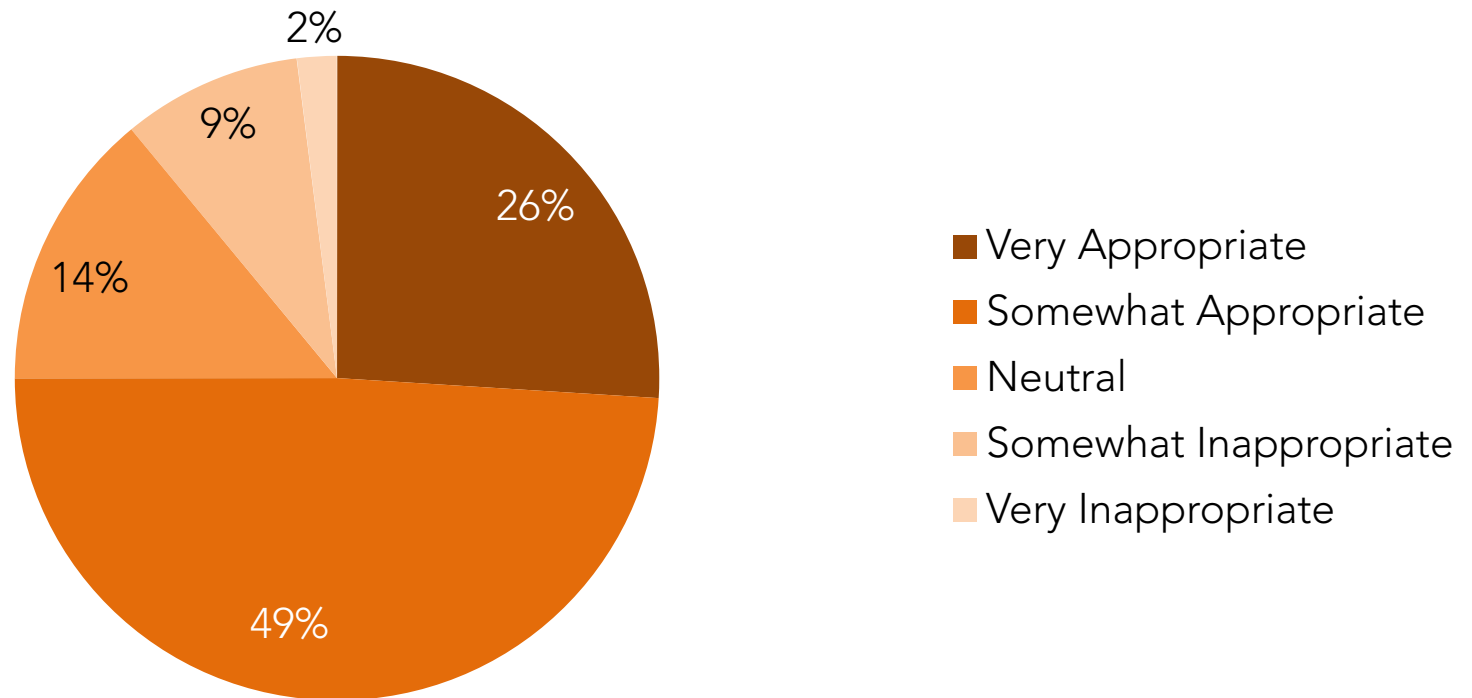
Appropriate for Basalt Creek? Warehousing Employment



N = 39

Workshop Results

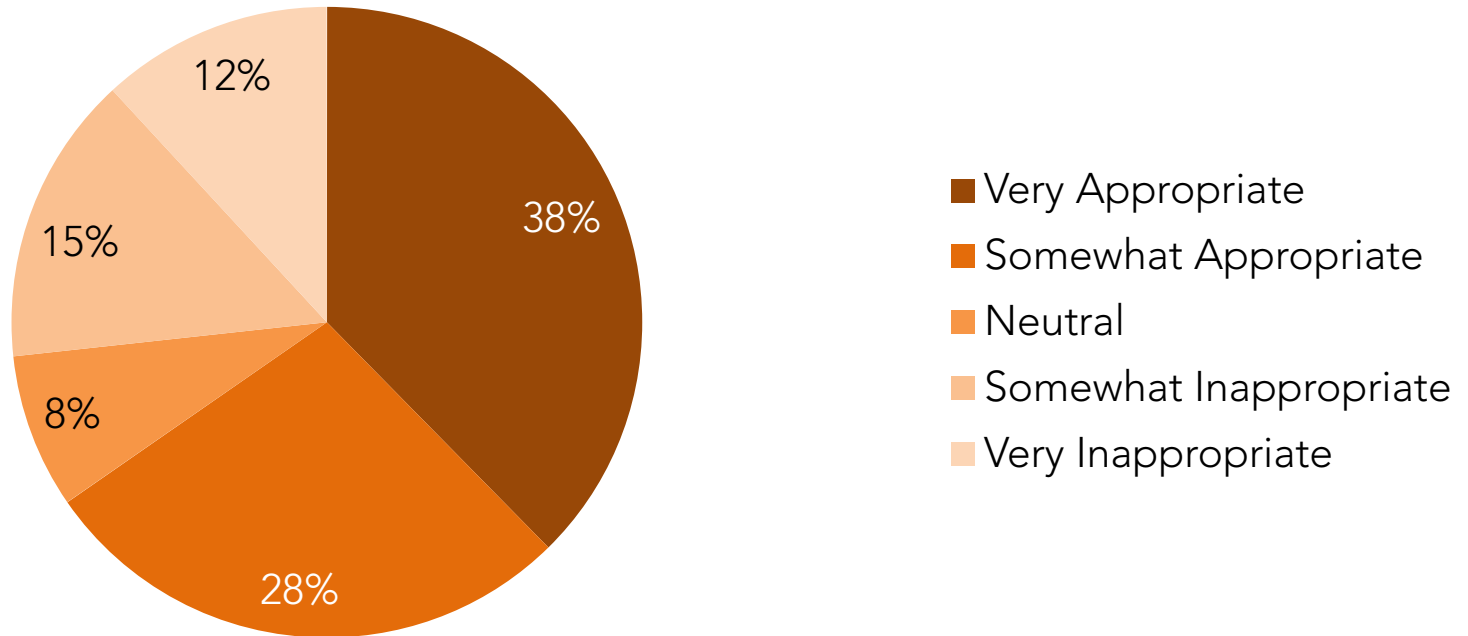
Appropriate for Basalt Creek?
Office Employment



N = 43

Workshop Results

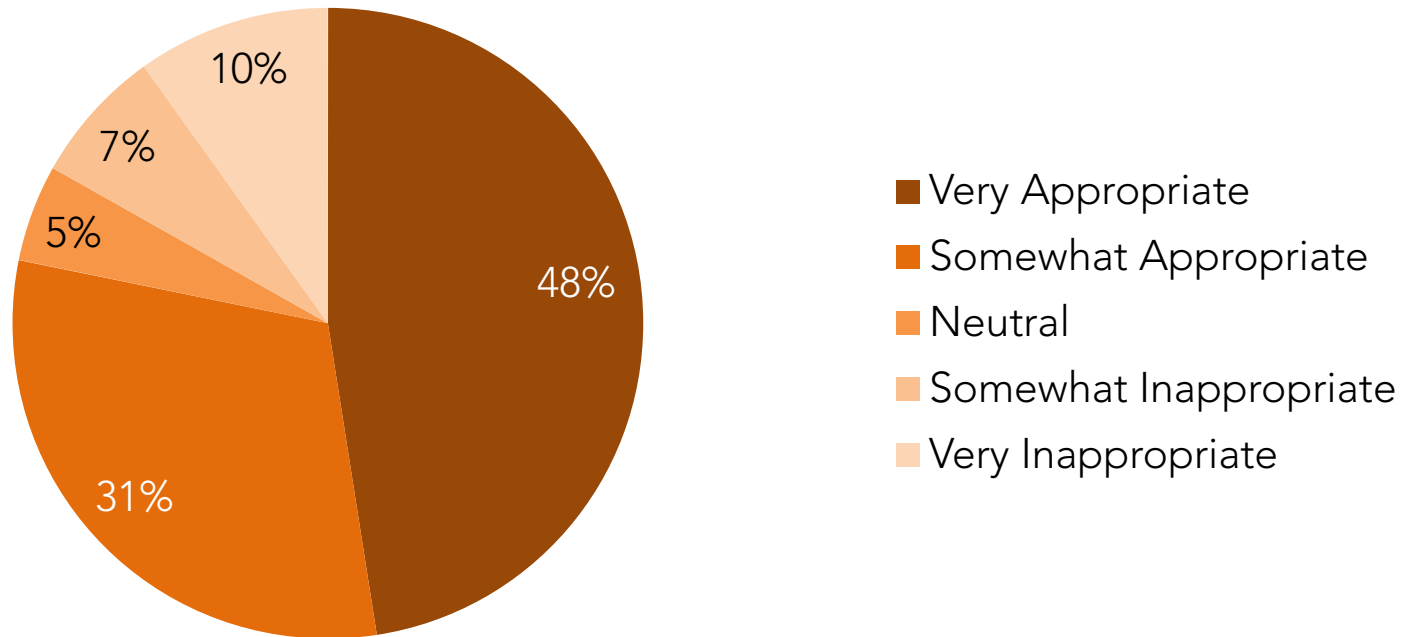
Appropriate for Basalt Creek?
Light Manufacturing Employment



N = 40

Workshop Results

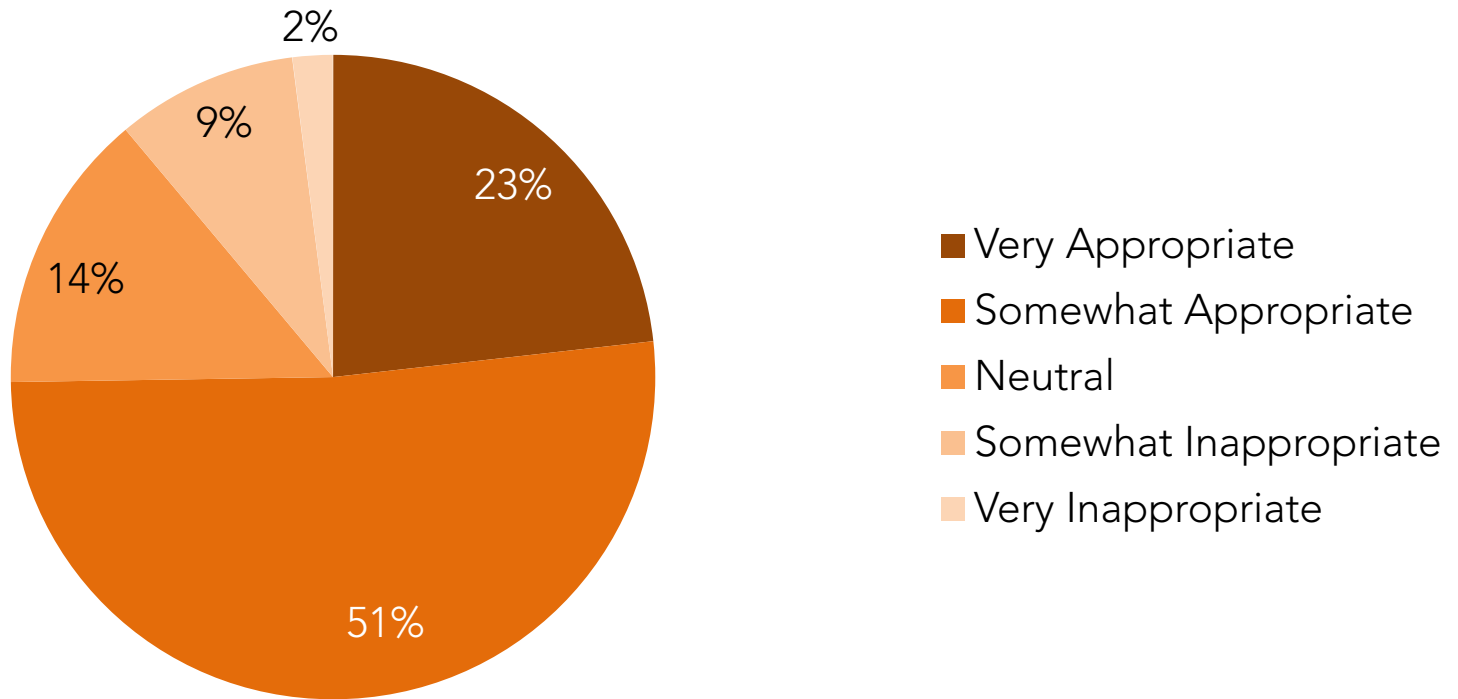
Appropriate for Basalt Creek? Industrial Flex Space Employment



N = 42

Workshop Results

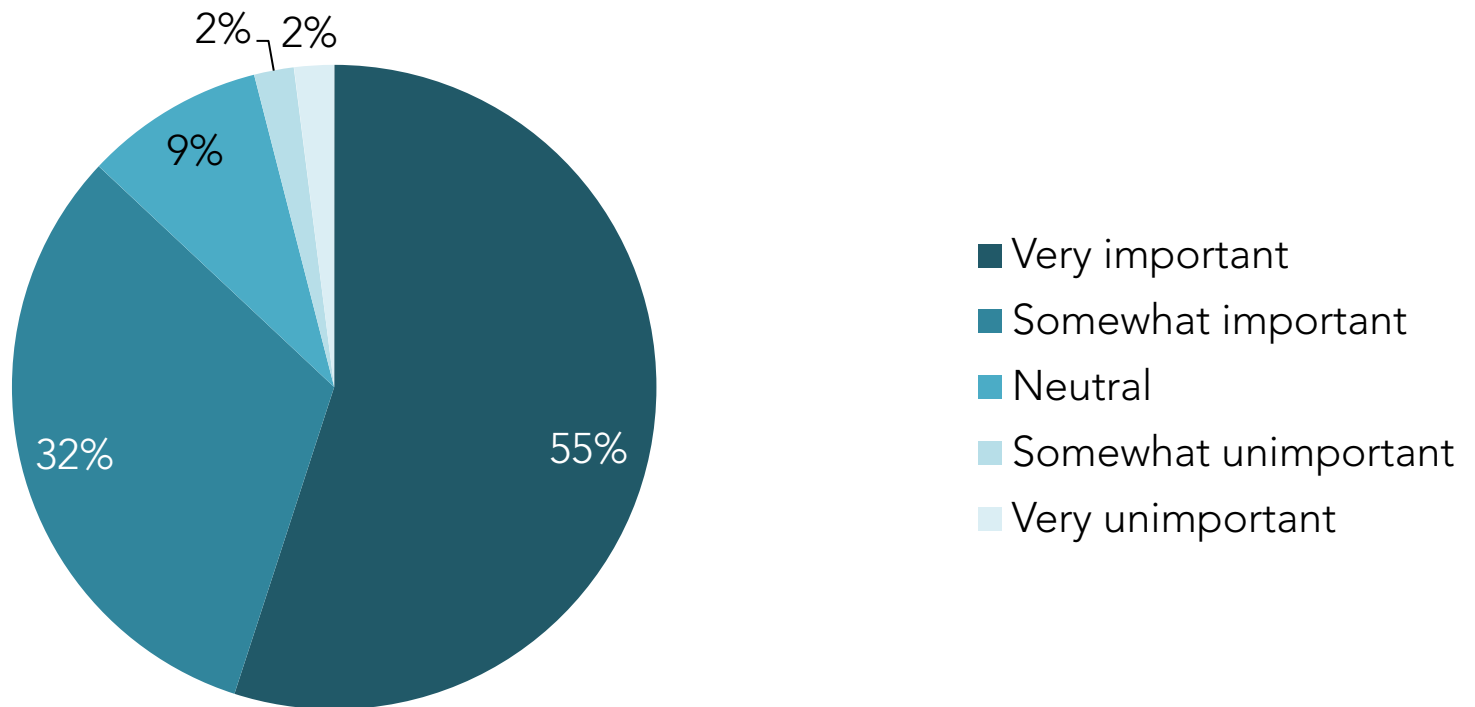
Appropriate for Basalt Creek? Small-Scale Retail Employment



N = 43

Workshop Results

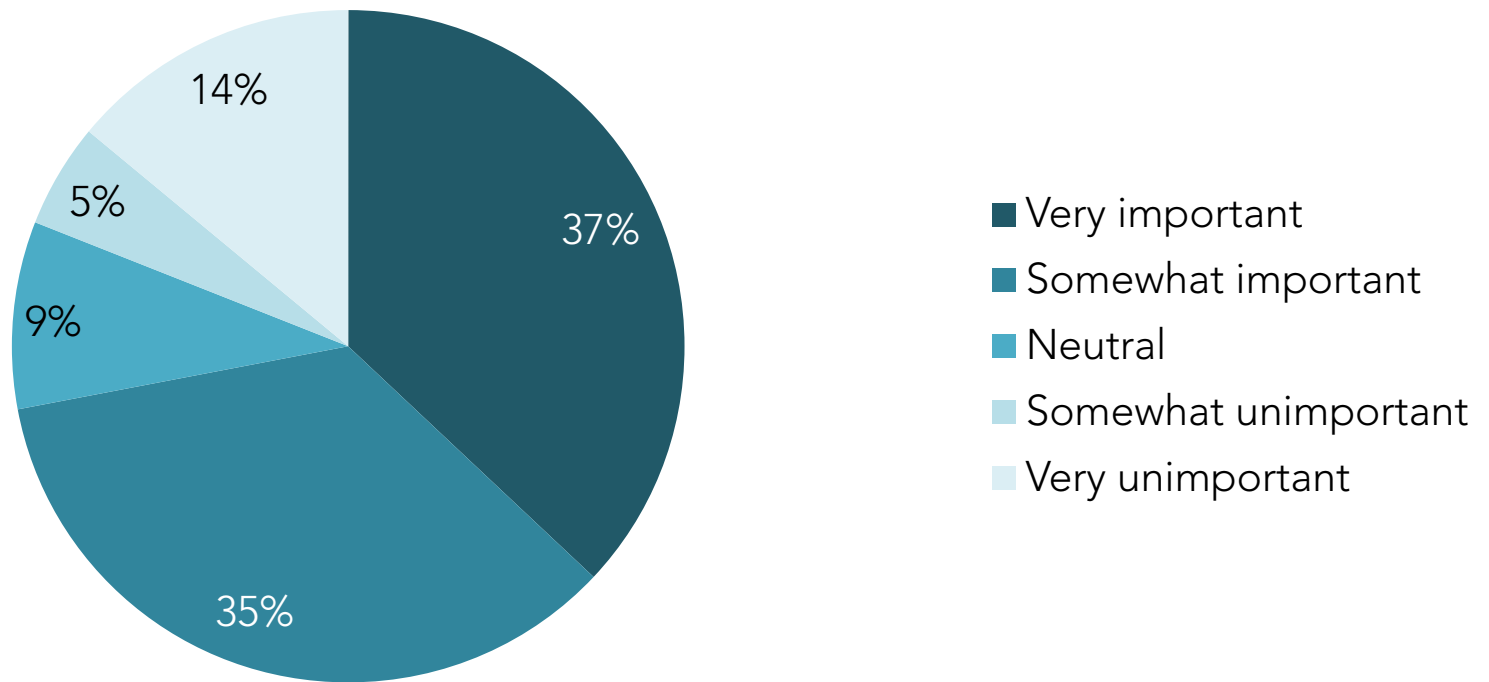
How important for Basalt Creek?
Jobs



N = 44

Workshop Results

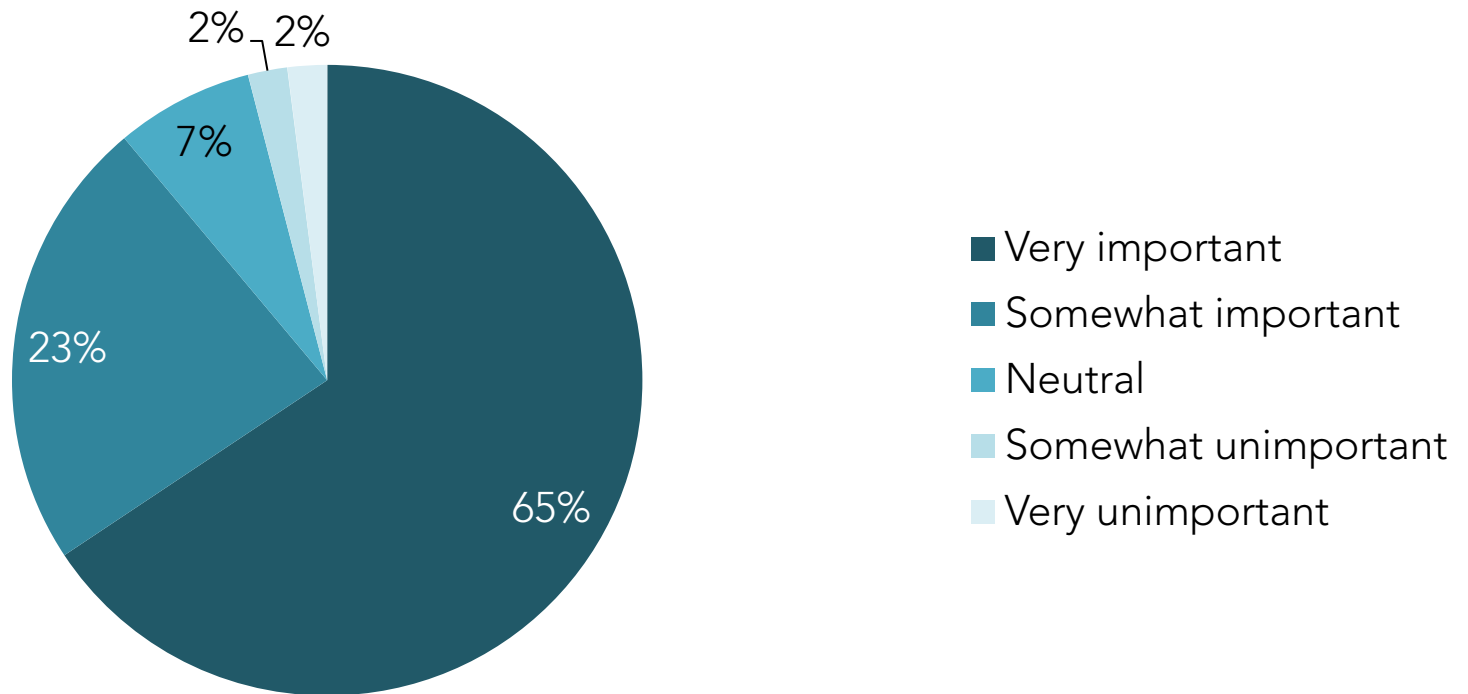
How important for Basalt Creek?
Environmental Protection



N = 43

Workshop Results

How important for Basalt Creek?
Housing



N = 43

Mapping Exercise Results

Map Exercise

Development Chips

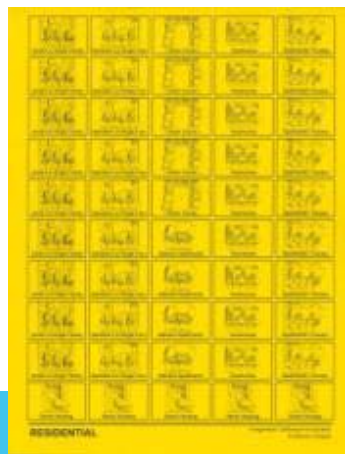
Employment



Commercial



Mixed Use



Residential

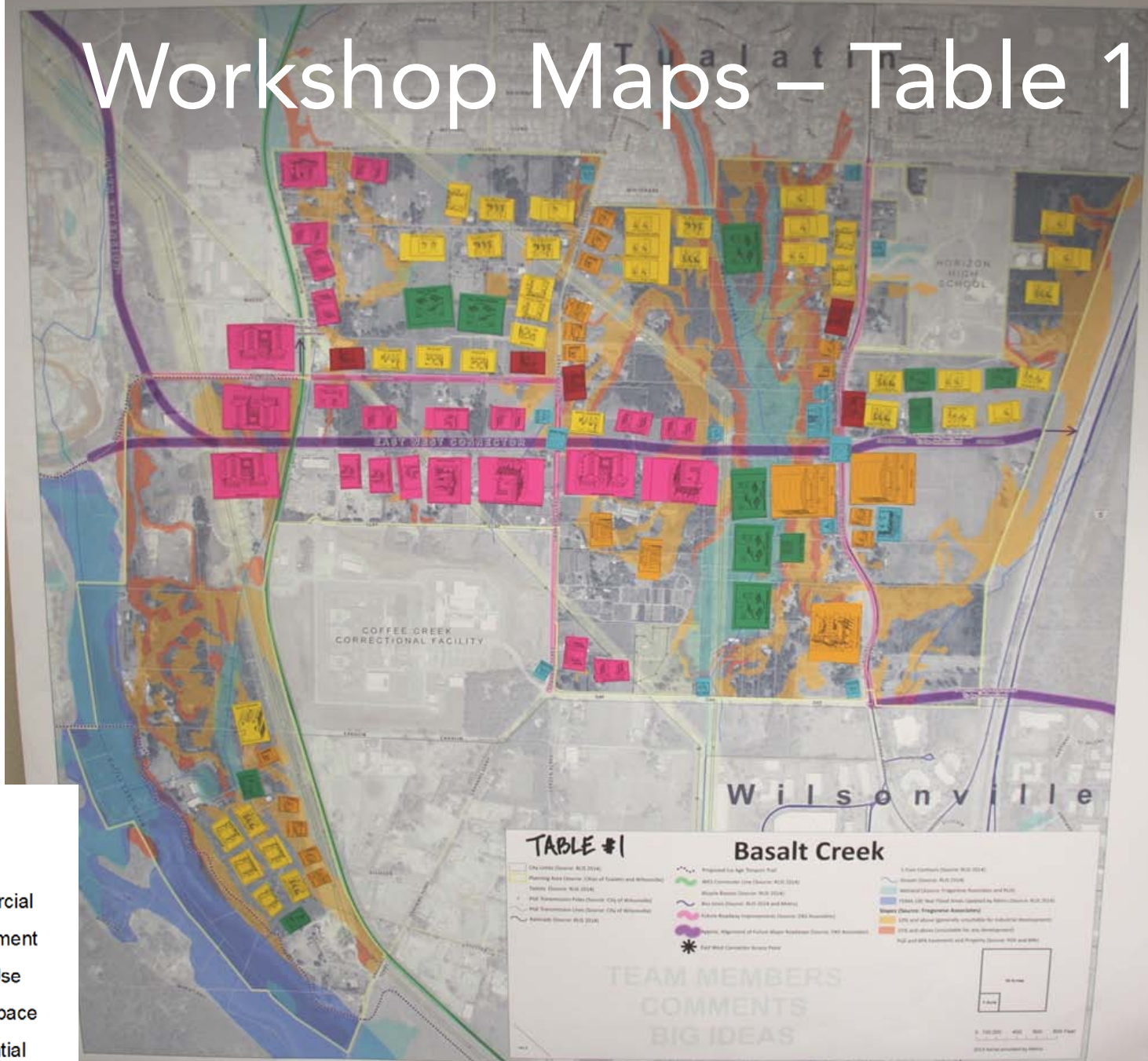


Civic



Open Space

Workshop Maps – Table 1



Legend

- Civic
- Commercial
- Employment
- Mixed Use
- Open Space
- Residential

TABLE #1 Basalt Creek

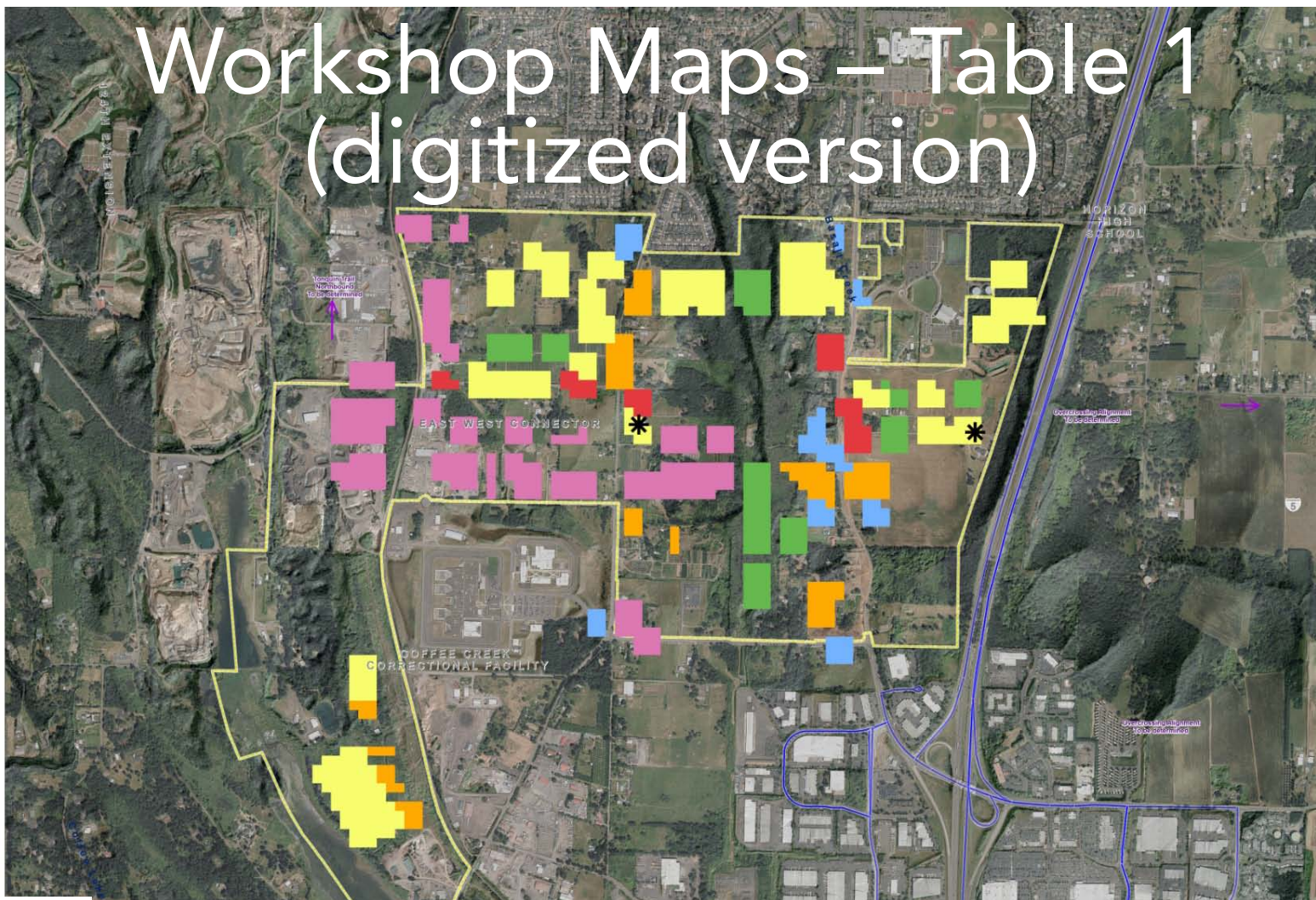
<ul style="list-style-type: none"> City Limits (Source: RCJ 2014) Planning Area (Source: City of Eugene and Willamette Valley (Source: WJ 2014) PAF Transportation Policy (Source: City of Willamette) PAF Transportation Line (Source: City of Willamette) Parkland (Source: RCJ 2014) 	<ul style="list-style-type: none"> Proposed Light Rail Transit Path WAF Connector Line (Source: RCJ 2014) Multiple Routes (Source: WJ 2014) Bus Rapid Transit (Source: WJ 2014 and Metro) Future Feasibility Transportation (Source: CRA Assessment) Proposed Alignment of Future Light Rail Transit (Source: TMT Assessment) East West Connector Bus Stop 	<ul style="list-style-type: none"> I-5 (Source: RCJ 2014) Oregon State Route 99 (Source: RCJ 2014) Highway 241 (Source: Oregon State Route 99 and RCJ 2014) Oregon (Source: Oregon State Route 99) City and State (Source: RCJ 2014) City and State (Source: RCJ 2014) City and State (Source: RCJ 2014)
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TEAM MEMBERS
COMMENTS
BIG IDEAS

2014 North arrow by ESRI



Workshop Maps – Table 1 (digitized version)



Legend

- Civic
- Commercial
- Employment
- Mixed Use
- Open Space
- Residential

Basalt Creek

- City Limits (Source: RLS 2014)
- Planning Area (Source: Cities of Tualatin and Wilsonville)
- Taxlots (Source: RLS 2014)
- PGE Transmission Poles (Source: City of Wilsonville)
- PGE Transmission Lines (Source: City of Wilsonville)
- Railroads (Source: RLS 2014)
- Proposed Ice Age Tonquin Trail
- WES Commuter Line (Source: RLS 2014)
- Bicycle Routes (Source: RLS 2014)
- Bus Lines (Source: RLS 2014 and Metro)
- Future Roadway Improvements (Source: DKS Associates)
- Approx. Alignment of Future Major Roadways (Source: DKS Associates)
- * East West Connector Access Point
- 5 Foot Contours (Source: RLS 2014)
- Stream (Source: RLS 2014)
- Wetland (Source: Fregonese Associates and RLS)
- FEMA 100 Year Flood Areas Updated by Metro (Source: RLS 2014)
- Slopes (Source: Fregonese Associates)**
- 10% and above (generally unsuitable for industrial development)
- 25% and above (unsuitable for any development)
- PGE and BPA Easements and Property (Source: PGE and BPA)

TEAM MEMBERS
COMMENTS
BIG IDEAS



0 1000 200 400 600 800 Feet

2013 Aerial provided by Metro



Table 1 Notes

Goals

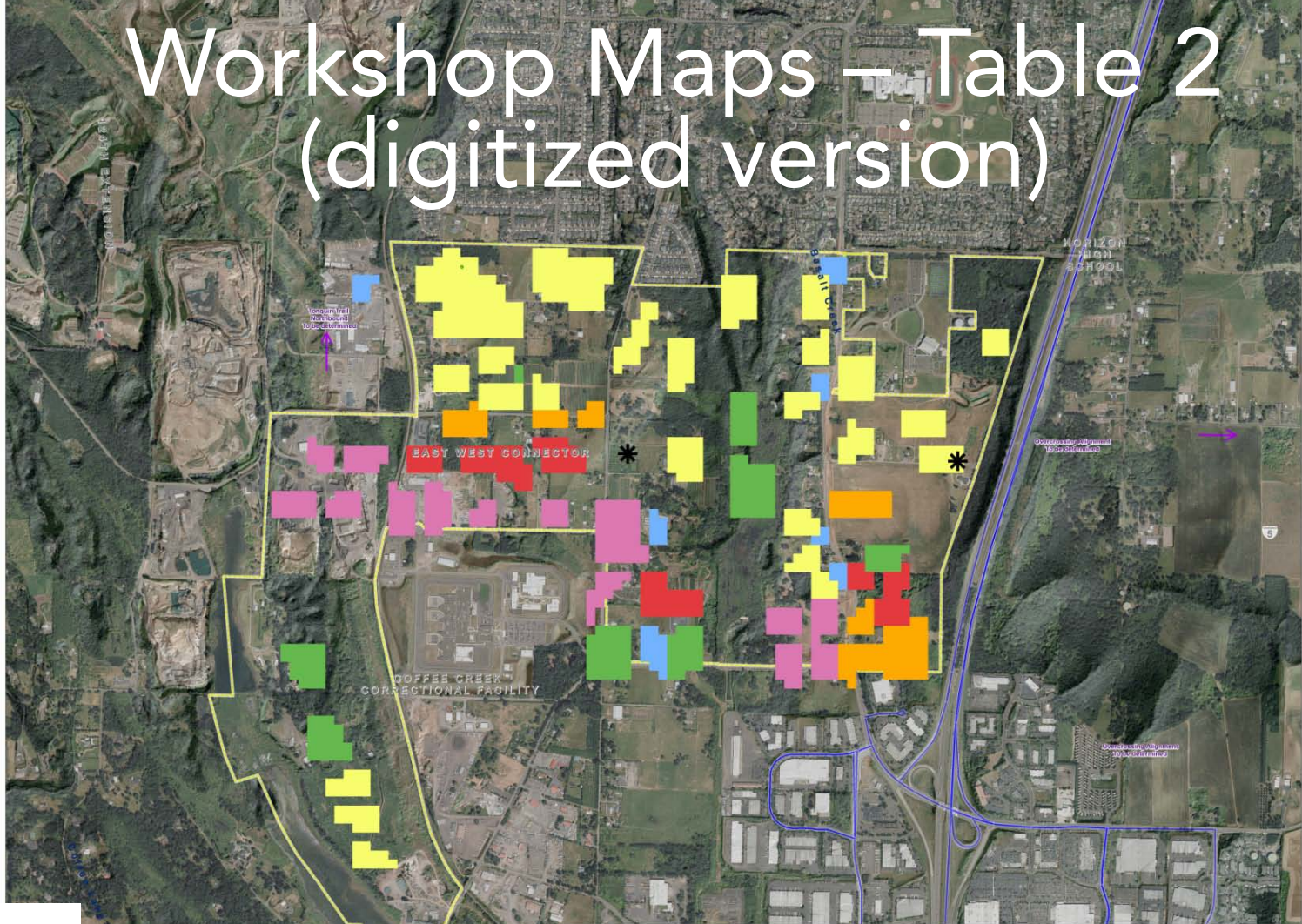
- Housing/schools close together
- Public amenities around wetlands
- Housing where there is transportation and other existing infrastructure
- Transit options that allow people to make trips without their cars
- Make the wetlands a source of pride and natural beauty (visual focal point/vistas)

Table 1 Notes cont.

Comments

- Civic entertainment use – public theater?
- Seems like E-W Connector will determine how land uses are arranged
- Could the nursery along Graham's Ferry be encouraged to develop as a unique attraction?
- This is an opportunity do something different – provide public amenities that make the community proud.

Workshop Maps – Table 2 (digitized version)



Legend

- Civic
- Commercial
- Employment
- Mixed Use
- Open Space
- Residential

Basalt Creek

<ul style="list-style-type: none"> City Limits (Source: RUS 2014) Planning Area (Source: Cities of Tullatin and Willowville) Tallots (Source: RUS 2014) PGE Transmission Poles (Source: City of Willowville) PGE Transmission Lines (Source: City of Willowville) Railroads (Source: RUS 2014) 	<ul style="list-style-type: none"> Proposed Ice Age Tongue Trail WES Commuter Line (Source: RUS 2014) Bicycle Routes (Source: RUS 2014) Bus Lines (Source: RUS 2014 and Metro) Future Roadway Improvements (Source: DNS Associates) Approx. Alignment of Future Major Roadways (Source: DNS Associates) East West Connector Access Point 	<ul style="list-style-type: none"> 5 Foot Contours (Source: RUS 2014) Stream (Source: RUS 2014) Wetland (Source: Fregnese Associates and RUS) FEMA 500 Year Flood Areas (Updated by Metro) (Source: RUS 2014) <p>Slopes (Source: Fregnese Associates)</p> <ul style="list-style-type: none"> 10% and above (generally unsuitable for industrial development) 25% and above (unsuitable for any development) PGE and BPA Esasments and Property (Source: PGE and BPA)
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TEAM MEMBERS
COMMENTS
BIG IDEAS

0 100 200 400 600 800 Feet
2013 Aerial provided by Metro



Table 2 Notes

Goals

- Increase recreation, more sports fields (plenty of them in Tualatin)
- Parks/natural area around Basalt Creek - preservation – West Railroad
- Concern around runoff into Basalt Creek
- Joint rec center
- Housing in Tualatin
- Incorporation into regional trail system along Basalt Creek
- Concern about widening of Boones Ferry for peds and bikes
- Location of EW/Boone's Ferry
- Water/sewer lines

Table 2 Notes cont.

Goals Cont.

- EW Connector at Boone's Ferry
- Smoother transition from industrial to housing
- Stop at WES –Trans
- Recreation (shared facilities)
- Natural area protection
- Housing –not everything need to be industrial south of the EW Connector

Big Ideas

- Connect to WES
- Smooth transition between uses
- Brew Pubs
- Crosswalks across Boone's Ferry

Workshop Maps – Table 3



Legend

- Civic
- Commercial
- Employment
- Mixed Use
- Open Space
- Residential

Basalt Creek

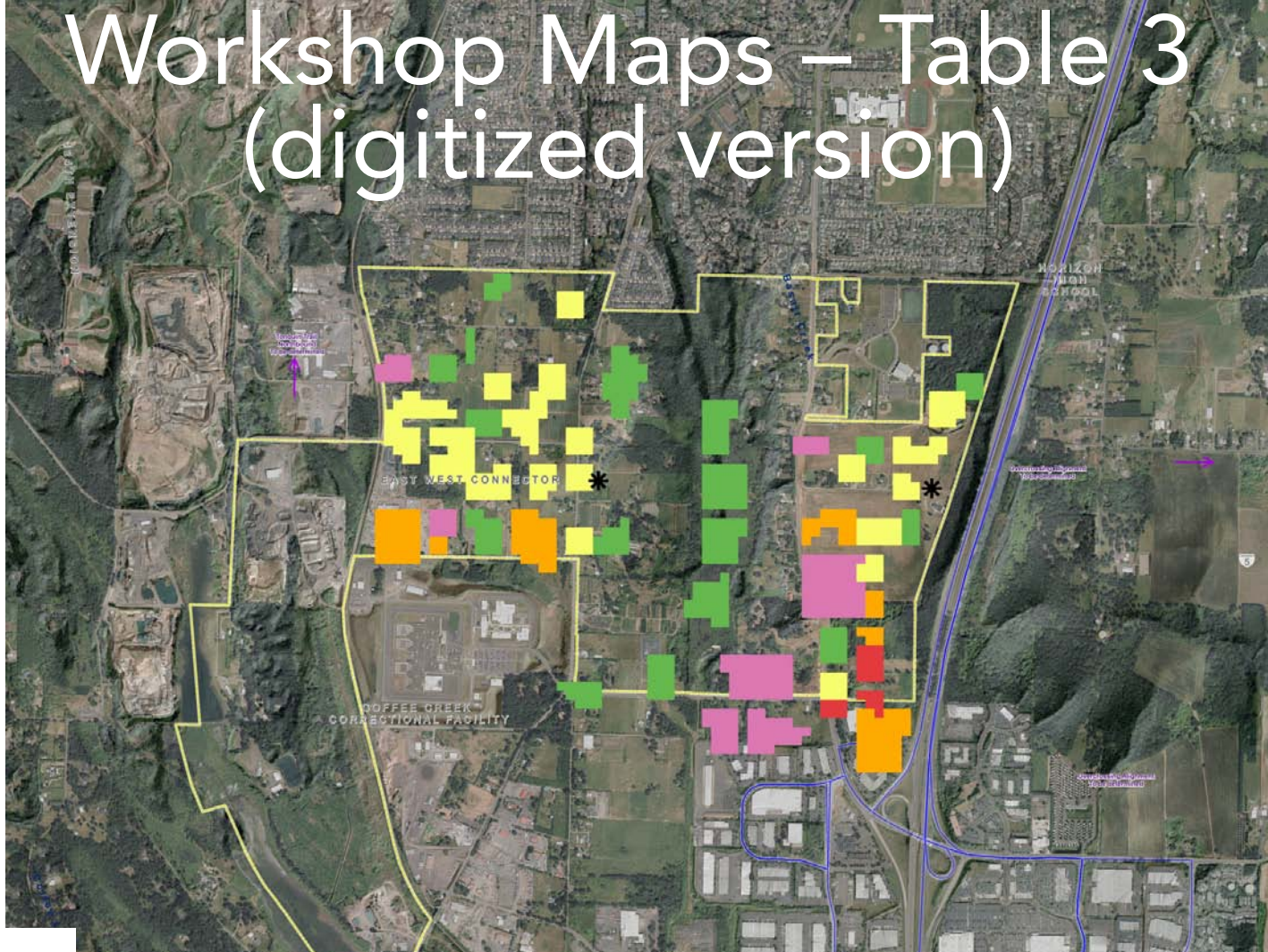
<ul style="list-style-type: none"> City Limits (Source: M.U. 2016) Planning Area (Source: City of Tualatin and Wilsonville) (Source: M.U. 2016) M.U. Transportation Policy (Source: City of Wilsonville) M.U. Transportation Lines (Source: City of Wilsonville) Roadway (Source: M.U. 2016) 	<ul style="list-style-type: none"> Proposed to Age Temporary Road M.U. Connector Line (Source: M.U. 2016) Wetland Buffer (Source: M.U. 2016) Bio-Local (Source: M.U. 2016 and M.U. 2019) Future Roadway Improvements (Source: CWS Assessment) M.U. Alignment of Future Major Roadways (Source: CWS Assessment) East-West Connector Water Point 	<ul style="list-style-type: none"> Major Corridor (Source: M.U. 2016) M.U. 2016 M.U. 2019 M.U. 2016 and M.U. 2019 M.U. 2016 and M.U. 2019 M.U. 2016 and M.U. 2019 M.U. 2016 and M.U. 2019 M.U. 2016 and M.U. 2019 M.U. 2016 and M.U. 2019 M.U. 2016 and M.U. 2019
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#3

TEAM MEMBERS
COMMENTS
BIG IDEAS

0 100,000 200 300 Feet
 M.U. 2016 and M.U. 2019
 M.U. 2016 and M.U. 2019

Workshop Maps – Table 3 (digitized version)



Legend

- Civic
- Commercial
- Employment
- Mixed Use
- Open Space
- Residential

Basalt Creek

- City Limits (Source: RUS 2014)
- Planning Area (Source: Cities of Tualatin and Wilsonville)
- Taxlots (Source: RUS 2014)
- PGE Transmission Poles (Source: City of Wilsonville)
- PGE Transmission Lines (Source: City of Wilsonville)
- Railroads (Source: RUS 2014)
- Proposed Ice Age Tongue Trail
- WES Commuter Line (Source: RUS 2014)
- Bicycle Routes (Source: RUS 2014)
- Bus Lines (Source: RUS 2014 and Metro)
- Future Roadway Improvements (Source: DCS Associates)
- Approx. Alignment of Future Major Roadways (Source: DCS Associates)
- East West Connector Access Point
- 5 Foot Contours (Source: RUS 2014)
- Stream (Source: RUS 2014)
- Wetland (Source: Frogmore Associates and RUS)
- FEMA 100 Year Flood Areas Updated by Metro (Source: RUS 2014)
- Slopes (Source: Frogmore Associates)
- 20% and above (generally unsuitable for industrial development)
- 25% and above (unsuitable for any development)
- PGE and 8th Assessments and Property (Source: PGE and BPR)

TEAM MEMBERS
COMMENTS
BIG IDEAS



010200 400 600 800 Feet
2013 Aerial provided by Metro



Table 3 Notes

Goals

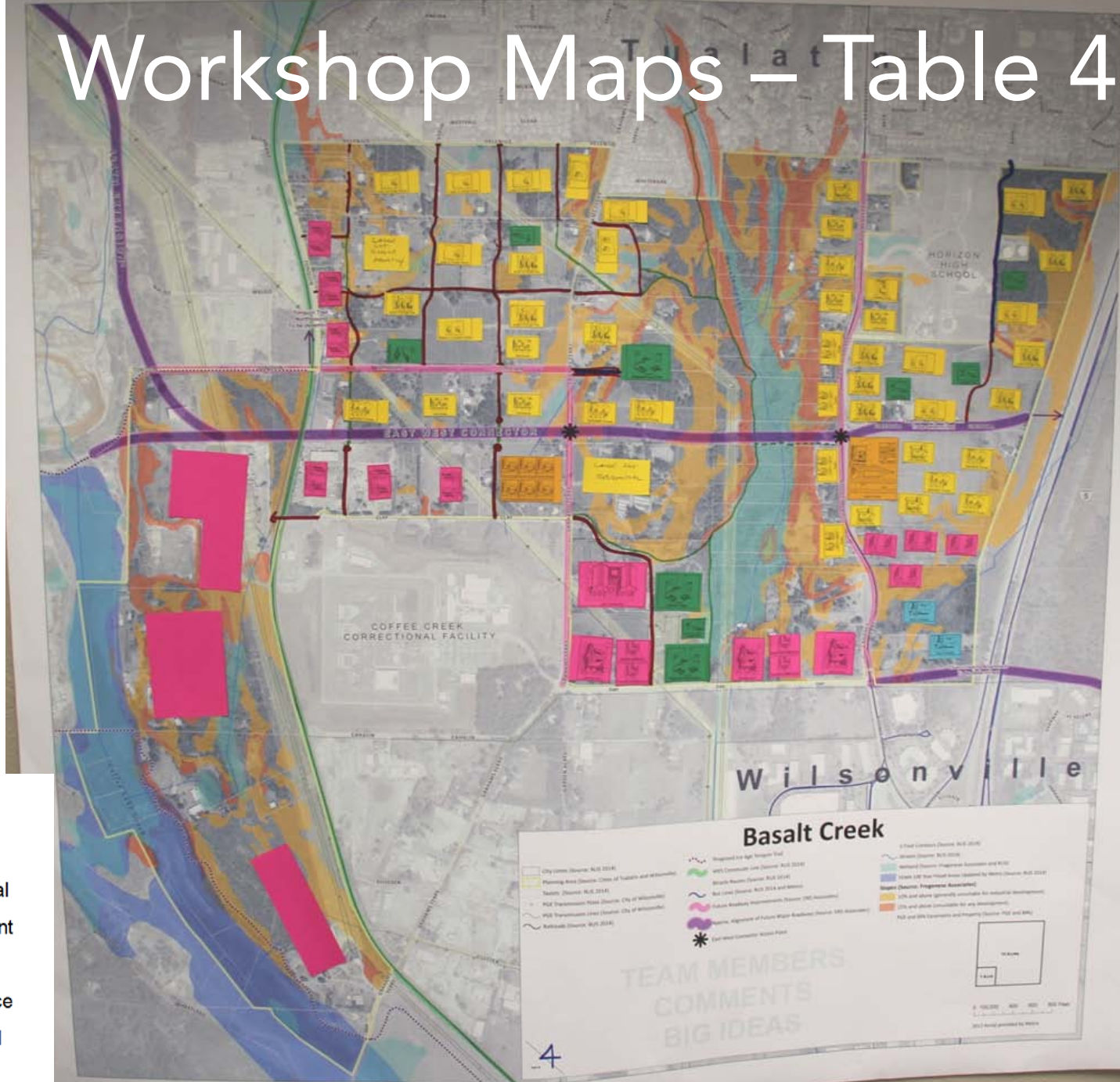
- Residential development
- Diverse housing mix (more than just single family)
- Celebrate natural features
- Interconnected trans network
- Integrate other regional plans
- Well laid out mix of land uses
- Integrated trail and greenways (multimodal connections)

Table 3 Notes cont.

Comments

- Bike/ped access from Tualatin to Wilsonville- in nature
- Employment center near I-5 (east of I-5)
- Buffering between residential and industrial (transitional)
- Trails on power line easements
- Small lot SF and apartments – what is the market?
- Mixed use housing
- Where to put hi-density housing
- Prevent noise pollution from industry
- Center?
- Sherwood school district
- Housing where kids can walk to school
- Hi-density, assisted living near overpass
- Retail and industrial toward the south (jobs and light industrial)

Workshop Maps – Table 4



Legend

- Civic
- Commercial
- Employment
- Mixed Use
- Open Space
- Residential

Basalt Creek

<ul style="list-style-type: none"> City Limits (Source: NLR 2014) Planning Area (Source: City of Tualatin and Washington County (Source: NLR 2014)) PUG Transportation Plans (Source: City of Wilsonville) PUG Transportation Lines (Source: City of Wilsonville) Railroads (Source: NLR 2014) 	<ul style="list-style-type: none"> Highway for Age Transport Plan WES Corridor Line (Source: April 2014) Weekly Routes (Source: NLR 2014) Bus Lines (Source: NLR 2014 and Source) Future Mobility Transportation Plans (2014 Approved) Utility Alignment of Future Water Treatment (Source: WWS Assessment) East West Corridor Water Point 	<ul style="list-style-type: none"> Flow Control (Source: NLR 2014) Water (Source: NLR 2014) Northwest Oregon Regional Assessment and WWS Lines for Flow Control Areas (Adopted by WWS (Source: NLR 2014)) Water (Source: Progress Associates) 20% and above (Source: Progress Associates) 20% and above (Source: Progress Associates) PUG and WWS Treatment and Receiving (Source: PUG and WWS)
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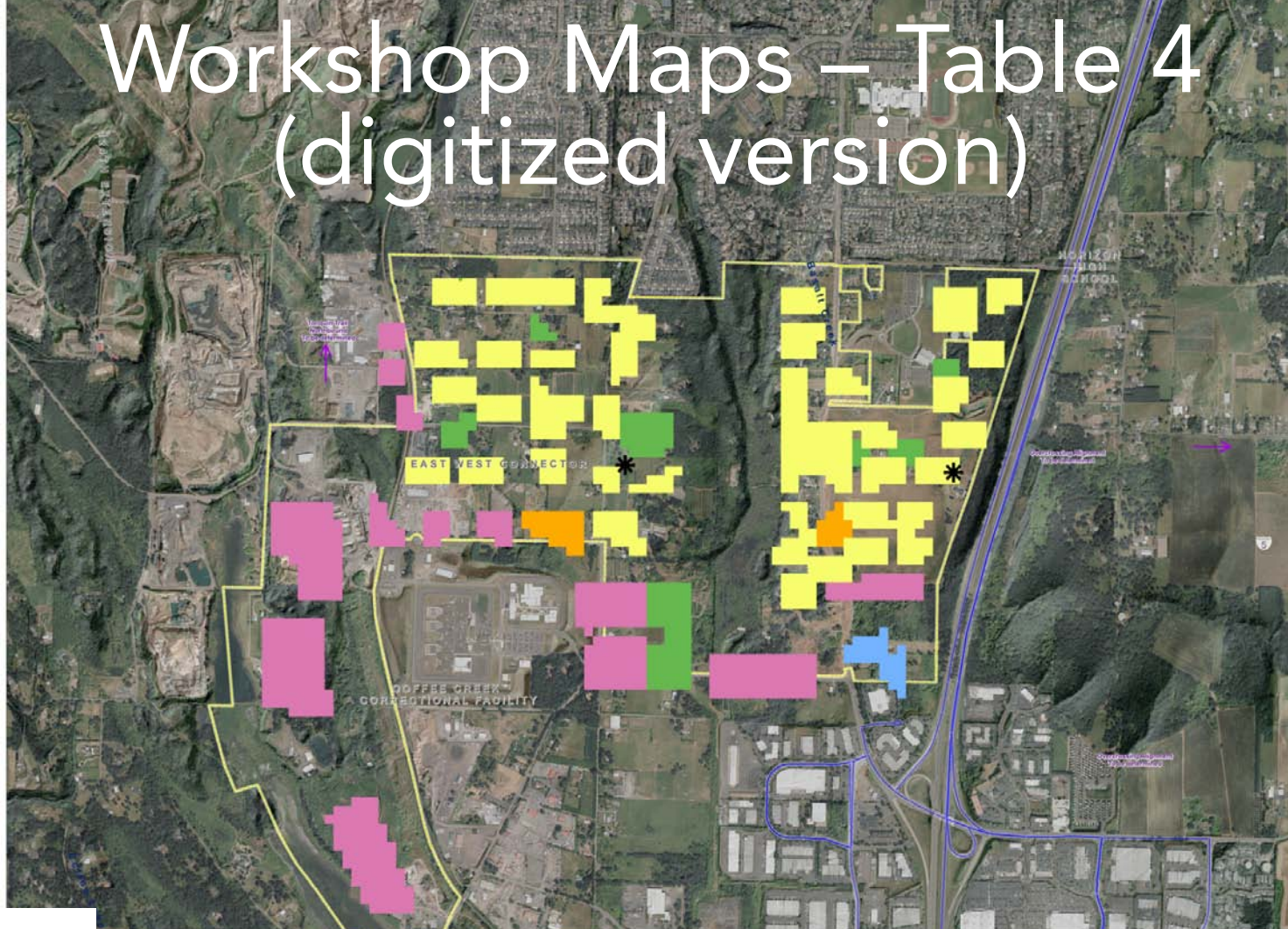
1" = 1000'

TEAM MEMBERS COMMENTS BIG IDEAS

4



Workshop Maps – Table 4 (digitized version)



Legend

- Civic
- Commercial
- Employment
- Mixed Use
- Open Space
- Residential

Basalt Creek

<ul style="list-style-type: none"> City Limits (Source: RUS 2014) Planning Area (Source: Cities of Tualatin and Wilsonville) Transit (Source: RUS 2014) PGE Transmission Poles (Source: City of Wilsonville) PGE Transmission Lines (Source: City of Wilsonville) Railroads (Source: RUS 2014) 	<ul style="list-style-type: none"> Proposed Ice Age Tongue Trail WES Commuter Line (Source: RUS 2014) Bicycle Routes (Source: RUS 2014) Bus Lines (Source: RUS 2014 and Metro) Future Roadway Improvements (Source: DHS Associates) Approx. Alignment of Future Major Roadways (Source: DHS Associates) * East West Connector Access Point 	<ul style="list-style-type: none"> 5 Foot Contours (Source: RUS 2014) Stream (Source: RUS 2014) Wetland (Source: Fregresse Associates and RUS) FEMA 100 Year Flood Area Updated by Metro (Source: RUS 2014) Slopes (Source: Fregresse Associates) 10% and above (generally unsuitable for industrial development) 2% and above (unsuitable for any development) PGE and BPA Assessments and Property (Source: PGE and BPA)
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TEAM MEMBERS
COMMENTS
BIG IDEAS

© 10/2015 400 800 800 Feet
2013 Aerial provided by Metro

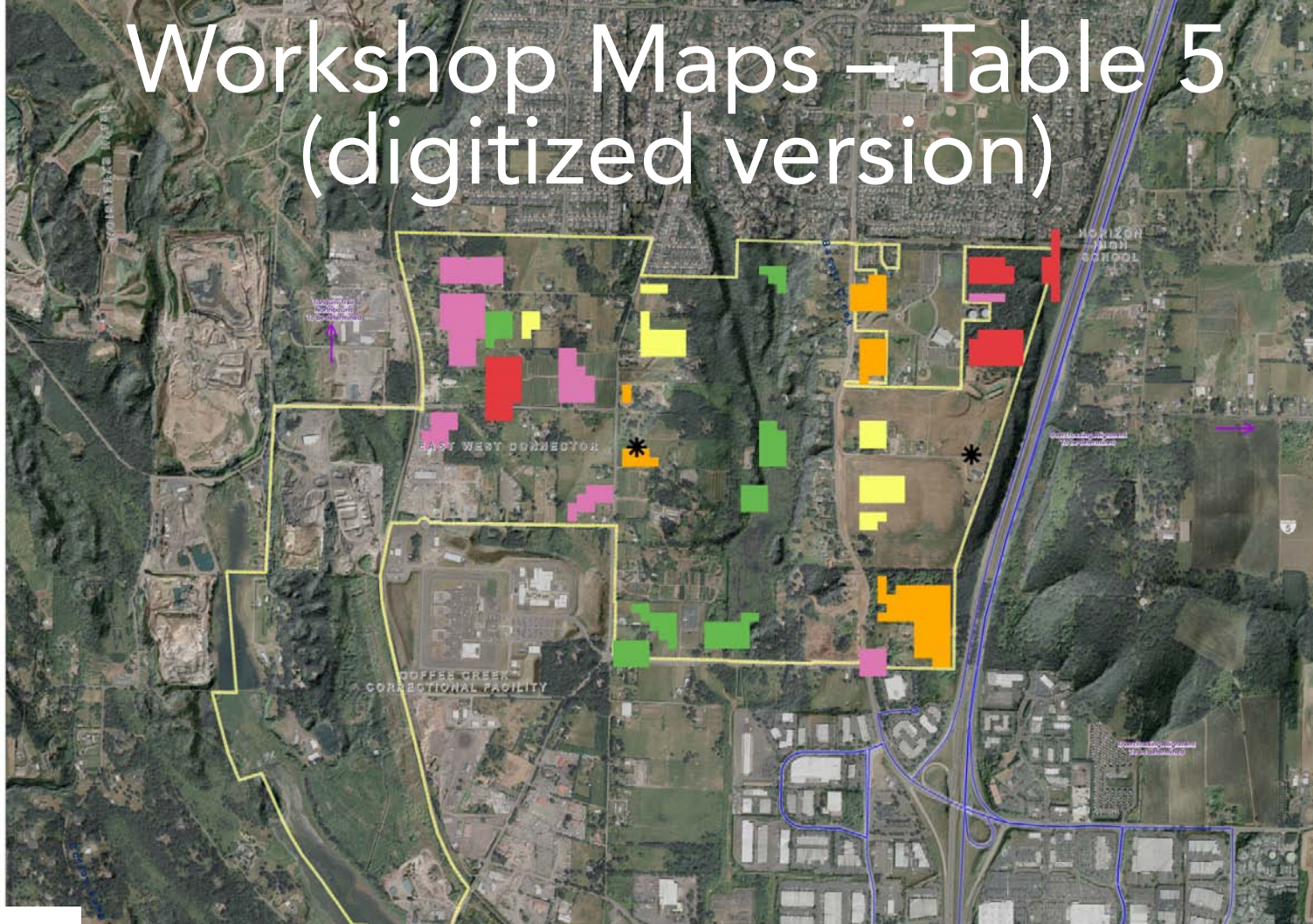
Table 4 Notes

- Residential at north that transitions to higher density/mixed use as you go south, eventually to light manufacturing.
- Access to small commercial services from residential areas.
- Places of worship at south end
- Sports complex and parks/open spaces
- Transitions between types of uses.

Workshop Maps – Table 5



Workshop Maps – Table 5 (digitized version)



Legend

- Civic
- Commercial
- Employment
- Mixed Use
- Open Space
- Residential

Basalt Creek

- City Limits (Source: RLS 2014)
- Planning Area (Source: Cities of Taalain and Willowville)
- Tazdelt (Source: RLS 2014)
- PGE Transmission Poles (Source: City of Willowville)
- PGE Transmission Lines (Source: City of Willowville)
- Railroads (Source: RLS 2014)
- Proposed Ice Age Tongue Trail
- WEI Commuter Line (Source: RLS 2014)
- Bicycle Routes (Source: RLS 2014)
- Bus Lines (Source: RLS 2014 and Metro)
- Future Roadway Improvements (Source: DKS Associates)
- Aztec Alignment of Future Major Roadways (Source: DKS Associates)
- * East West Connector Access Point
- 5 Foot Contours (Source: RLS 2014)
- Stream (Source: RLS 2014)
- Wetland (Source: Fregonese Associates and RLS)
- FEMA 100 Year Flood Areas Updated by Metro (Source: RLS 2014)
- Slopes (Source: Fregonese Associates)
 - 10% and above (generally unsuitable for industrial development)
 - 25% and above (unsuitable for any development)
 - PGE and BPA Elements and Properties (Source: PGE and BPA)

TEAM MEMBERS
COMMENTS
BIG IDEAS



0 10000 400 800 1600 Feet
2013 Aerial provided by Metro

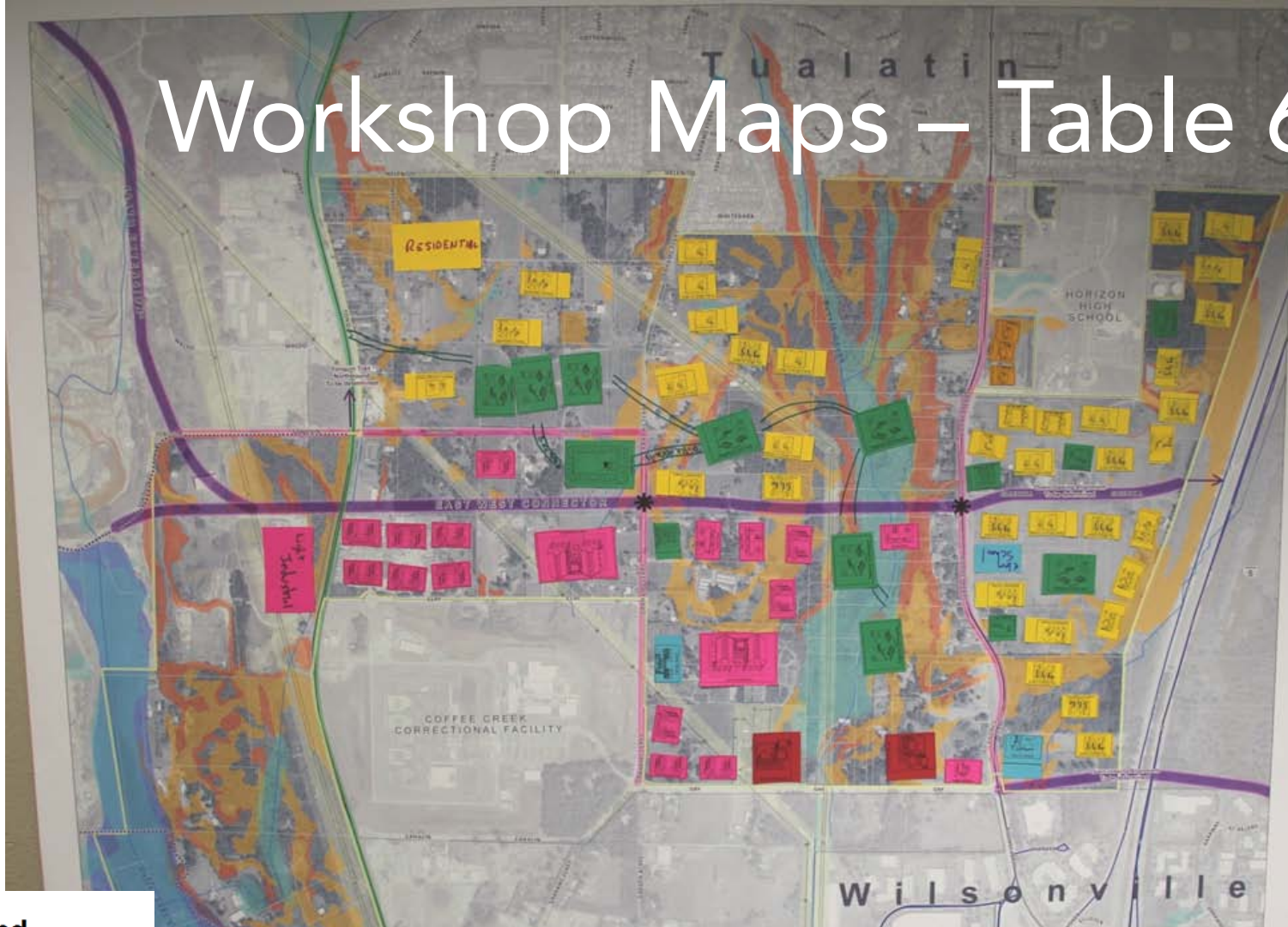


Table 5 Notes

Goals

- Maintain neighborhood continuity
- Comments
- Not great for industrial warehouse land because of transportation access
- No big box, but need small scale grocery for people living in the area (Haggen-sized)
- Big demand for sports fields
- Big Ideas
- WES Station
- Natural area on Basalt Creek (like Tryon Creek)
- Sports Complex
- Clean green industrial flex as buffer to residential

Workshop Maps – Table 6



Legend

- Civic
- Commercial
- Employment
- Mixed Use
- Open Space
- Residential

Basalt Creek

<ul style="list-style-type: none"> City Limits (Source: N/C 2010) Planning Area District (State of Oregon and Wilsonville) Source (Source: N/C 2010) PUD Subdivision Policy (Source: City of Wilsonville) PUD Subdivision (Source: City of Wilsonville) Railroad (Source: N/C 2010) 	<ul style="list-style-type: none"> Proposed via High Temporal Trail WSI Community View (Source: N/C 2010) Basalt Basin (Source: N/C 2010) Basalt Basin (Source: N/C 2010 and Revised) Future Roadway Improvement (Source: WSI Assessment) Future Alignment of Future Major Roadway (Source: WSI Assessment) * Look West (Connective Roadway) 	<ul style="list-style-type: none"> 3-4 Foot Curbless (Source: N/C 2010) Street (Source: N/C 2010) Walkway (Source: Engineering Associates and N/C) Access (See that Road Item updated by Wilsonville N/C 2010) Water (Source: Progress Associates) Utility and other (generally consistent for industrial development) PUD and other (available for site development) PUD and other (available for site development) PUD and other (available for site development)
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#6

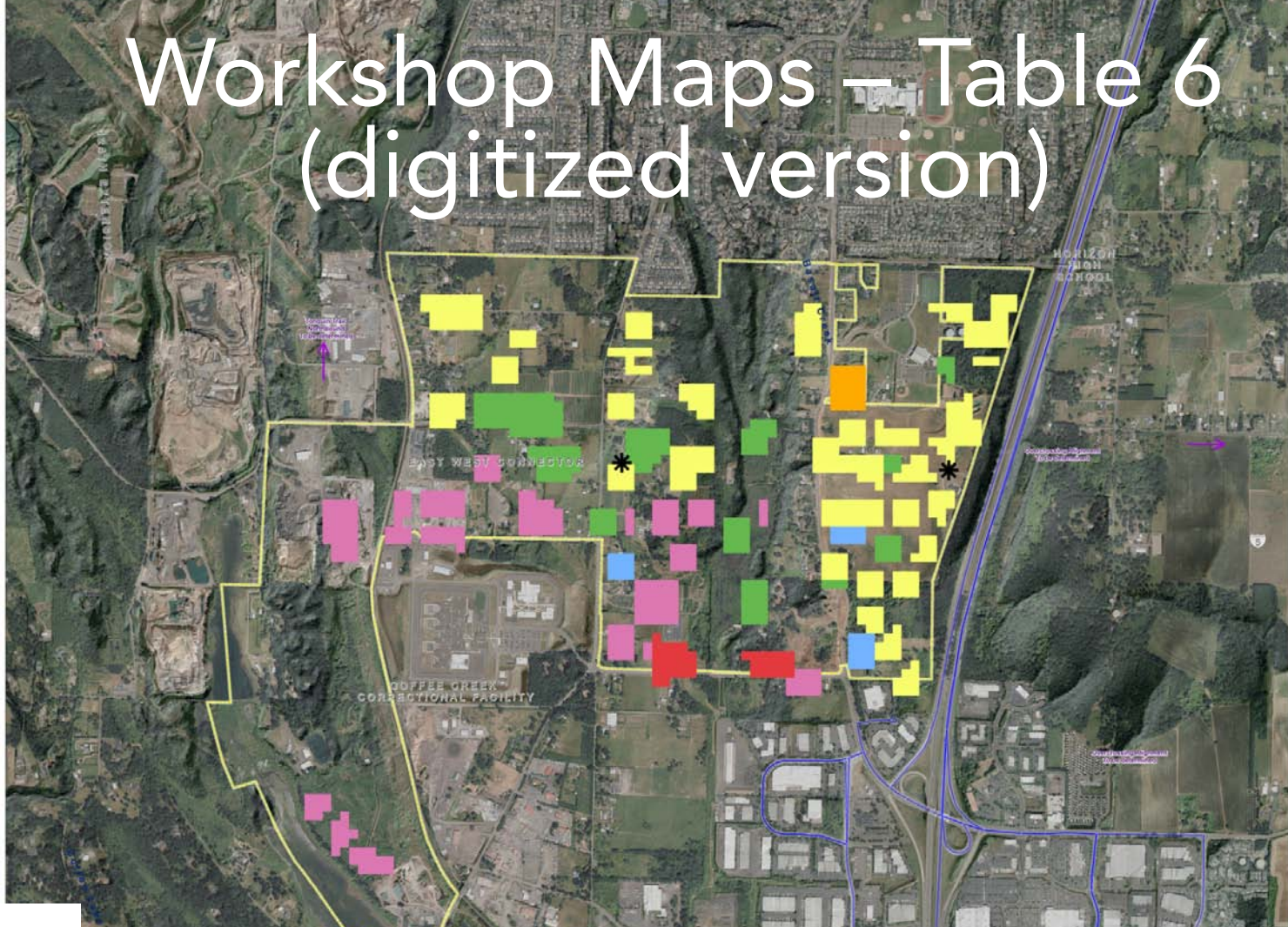
TEAM MEMBERS COMMENTS BIG IDEAS

N
E
S
W

0 100 200 400 600 800 Feet
 N/C 2010
 All rights reserved by WSI



Workshop Maps – Table 6 (digitized version)



Legend

- Civic
- Commercial
- Employment
- Mixed Use
- Open Space
- Residential

Basalt Creek

<ul style="list-style-type: none"> City Limits (Source: RUS 2014) Planning Area (Source: Cities of Tualatin and Wilsonville) Zoners (Source: RUS 2014) PGE Transmission Poles (Source: City of Wilsonville) PGE Transmission Lines (Source: City of Wilsonville) Railroads (Source: RUS 2014) 	<ul style="list-style-type: none"> Proposed Ice Age Tomquin Trail WES Commuter Line (Source: RUS 2014) Bicycle Routes (Source: RUS 2014) Bus Lines (Source: RUS 2014 and Metro) Future Roadway Improvements (Source: DKS Associates) Approx. Alignment of Future Major Roadways (Source: DKS Associates) * East West Connector Access Point 	<ul style="list-style-type: none"> 5 Foot Contours (Source: RUS 2014) Stream (Source: RUS 2014) Wetland (Source: Frogness Associates and RUS) FEMA 100 Year Flood Areas (Updated by Metro (Source: RUS 2014)) <p>Slopes (Source: Frogness Associates)</p> <ul style="list-style-type: none"> 10% and above (generally unsuitable for industrial development) 20% and above (unsuitable for any development) PGE and BPA Easements and Property (Source: PGE and BPA)
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TEAM MEMBERS
COMMENTS
BIG IDEAS



0 100 200 400 600 800 Feet
2013 Aerial provided by Metro



Table 6 Notes

Goals

- Get people to live near their work!
- Offer more opportunities/options for sports field
- Connect neighborhood amenities/green spaces (i.e. walking/bike trails)
- Small parks in residential areas
- Maintain rural setting/provide safety/comfort

Table 6 Notes cont.

Ideas:

- Clustering of apartments/retail/parks
- Definitive boundaries – buffer zone (greenbelt)
- Trails, bike paths
- Neighborhood parks with multiple uses
- WES Station
- Easy access to freeway
- Community parks and gardens
- Assisted living centers
- Retail near intersection
- Industrial area down south
- G.F/E-R to ferry all residential
- Retail opportunity in front of school

Basalt Creek Workshop and Online Survey Results

Highlights:

WHO WAS THERE/WHO RESPONDED

Workshop

- About 45 participants who also answered most of the instant polling questions
- Usual suspects in terms of age (older)
- More Tualatin residents (45%) than Wilsonville residents (20%)
- A quarter (25%) of participants live in the planning area
- Most heard about the workshop via email (62%)

Survey

- 159 respondents total
- Results presented here are only responses of those who did not attend the workshop
- More Wilsonville residents (37%)
- Only 10% live in the planning area
- Respondents generally younger than workshop participants. Far more responses from those age 31-55, and 9% of the responses were from 19-31-year-olds (which were missing from workshop)

RETAIL

Workshop

- Some support for retail in the area overall; more thought it was important than unimportant, but not by a large margin (49% vs. 37%)
- Large majority (93%) against big box retail, not many supported corner/convenience retail
- Most supported retail types were neighborhood and internally oriented to campus employees – though internally – oriented retail also had the largest number of neutral responses, maybe because people are not familiar with this type of retail
- Slightly over half supported mixed-use (54%)

Survey

- More focus on importance of retail (61% thought it was important)
- Slightly more support for strip retail (46% thought it was appropriate for the area, vs. 38% from the workshop)
- Less adamantly opposed to big box retail (only 70% thought it was inappropriate, as opposed to 93% of workshop participants)
- Most appropriate type of retail identified was neighborhood retail (67%, compared with 59% from workshop). Strip commercial and corner/convenience store followed with 46% each deeming them appropriate for the area.

OTHER COMMERCIAL

Workshop

- Strong support for coffee shops (71%) and farm stands (68%)
- Mixed opinions on restaurants and supermarket (about half and half)
- Under half (45%) supported medical services, though this type also received the largest number of neutral votes (19%) out of “other commercial” category

Survey

- Most popular was restaurant (70% deemed appropriate)
- Most support for coffee shops (64%) and farm stands (62%)
- Least support for contractor’s supplies and services (only 25% thought it was appropriate)
- 44% each though supermarkets and medical services were appropriate

EMPLOYMENT

Workshop

- The top three employment types considered somewhat or very appropriate for Basalt Creek were industrial flex space (79%), office (75%) and small-scale retail (74%).
- Both light industrial and warehousing were deemed generally appropriate as well by most participants (66% and 55% respectively)

Survey

- Most support was for small-scale retail (68%), a mix of all employment types (54%), office (49%) and light manufacturing (47%)
- Not as much support for warehousing (29%) as in the workshop (66%)

PLAN FOCUS

Workshop

- Almost all of the themes presented were seen by most participants as somewhat or very important. The top rated were housing (88%), jobs (87%), and parks (81%).
- Transportation choices (80%) and protecting natural resources (79%) were not far behind
- Protection of/access to historic resources and public access to Coffee Creek Lake and Basalt Creek were 59% and 54%, respectively

Survey

- All items were supported by 70% or more of respondents, except for housing (54% thought it was somewhat or very important, significantly less than at the workshop)
- Top-rated were natural resources (85%), parks (79%), , environmental protection and public access to Coffee Lake and Basalt Creeks (71%), historical resources (71%)
- Transportation options and jobs were deemed important by 70% of respondents (significantly less than at workshop)

LEVEL OF AGREEMENT, PROPORTIONATELY (IN ORDER OF MOST TO LEAST)

Workshop

- No big box retail (93%)
- Housing important (88%)
- Jobs important (87%)
- Park important (51%)
- Transportation choice important (80%)
- Support for industrial flex space employment (79%)
- Protection of natural resources important (79%)
- Support for office employment (75%)
- Support for small-scale retail employment (74%)
- Support for Coffee shops (71%) and Farm Stands (68%)
- Support for light manufacturing employment (66%)
- Support for retail internally oriented to employees on a campus (64%)
- No corner/convenience retail (59%)
- Support for neighborhood retail (59%)
- Support for restaurants (55%)
- Support for warehousing employment (55%)
- Support for public access to Coffee Lake and Basalt Creek (54)
- Support for mixed use retail (54%)
- No supermarket (50%)
- Support for overall retail in Basalt Creek (47%)
- Support for medical services (45%)
- Support for restaurants (43%)
- Support for strip retail (38%)
- Support for a mix of all employment types (38%)

Survey

- Support for protection of natural resources (85%)
- Support for parks (79%)
- Support for environmental protection (76%)
- Public access to Coffee Lake and Basalt Creeks (71%)
- Protection of/access to historic resources in Basalt Creek (71%)
- No big box (70%)
- Support for restaurants (70%)
- Support for transportation choices (70%)
- Support for jobs (70%)
- Support for small-scale retail employment (68%)
- Support for neighborhood retail (67%)
- Support for coffee shops (64%)

- Support for farm stands (62%)
- Support for retail services in general in Basalt Creek (61%)
- No warehousing employment (58%)
- Support for office employment (55%)
- Support for housing (54%)
- Support for a mix of all employment types (54%)
- No Contractors' Supplies and Services (47%)
- Support for light manufacturing employment (47%)
- Support for strip retail (46%)
- Support for corner/convenience retail (46%)
- Support for medical services (44%)
- Support for supermarket (44%)
- No Industrial flex space employment (42%)
- No retail internally oriented to employees on a campus (33%)

MOST HIGH/LOW RESPONSES (PROPORTIONATELY)

Workshop

- Housing very important (65%)
- Protection of natural resources very important (61%)
- Jobs very important (55%)
- Parks very important (51%)
- Industrial Flex Space Employment very appropriate (48%)
- Transportation choice very important (42%)
- Light manufacturing employment very important (38%)
- Warehousing employment very appropriate (35%)

Survey

- Protection of natural resources very important (66%)
- Emphasis on parks very important (49%)
- Environmental protection very important (54%)
- Protection of/access to historic resources very important (48%)
- Public access to creeks very important (42%)
- Transportation choices very important (41%)
- Industrial flex space employment very inappropriate (25%)
- Housing very unimportant (20%)

Detailed Results

WHO WAS THERE/WHO RESPONDED

Workshop

- Almost half (46%) of participants (42 answered the age question) were between ages of 56 and 70. According to the polling, there was no one under 30 present at the workshop. 15% were older than 70, and the rest (39%) were between 31 and 55.
- Most (62%) had participated in a public workshop before (however, only 39 answered this question, though ultimately there were at least 45 participants – it is unclear because there were a few attendees that did not sign the sign-in sheet).
- Most heard about the workshop via email (62%). 19% heard about it through the project or city website. Only 2% heard about the workshop via social media (Facebook or Twitter). 5% saw a flyer that alerted them to the workshop.
- 29% of workshop attendees said they own property in the area, 10% said they live in the area, and 5% said they work in the area. However, it is possible that more people do each of these things in the area, as 44% selected the answer “I do more than one of the above near the area” (in retrospect, this was a poorly worded question and is minimally useful for analysis).
- The most participants live in Tualatin (45%), while 25% live in the Basalt Creek planning area (currently unincorporated Washington County). Only 20% were residents of Wilsonville, while 10% live in a different city/area.

Survey

- Over half (60%) of respondents were between ages 31 and 55. Only 24% were age 56-70. 9% of responses came from 19-30-year-olds. 7% were older than 71.
- Residency was more balanced among survey respondents than workshop attendees – 37% were Wilsonville residents and 33% lived in Tualatin. 20% lived in some other area.
- Only 10% lived in the Basalt Creek planning area itself, though when asked about primary connection to the planning area, 64% responded that they live in the area (presuming they mean in the vicinity of the planning area). 13% own properties in the area, 8% work in the area and 13% do none of the above/other. The “I do more than one of the above near the area” option was eliminated from the survey questions.
- Most heard about the survey via email (39%) or social media (30%). 11% found it via a City or project website, and 9% through word-of-mouth.

WORKSHOP - RETAIL

- General: About a third of participants thought having retail services in Basalt Creek for existing and future neighborhoods was somewhat important, while 17% said it was very important. Overall, 49% thought retail for neighborhoods had some level of importance, while 37% thought it was somewhat or very unimportant. 15% were neutral.
- Strip Retail: Asked whether strip retail was appropriate for the Basalt Creek area, opinions were fairly evenly divided between yes (38%), no (28%) and neutral (26%). Only 8% were not sure.

- Neighborhood Retail: Most people thought neighborhood retail was appropriate for Basalt Creek (59%). 27% said it was not appropriate and 15% were neutral.
- Convenience/Corner Stores: Most (59%) said convenience or corner stores were not appropriate for Basalt Creek. 32% thought it was appropriate, 10% were neutral.
- Big Box: An overwhelming majority (93%) of participants did not think big box retail was appropriate for Basalt Creek.
- Internally oriented to employees on a campus: Most thought this type of retail was appropriate for Basalt Creek (64%). Only 13% said it was not, while 23% were neutral. The concept may have not been very familiar to many workshop participants.
- Mixed-Use (ground floor retail with office or housing above): A slight majority said mixed use was appropriate for Bas alt Creek (54%), while 34% said it was not appropriate. 10% were neutral and 2% were not sure.

SURVEY - RETAIL

- General – Over 60% thought having retail in the Basalt Creek area was important – either very important (30%) or somewhat important (31%). 22% thought it was somewhat or very unimportant, and 16% were neutral.
- Strip Retail: 46% thought this was appropriate, and 27% did not. 17% were neutral.
- Neighborhood Retail: 67% thought this was appropriate. 13% said it was not, and 14% were neutral.
- Convenience/corner stores: 46% thought this was appropriate, while 25% did not think so. 27% were neutral, one of the largest neutral votes in the survey.
- Big Box: Most did not think big box retail was appropriate for the planning area (70%) - less than had that opinion at the workshop. 15% responded that it was appropriate, and 13% were neutral.
- Internally oriented to employees on a campus: Opinions were fairly evenly split about this kind of retail – 32% supported it, 33% did not, and 23% were neutral. 13% were unsure, possibly indicating that many were not familiar with this type of retail.
- Mixed-Use: didn't ask this question in the survey.

OTHER COMMERCIAL

Workshop

- Supermarket: Half (50%) of workshop participants said a supermarket was not appropriate for Basalt Creek. 43% thought that it was, while 5% were neutral.
- Restaurants: 55% of participants thought restaurants were appropriate for Basalt Creek, while 38% said they were not and 7% were neutral.
- Medical Services: 45% thought medical services were appropriate for the area, 36% said they were not, and 19% were neutral.

- Farm Stands: There was strong support for farm stands, with 68% of participants agreeing they were appropriate for Basalt Creek. 23% thought they were not, while 7% were neutral and 2% were unsure.
- Coffee Shops: A large majority (71%) thought coffee shops were appropriate for the Basalt Creek area.
- Contractor's Supplies and Services: 56% did not think this was an appropriate commercial use for Basalt Creek, while 33% thought it was. 9% were neutral

Survey

- Supermarket: 44% supported a supermarket in the study area. 34% did not support this idea. 17% were neutral.
- Restaurants: 70% supported restaurants. Only 10% said this was not appropriate for the area, and 15% were neutral.
- Medical Services: 44% thought medical services were appropriate, while 30% did not. 20% were neutral.
- Farm Stands: 62% though farm stands were appropriate. 13% did not, while 21% were neutral.
- Coffee Shops: 64% thought coffee shops were appropriate, and 11% did not. 22% were neutral.
- Contractor's Supplies and Services: 47% said this type of retail was not appropriate for this area. 25% said it was, and 21% were neutral.

EMPLOYMENT

Workshop

- Industrial Flex Space: 48% thought this was very appropriate, and 31% thought it was somewhat appropriate. Only 17% thought it was very or somewhat inappropriate (10 and 7%, respectively).
- Office: The most participants thought office employment was somewhat appropriate for the area (49%) while 26% thought it was very appropriate. Only 11% thought it was somewhat or very inappropriate for the area.
- Small-Scale Retail: 74% thought this employment type was appropriate, though more chose somewhat appropriate (51%) than very appropriate (23%).
- Light Manufacturing: 66% thought this employment type was very or somewhat appropriate. 27% thought it was inappropriate. 8% were neutral.
- Warehousing: 55% thought this type of employment was either somewhat or very appropriate; with more saying they thought it was very appropriate (35%). 35% thought it was somewhat or very inappropriate, with only 10% saying it was very inappropriate. 10% were neutral.
- Mix of All: Responses were evenly split between appropriate (38%), inappropriate (35%) and neutral (27%).

Survey

- Office: 55% thought it was either somewhat (29%) or very (26%) appropriate. 34% thought it was somewhat (21%) or very (13%) inappropriate

- Warehousing: 38% thought this type of employment was very inappropriate for the area, and 20% thought it was somewhat inappropriate. Only 29% thought it was very or somewhat appropriate, while 13% were neutral.
- Light Manufacturing: 47% thought this type of employment was very (19%) or somewhat (28%) appropriate for the area. 27% thought it was very (12%) or somewhat (15%) inappropriate. 8% were neutral.
- Industrial flex space: 42% thought this was very (25%) or somewhat (17%) inappropriate for the area. 39% thought it was very (16%) or somewhat (23%) appropriate. 20% were neutral.
- Small-scale retail: 68% thought small-scale retail was very (32%) or somewhat (36%) appropriate for the area. 18% were neutral, while 15% thought it was very (7%) or somewhat (8%) inappropriate.
- Mix of all: 54% thought this was appropriate, and 24% thought it was inappropriate. 22% were neutral.

PLAN FOCUS

Workshop

- Housing: 65% said this was very important, and 23% said it was somewhat important. Only 4% thought it was somewhat or very unimportant.
- Jobs: 87% said somewhat or very important, with over half overall saying it was very important (55%). 9% were neutral, while only 4% said it was somewhat or very unimportant.
- Parks: 81% said this was somewhat or very important, with over half of participants saying it was very important (note that only 37 participants responded to this question, as it was worded incorrectly and a bit hard to understand)
- Transportation Choices: 42% said very important and 38% said somewhat important. 16% were neutral. Only 4% thought this theme was somewhat or very unimportant.
- Protect natural resources: 61% said very important and 18% said somewhat important. 11% were neutral, while only 9% said somewhat or very unimportant
- Environmental protection: 72% said somewhat or very important, about evenly split between the two (35 and 37%, respectively). 9% were neutral and 19% said this was somewhat or very unimportant (5 and 14% respectively).
- Protection of/access to historic resources in Basalt Creek: 59% thought this theme was somewhat or very important, with slightly less saying very (27%) than somewhat (32%). 17% were neutral, and another 17% thought this was very unimportant. 7% thought it was somewhat important.
- Public Access to Coffee Lake and Basalt Creek: 54% thought this was either somewhat or very important. 25% thought it was somewhat or very unimportant. 20% were neutral.

Survey

- Protect natural resources: 85% said this was very (66%) or somewhat (19%) important. Only 8% said it was somewhat or very unimportant, and 8% were neutral.

- Parks: 49% said parks were very important, and an additional 30% said it was somewhat important (total of 79%). Only 9% said it was somewhat or very unimportant.
- Environmental protection: 76% thought it was very (54%) or somewhat (22%) important. Only 11% thought it was unimportant, and 14% were neutral.
- Public access to Coffee Lake and Basalt Creeks: 71% thought this was very (42%) or somewhat (29%) important. 13% thought it was unimportant, and 17% were neutral.
- Transportation Choices: 70% thought transportation choices were very (41%) or somewhat (29%) important. Only 16% thought it was unimportant. 15% were neutral.
- Protection of/access to historic resources: 71% thought it was very (48%) or somewhat (23%) important. 13% thought it was unimportant, and 16% were neutral.
- Jobs: 70% said this was either very (32%) or somewhat (38%) important. Only 15% said it was somewhat or very unimportant. 15% were neutral.
- Housing: Only 21% thought this was very important, and 33% thought it was somewhat important (total of 54%). 31% thought it was somewhat or very unimportant and 15% were neutral.

Map Exercise

Game pieces/Development Chips

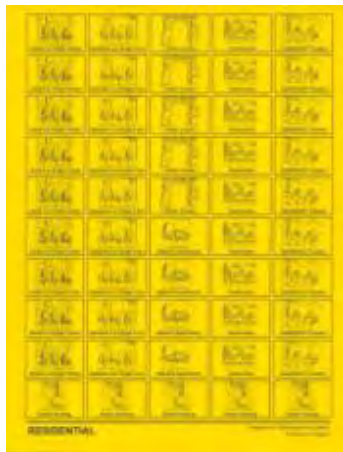
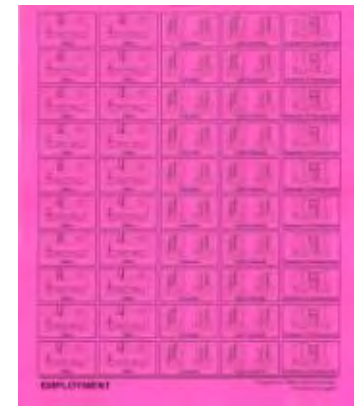
Mixed Use



Commercial



Employment



Residential

Civic

Open Space

Mixed Use

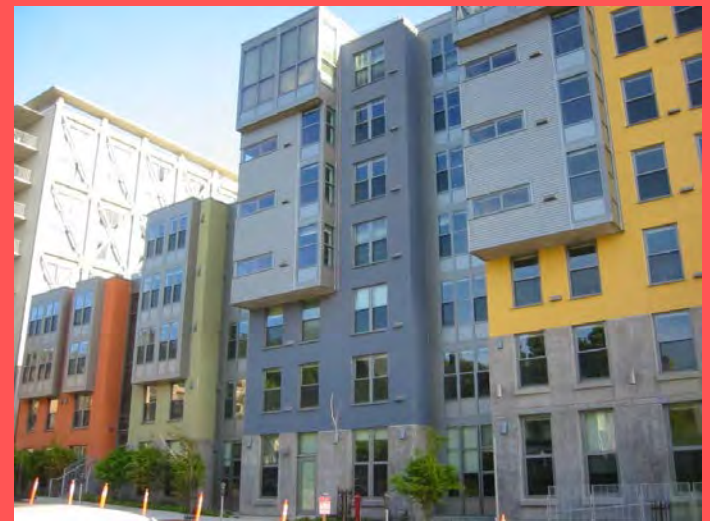
Housing
over
retail



Mixed Use



Housing over retail



Mixed Use

Office over retail



Mixed Use



Employment Types

Office



Employment Types



Light industrial and flex space



Employment Types



Warehousing



Commercial

Restaurants



Commercial

Cafes, retail shops and grocery stores



Residential

Small Lot Single Family



Residential

Large Lot Single Family



Residential

Cottage Homes



Residential



homes



Residential

Apartments and Condos, and Assisted Living



Civic

Libraries, parking, community centers



Civic

Senior Centers, Traffic calming



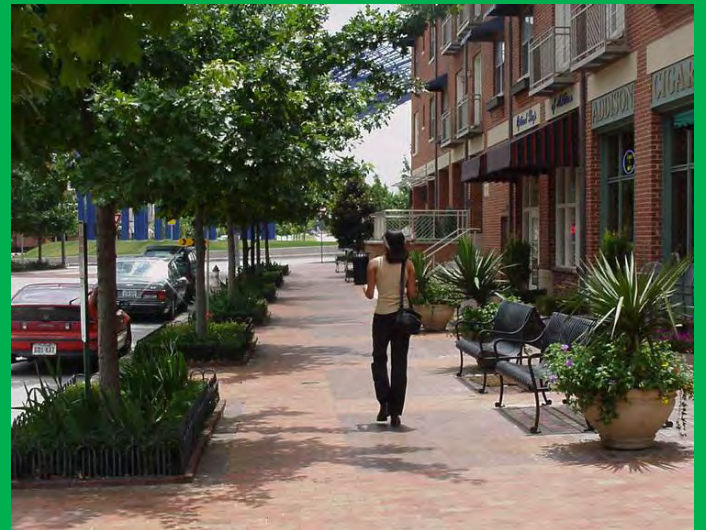
Open Space

Pedestrian and bicycle paths, trails, parks



Open Space

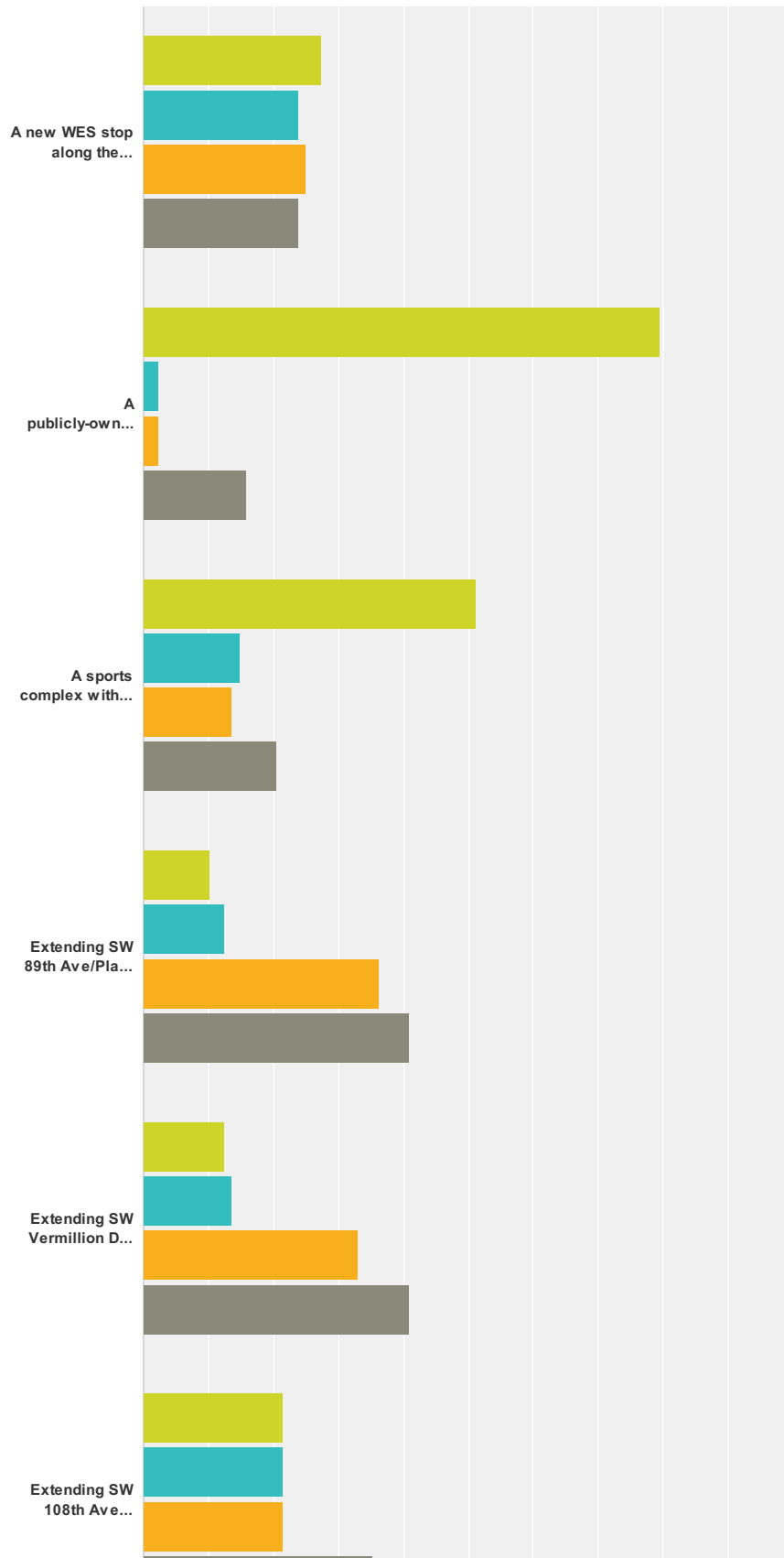
menades, playgrounds



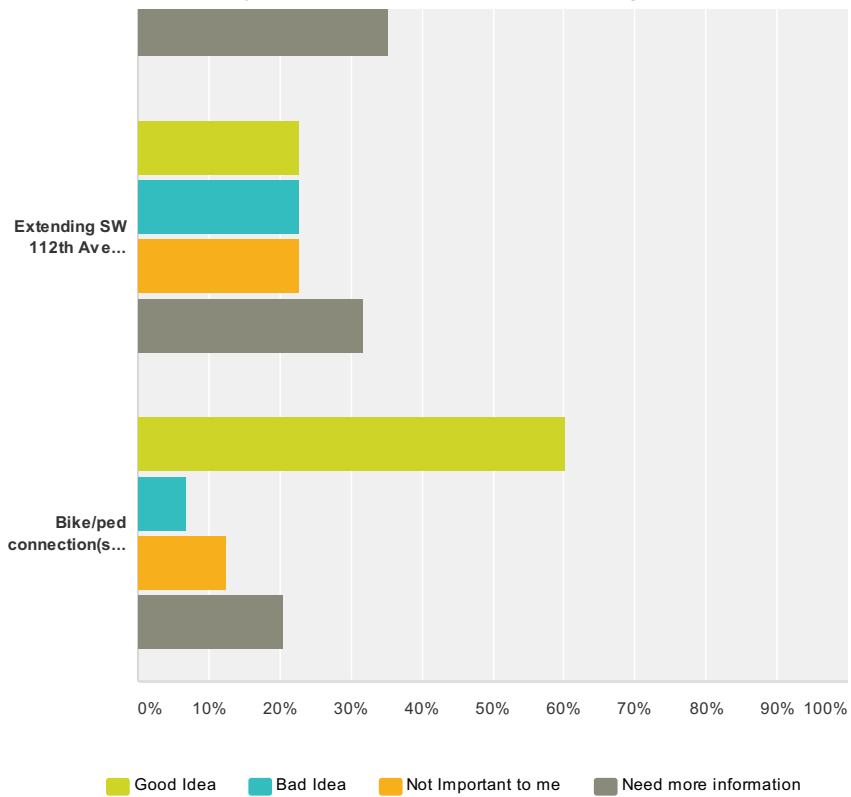
CIO Survey for Basalt Creek Planning Area

Q1 Several "big ideas" were discussed at the BCP Workshop on July 17. Please indicate your opinion on each of the following "big ideas" for features in the Basalt Creek Area:

Answered: 88 Skipped: 0



CIO Survey for Basalt Creek Planning Area



	Good Idea	Bad Idea	Not Important to me	Need more information	Total
A new WES stop along the current tracks/route, south of present-day Tualatin	27.27% 24	23.86% 21	25.00% 22	23.86% 21	88
A publicly-owned, large nature park/trail/refuge for the Basalt Creek Natural Area, which runs nearly the entire north-south distance between Victoria Woods (north) to Day Road (south), approximately 1,000 feet west of Boones Ferry Road.	79.55% 70	2.27% 2	2.27% 2	15.91% 14	88
A sports complex with all-weather fields	51.14% 45	14.77% 13	13.64% 12	20.45% 18	88
Extending SW 89th Ave/Place southward	10.23% 9	12.50% 11	36.36% 32	40.91% 36	88
Extending SW Vermillion Dr southward	12.50% 11	13.64% 12	32.95% 29	40.91% 36	88
Extending SW 108th Ave southward	21.59% 19	21.59% 19	21.59% 19	35.23% 31	88
Extending SW 112th Ave southward	22.73% 20	22.73% 20	22.73% 20	31.82% 28	88
Bike/ped connection(s) to the Ice Age Tonquin Trail (west of RR tracks)	60.23% 53	6.82% 6	12.50% 11	20.45% 18	88

CIO Survey for Basalt Creek Planning Area

Q2 Please describe any "big ideas" you would like to have considered for the Basalt Creek Area. If none, please hit the "Next" button (below) to continue the survey.

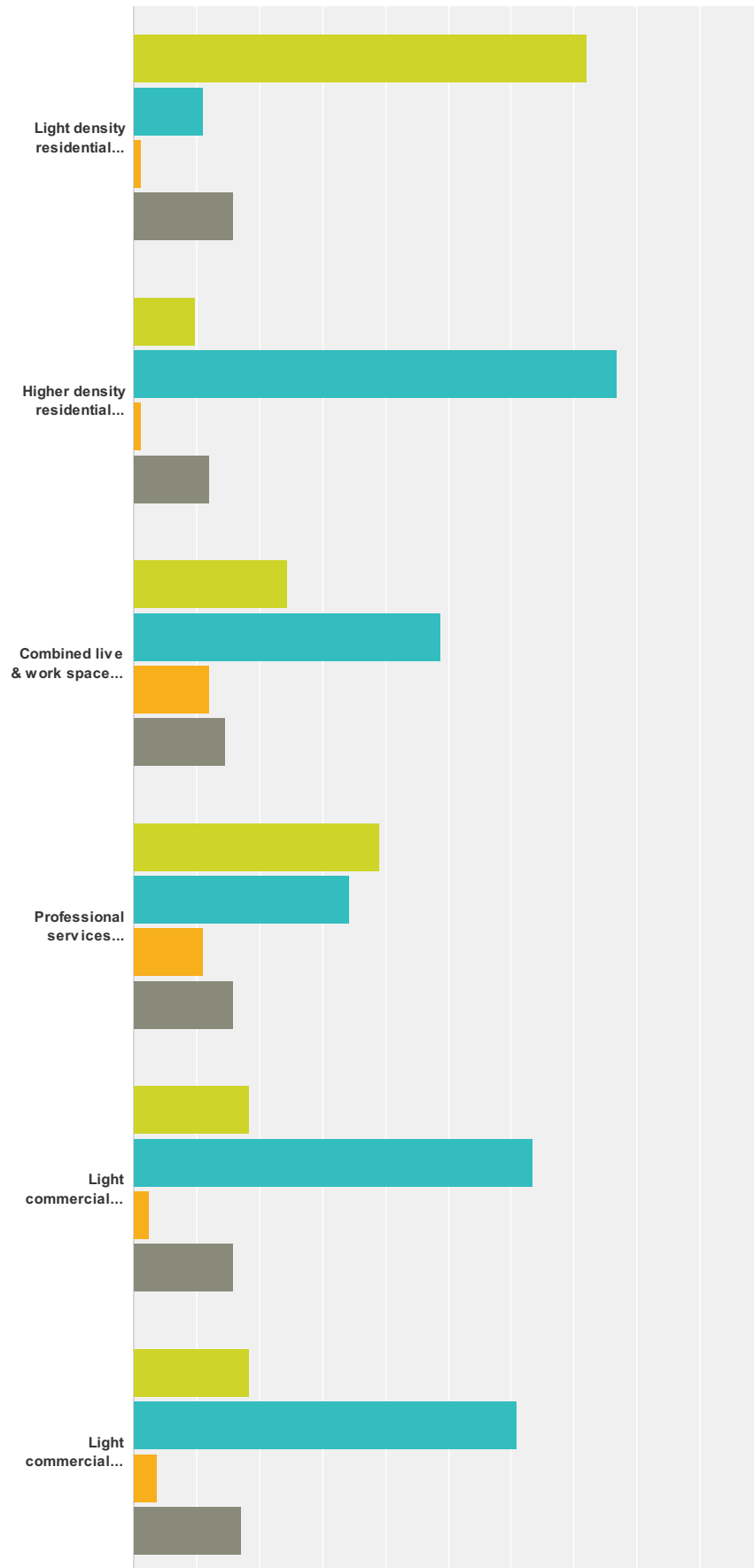
Answered: 18 Skipped: 70

#	Responses	Date
1	Obtain the blighted area tracts East of the RR tracks just North of Tonquin Road and following Tonquin Loop then just West of 112th leading into the Nootka Street area to provide for a rerouted Ice Age Tonquin Trail with a trail head with a park along the Kolk Pond East of the RR Tracks and connectivity to the Ibach Neighborhood. This would help serve as a buffer between the RR Tracks and future Basalt improvements. Put a future WES Station off Clay Street to access the prison and the South West Concept Plan and Basalt Planning Areas. Keep in mind that with RR Tracks, 124th, Grahams Ferry Rd. improvements that all increase traffic and or noise, neighborhoods should be discouraged West of Grahams Ferry Road.	7/10/2014 9:55 PM
2	The most important thing is to reduce the traffic on Tualatin Sherwood Road!	7/9/2014 10:55 AM
3	Extend SW Helenius Rd. west to SW 112th.	7/8/2014 2:01 PM
4	Reduce the heavy traffic on Grahams Ferry Road (in particular heavy trucks) North of Tonquin Road. Maintain or improve I-5 access for residents on both sides of Grahams Ferry Road. If the area South of Victoria Woods is developed as suggested in the previous page, have better access to this area for residents of Victoria Gardens. Do not forget the pedestrian crossing on Grahams Ferry Road near Dogwood St.	7/2/2014 3:23 PM
5	We need a light at the corner of Grahams Ferry and Helenius anything is built back there encouraging additional traffic.	7/1/2014 11:05 AM
6	A controlled cross-walk crossing Grahams Ferry Road near or north of Victoria Gardens.	6/30/2014 8:51 PM
7	postpone the north overcrossing of I-5 until after the Day Road overcrossing has been in service long enough to determine if the north overcrossing is even necessary.	6/28/2014 8:34 PM
8	Connection to I-5 from east-west connector	6/28/2014 7:39 PM
9	This area needs to be kept family friendly. No businesses, no big roads. Keep homes, parks, and nature areas.	6/27/2014 7:24 AM
10	protect existing residential areas from noise and pollutants caused directly or indirectly by industrial development.	6/26/2014 11:43 PM
11	Protect residential housing from excessive noise and traffic. Protect housing areas from pollutants.	6/26/2014 10:46 PM
12	Eliminate truck traffic on Boones Ferry Road from Norwood north. Force trucks to use a bypass and/or I-5 to Tualatin-Sherwood Road. There are too many neighborhoods and children/pedestrians using Boones Ferry and this should be treated like a neighborhood road..not to mention the school zones.	6/26/2014 12:49 PM
13	I think this plan also needs to consider the growth and commuter traffic which will heavily use this corridor from Sherwood, points west of Sherwood and Yamhill County. As someone who lives in South Tualatin and works in Newberg I have seen the commuter traffic (thankfully often going the opposite direction) as well as trucking traffic increase significantly in recent years. While this plan helps divert traffic trying to reach I5 South away from the core of Tualatin when coming from the west and 99W along TS Road (via 124th extension and new East-West connector) it does little to divert all of the traffic from still having to go through limited routes in Sherwood (either cutting through neighborhoods via Sunset / Murdoch to Tonquin) or adding more traffic to an overburdened T-S Road, which isn't just the Tualatin part, the entire length. Consideration needs to be given to have this East-West Connector find a way all the way to 99W somewhere south of Sherwood. This would instantly ease traffic problems on 99W and T-S road which will only continue to get worse as Sherwood and Yamhill County grow. Any such road would instantly be the preferred route to I-5 to and from the bulk of Sherwood and its surroundings as well as all of the commuter traffic going to and from Newberg and McMinnville.	6/26/2014 12:43 PM
14	Develop a Plaza to host Farmers Markets, concerts, Community events, etc.	6/26/2014 8:00 AM
15	Commercial/Residential mix similar to Villa Bois or Santana Row (San Jose)	6/25/2014 5:20 PM
16	Look into the future to consider the long-term impacts of more homes, more commercial, and more congestion from this development. Too much growth can reduce the quality of living. In any development plan, please include lots of landscaping and natural areas to attract wildlife, insects, birds, and flora diversity. Include lots of trees to replace what has been removed ! Encourage pedestrian and local residents to walk, bike, or enjoy the space (such as jogging through the area). Walking or jogging paths. Encourage walking with aesthetic paths and quiet spaces away from the streets and roads. Design any roads to meander rather than be straight. This helps by reducing speed, improving the safety, and improving the visual impact of the development.	6/25/2014 4:29 PM
17	In line with the "local village" concept, it would be nice to have some small convenience retail in the area—a small shopping center with convenience store, pizza place, sandwich shops, etc. The nearest of these at present are 2 miles north or south. Would be nice if they were within walking/biking distance.	6/25/2014 3:04 PM
18	It would be nice to have a few small businesses nearby. Our current neighborhood is very car-dependent for even the most minor of errands. It would also be nice if there were some walking or bike trails or, at least, a place to safely walk or bike along any new or existing roads.	6/24/2014 9:39 PM

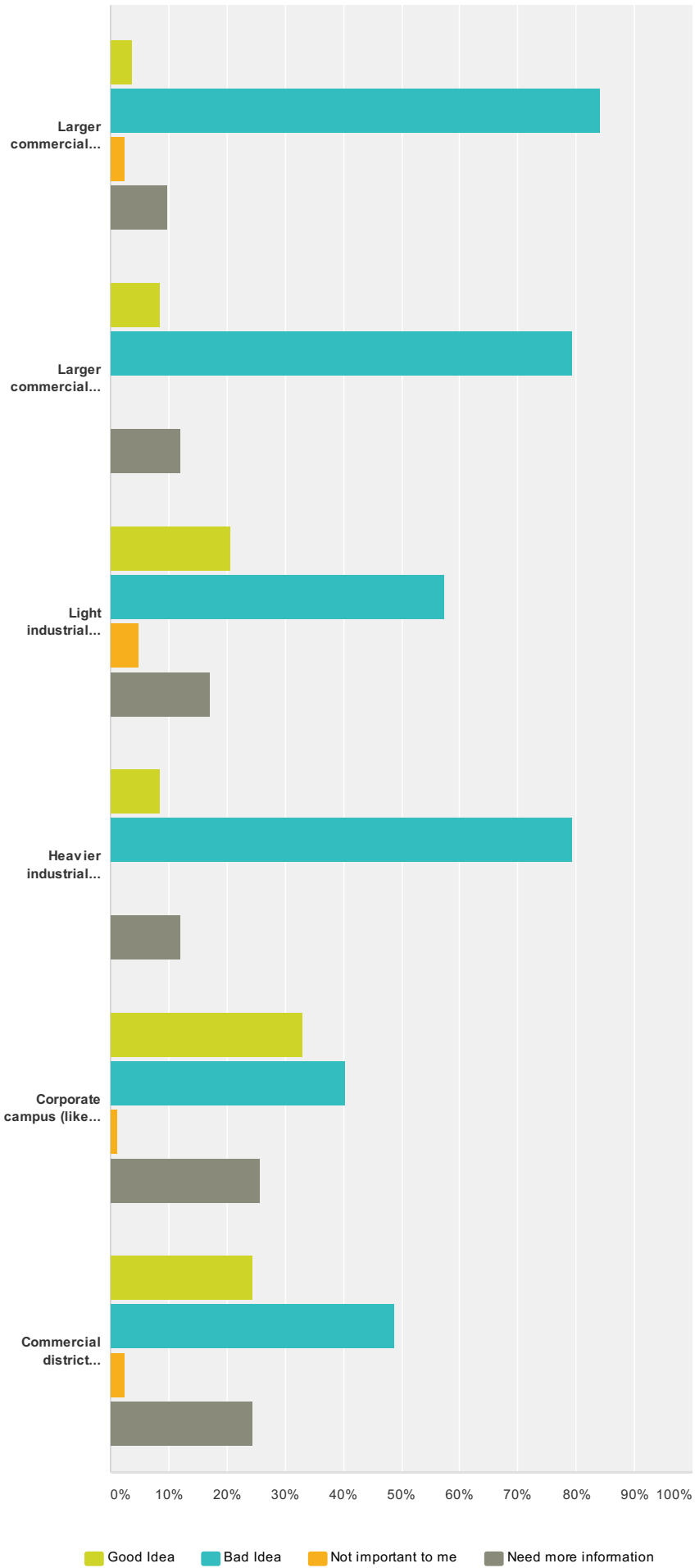
CIO Survey for Basalt Creek Planning Area

Q3 Please indicate your opinion on the desirability of the following zoning/density options in the Basalt Creek Area:

Answered: 82 Skipped: 6



CIO Survey for Basalt Creek Planning Area



CIO Survey for Basalt Creek Planning Area

	Good Idea	Bad Idea	Not important to me	Need more information	Total
Light density residential (min. 5000 sq. ft. lots)	71.95% 59	10.98% 9	1.22% 1	15.85% 13	82
Higher density residential (includes apartments, condos, row houses, etc)	9.76% 8	76.83% 63	1.22% 1	12.20% 10	82
Combined live & work spaces (commercial at ground level, residential above)	24.39% 20	48.78% 40	12.20% 10	14.63% 12	82
Professional services (doctors, lawyers, etc.)	39.02% 32	34.15% 28	10.98% 9	15.85% 13	82
Light commercial (strip mall-style retail) along Grahams Ferry Road	18.29% 15	63.41% 52	2.44% 2	15.85% 13	82
Light commercial (strip mall-style retail) along Boones Ferry Road	18.29% 15	60.98% 50	3.66% 3	17.07% 14	82
Larger commercial (big-box stores, etc.) along Grahams Ferry Road	3.66% 3	84.15% 69	2.44% 2	9.76% 8	82
Larger commercial (big-box stores, etc.) along Boones Ferry Road	8.54% 7	79.27% 65	0.00% 0	12.20% 10	82
Light industrial (flexible space, warehouse, etc.)	20.73% 17	57.32% 47	4.88% 4	17.07% 14	82
Heavier industrial (fabrication, manufacturing, etc.)	8.54% 7	79.27% 65	0.00% 0	12.20% 10	82
Corporate campus (like Mentor Graphics or Xerox in Wilsonville)	32.93% 27	40.24% 33	1.22% 1	25.61% 21	82
Commercial district surrounding a new WES stop to serve employment center(s)	24.39% 20	48.78% 40	2.44% 2	24.39% 20	82

CIO Survey for Basalt Creek Planning Area

Q4 Please describe any other ideas for zoning or density options. If none, please hit the "Next" button (below) to continue the survey.

Answered: 17 Skipped: 71

#	Responses	Date
1	Current road and access would not allow for greater density, more population, and/or businesses.	7/13/2014 1:13 PM
2	Road improvements?? access, ingress and egress to the area?? Current road will not service any additional development, population, or development.	7/12/2014 2:05 PM
3	keep residential away from Rail Road Tracks, 124th and Grahams Ferry rd. and Industrial areas and the prison.	7/10/2014 9:59 PM
4	My concern is to prevent large apartment complexes, and to minimize the number of condos. I am not convinced that Tualatin/Wilsonville needs more, or that they improve the community as we enjoy it, nor improve values. Larger minimum residential properties is great.	7/9/2014 5:41 AM
5	I like what is there now. Would prefer not further developing as plans to accommodate extra traffic are questionable. Just look at the Tualatin Sherwood Rd. parking lot. Traffic planning needs to take place before development.	7/8/2014 2:13 PM
6	We live in Tualatin because we didn't want to live in Portland (or any other big city). Don't bring the big city culture to us.	7/2/2014 3:29 PM
7	Please protect property values and residential experience by having a healthy buffer between commercial/retail and existing residential.	7/1/2014 11:08 AM
8	everything 1000' north of Day road should be reserved for residential. Topography makes no sense for other uses. Option to stop residential at tonquin new alignment to Boones Ferry, but prefer that to the line between single family and multifamily before you get to any commercial.	6/28/2014 8:36 PM
9	Higher density, higher traffic options (which draw people from farther away) should be closer to I-5 exit. Smaller retail options would be nice if they are done right, because that has the potential to cut down on # and distance of auto trips for Tualatin residents (the only retail options are on the edges of the city limits or the adjacent city).	6/28/2014 4:19 PM
10	Would like stop signs on dogwood and cottonwood street. cars go way too fast down hill with young kids running around these seem to be the most used east west streets also a traffic light along Graham's ferry at Helenius	6/28/2014 3:45 PM
11	Avoid additional high traffic industry on Boones Ferry due to existing heavy traffic. Too much density seems to increase crime rates.	6/26/2014 10:51 PM
12	Should keep to the character of what is already present. On the N. Wilsonville side having business parks, perhaps warehouse / light industrial is most reasonable. On the Tualatin side single family homes to match the density of Tualatin and/or the Stafford area is most desirable. In Tualatin closer to the 124th extension light industrial / warehouses seems also reasonable. It seems there is more than enough Big - Box and strip malls within 1-5 miles of the area in question to serve its needs but a small business area with say a Grocery store and the similar business that tend to surround would be reasonable. I personally would love to see a sports complex like Tualatin Hills in Walker Rd in Beaverton put in there, but I realize that is probably a pipe dream due to funding issues (but every time I drive down Boones Ferry and see the large open space near Greenhill Lane, that is what I imagine someday being there.	6/26/2014 12:52 PM
13	Tualatin does not need any more development other than a connector to the industrial area on Tualatin Sherwood Road. Keep Basalt Creek underdeveloped. Create a park or preserve.	6/26/2014 12:51 PM
14	No More High Density options!!!! No More Apartments!!!! No more Condos!!!! No more Row houses!!!!	6/26/2014 9:44 AM
15	Cluster developments with green spaces between, vs. continuous sprawling	6/26/2014 8:03 AM
16	Retain lots of open space, park areas, wildlife zones, water features, etc.	6/25/2014 4:32 PM
17	There is a nice forested area behind the Christian School just off Norwood. It would be nice to retain as much of the standing timber there as possible, at least the older trees. We asked (and they complied) the Christian School to retain the older trees along Norwood, and it made for a nice development and view from Norwood heights.	6/25/2014 3:09 PM

CIO Survey for Basalt Creek Planning Area

Q5 Please describe your concerns (noise, pollution, etc.) about development in the Basalt Creek Area. If "none," then you are finished - thank you for participating!

Answered: 44 Skipped: 44

#	Responses	Date
1	There is no thought given to better roads. Access is now difficult, current roads would not support any kind of expansion, growth, and development. Just adding more population and business would not be a solution.	7/13/2014 1:15 PM
2	I see no plans for road improvements, access to the area, ingress and egress?? - without better planning for increased traffic, population, I do not understand how any progress, expansion, or additional development can proceed..	7/12/2014 2:03 PM
3	Proper planning would increase distance between RR Tracks, Industry and major road improvements by keeping residential away from West of Grahams Ferry Road.	7/10/2014 10:01 PM
4	Would prefer no further development as plans to accommodate extra traffic are questionable. Just look at the Tualatin Sherwood Rd. parking lot. Traffic planning needs to take place before development. Other concerns are noise pollution, environmental pollution (air, water, etc.) loss of habitat for wildlife, overpopulation, increased traffic in current residential areas.	7/8/2014 2:15 PM
5	Since it's surrounded by residential, any noise abatement would be preferred.	7/8/2014 1:38 PM
6	I am concerned about creating too much density, traffic and industry and taking away from a community feel. In general Tualatin has done an extremely poor job of planning and has created too many big box stores/malls that don't really support the community feel. I would prefer growth that focuses on creating livability, adds to the culture of the area and supports family owned restaurants and businesses. I have not seen any of that in Wildonville or Tualatin.	7/3/2014 5:37 PM
7	Based on questions and recommendation in the previous pages of this survey, I am even more concerned about noise, pollution, dense living, etc.	7/2/2014 3:32 PM
8	To enhance property values and quality of Tualatin residential life, please avoid any business with pollutants, please respect existing residential by planning buffer space and smart traffic patterns. Crosswalks for all subdivisions to nearest schools are also important and currently lacking, especially along Grahams Ferry. There is currently no safe way for a high school student to walk from Victoria Gardens to the High school. Thank you for considering our input.	7/1/2014 11:11 AM
9	We are brand new to the area and I am in Victoria Gardens. I feel like there are already enough apartment complexes available. I also feel as if there is too much big truck traffic due to the quarries, etc. on Tonquin road. I would not like to see industrial warehousing going in which would increase big vehicle traffic even more. Something that the community could use would be great!	7/1/2014 9:45 AM
10	Concerns about 1) adding traffic to Graham's Ferry and 2) adding competing business to those at Tualatin town square, creating risk to that area becoming much less viable (it appears now to be struggling, and has so much potential)	7/1/2014 6:40 AM
11	Noise, pollution, traffic, undesirable people.	7/1/2014 6:22 AM
12	TRAFFIC!!!! And with added people/cars more noise and pollution. You bring in more housing you bring crime, and I have notice crime has been increasing lately, mostly petty stuff but it is increasing.	6/30/2014 9:30 PM
13	We are in Victoria gardens and would like the area around our neighborhood to be residential area. There is already quick access to retail and commercial areas rather easy	6/30/2014 8:17 PM
14	Too much traffic as it is. The area continues to build, and grow except for roads and freeways and public transportation to accommodate the newer built up areas.	6/30/2014 6:53 PM
15	Obvious noise, traffic congestion, overdevelopment of houses on too small lots, etc.	6/30/2014 6:33 PM
16	Noise, traffic, pollution, over population, decrease in house value,	6/30/2014 5:51 PM
17	noise pollution, destroy the beauty of nature	6/30/2014 5:43 PM
18	traffic flow overwhelming existing streets and on/off freeway ramps	6/29/2014 8:08 AM
19	no concerns, roads are necessary.	6/28/2014 8:37 PM
20	Additional noise and traffic in the lback park neighborhoods.	6/28/2014 7:42 PM
21	I am concerned about noise and traffic and livability for Tualatin residents. Anything away from the highway exit should promote livability for Tualatin residents. Anything that promotes or creates the need for connectivity to/from Tualatin should leverage the freeway, not create more headaches for residents. Also, since WES is ultimately designed to be a commuter rail stretching to Marion County, I don't think it makes sense to add another stop in Tualatin. The Southwest Corridor Project should be the "alternative vehicle" to address transportation concerns for Tualatin residents who want to improve transportation connection options between the suburbs and to the city center.	6/28/2014 4:24 PM
22	anything that involves in and out driving/parking, etc along Boones Ferry or Grahams's Ferry will create a traffic and SAFETY problem. Hence my dislike for strip malls, low or high density living, or any other type of development where line of sight and slowing of traffic to enter and exit should be avoided at all costs!!!!	6/28/2014 12:25 PM
23	do not want more traffic going down Boones Ferry Rd by Tualatin HS. It already gets too congested and hard to turn into and out of the HS during peak times.	6/27/2014 9:05 AM
24	Air and noise pollution material affect the quality of residential life. I'm concerned that bringing in commercial and heavy industry development will result in added noise and air pollution.	6/26/2014 11:48 PM
25	Very concerned about noise and pollution since both are already problems. Children in our area are too exposed to diesel fumes and other pollutants.	6/26/2014 10:53 PM
26	My concerns would be increased traffic, and increased noise.	6/26/2014 4:20 PM

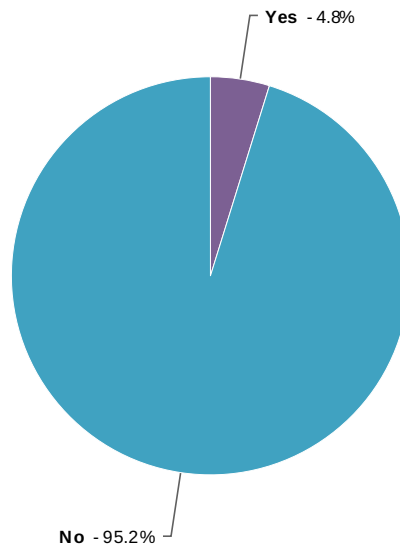
CIO Survey for Basalt Creek Planning Area



27	Obviously this is right next some nice neighborhoods in South Tualatin. I think those can co-exist with a transition to light industrial areas as is the case elsewhere in Tualatin. I would hate to see heavy industrial installations. The 124th extension and new East-West Connection WILL BE just as noisy (i.e. very) as Tualatin-Sherwood Road given all of the truck traffic they will attract / siphon away so that needs to be taken into account (if single family homes are planned then either some sort of barrier of a wall or businesses providing buffer otherwise those will be very undesirable places to live right along these streets. That said, making it limited access is also important otherwise it will end up like 99W through Tigard with slow traffic due to all of the turning in and out of driveways.	6/26/2014 12:59 PM
28	It will be difficult enough to deal with heavy truck traffic in the Basalt Creek Area. We do not need more development (businesses, high density housing) in this area of Tualatin. Please keep it natural.	6/26/2014 12:52 PM
29	I'm concerned with traffic, noise, and too much commercialism on the south end of Tualatin which is a quiet, residential community. I don't like the idea of stores and high density housing which will cause more traffic in the vicinity of my neighborhood.	6/26/2014 10:54 AM
30	I do not want anymore high density housing in Tualatin. No more apartments, condos, or row houses.	6/26/2014 9:45 AM
31	noise, traffic, pollution	6/26/2014 9:21 AM
32	Noise, traffic, reduced property values	6/26/2014 9:13 AM
33	Noise carries a lot in this area. In Victoria Woods, we can easily hear the Gun club out on Tonquin as if it were right next door. I would not want industrial or even light industrial in the Basalt Creek area as it would bring even more noise. Would not really want retail either as the traffic it will bring along Boones Ferry will increase and it's already getting pretty bad and brings a lot of road noise.	6/26/2014 9:03 AM
34	I am concerned about Highway noises, Diesel exhaust in the proximity of School, day care, athletic fields, etc. To span a bridge across a wetlands and overcome a steep incline doesn't seem to fit the lay of the land. I would rather see a Highway "tucked in" to the terrain.	6/26/2014 8:06 AM
35	With the extension of 124th avenue to Grahams Ferry Rd, there will be even more traffic on Day road. What options for travel between Grahams Ferry & Boones Ferry might be developed?	6/25/2014 9:19 PM
36	Noise, pollution, traffic and crime	6/25/2014 5:22 PM
37	I am concerned about most development of this area. I worry about changes to traffic, noise, resource use, loss of open space for the earth to breathe. Please make sure all development has high standards and requirements for the buildings and resource use of the area - low water use, low light pollution, enhanced wildlife zones, ponds (like the road-side water features along Sequoia Pkwy from Carmen Drive to Bonita Road in Tigard). LEED certified buildings and homes.	6/25/2014 4:39 PM
38	Pollution, noise and traffic. Must contain high green building standards. This development of all sorts can be good if accessible to neighbors by foot, bike and mass transit. All development cost must be paid for by developer and development. Should be net zero.	6/25/2014 3:50 PM
39	Too much traffic and reducing quality of life for current residence.	6/25/2014 3:12 PM
40	We already have a lot of noise from I5, but it is tolerable at our location, but I fear that deforestation in that area will change that.	6/25/2014 3:10 PM
41	Concern is regarding increase in traffic.	6/25/2014 10:12 AM
42	additional traffic congestion requiring more time to travel anywhere within area.	6/25/2014 7:46 AM
43	Very concerned about noise and pollution!	6/24/2014 10:35 PM
44	Pollution, increased traffic, how the constant (and big!) blasting from the quarry will affect any new structures	6/24/2014 9:49 PM

New Summary Report - 18 July 2014

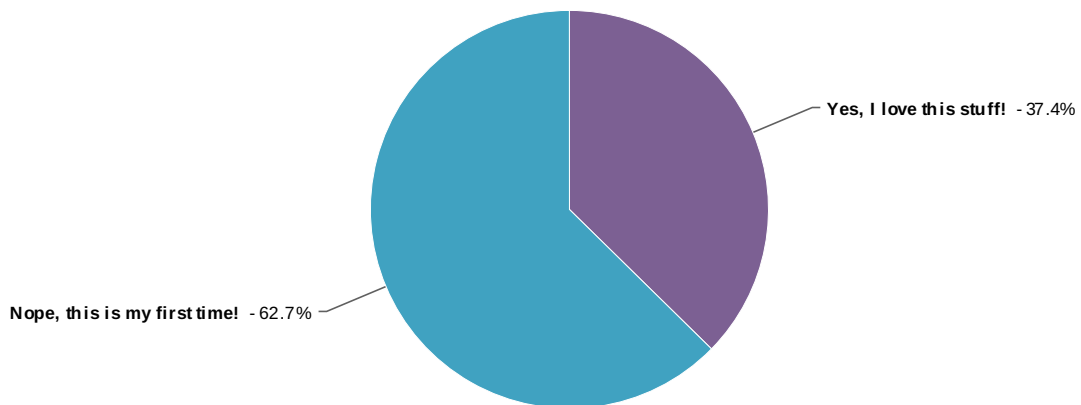
Survey: Basalt Creek Workshop Survey



1. Did you attend the June 17th Basalt Creek Workshop?




Yes	4.8%		8
No	95.2%		159
Total			167

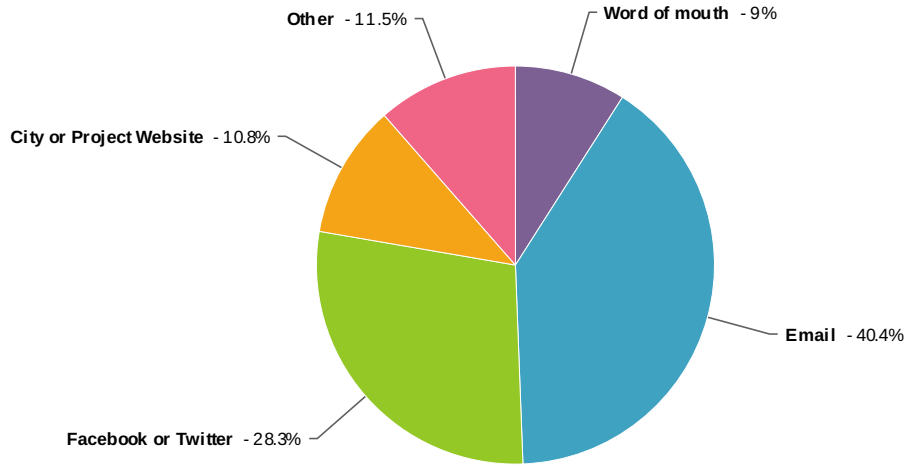
2. Have you participated in a planning workshop or activity with the City of Tualatin or City of Wilsonville before?

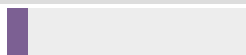


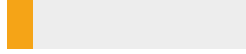
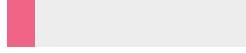


Yes, I love this stuff!	37.4%		62
Nope, this is my first time!	62.7%		105

Nope, this is my first time!	62.7%		104
		Total	166

3. How did you hear about this survey?



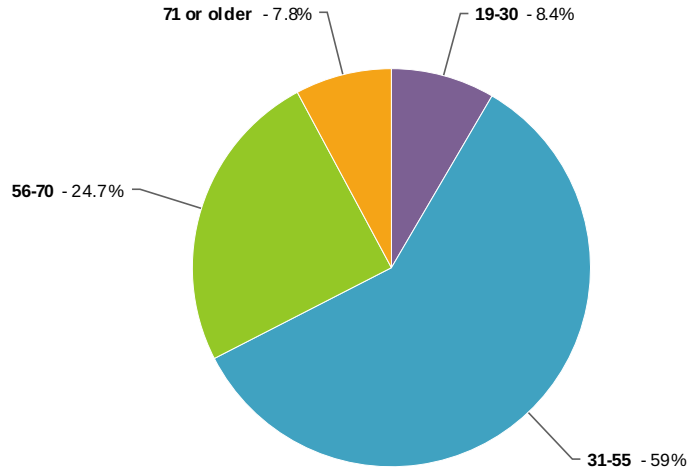
Word of mouth	9.0%		15
Email	40.4%		67
Facebook or Twitter	28.3%		47
City or Project Website	10.8%		18
Other	11.5%		19
		Total	166

Responses "Other"

Responses "Other"	Count
Left Blank	148
Home Builders Association	1
LETTER	1
PBJ	1
Portland Business Journal	6
The Wilsonville Spokesman	1
Wilsonville Newspaper	1
cio group	1
mailing	1
neighborhood website	1
newspaper	2

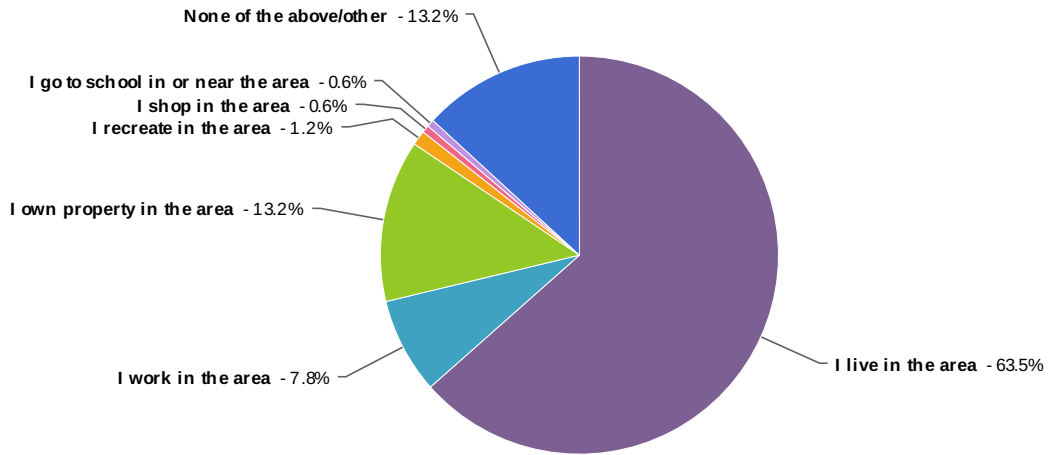
nextdoor	1
notification by letter	1
portland business journal	1

4. How old are you?



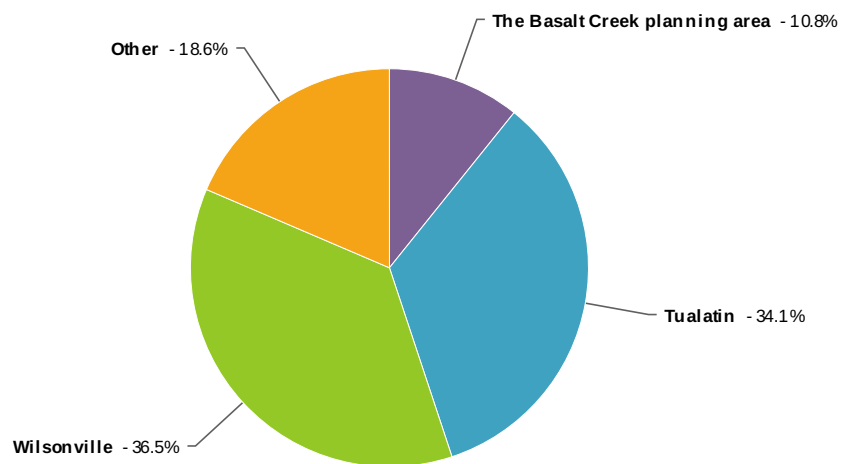
				Statistics	
Under 18	0.0%		0	Sum	6,523.0
19-30	8.4%		14	Average	39.3
31-55	59.0%		98	StdDev	14.9
56-70	24.7%		41	Max	71.0
71 or older	7.8%		13		
Total			166		





5. What is your primary connection to the Basalt Creek planning area?



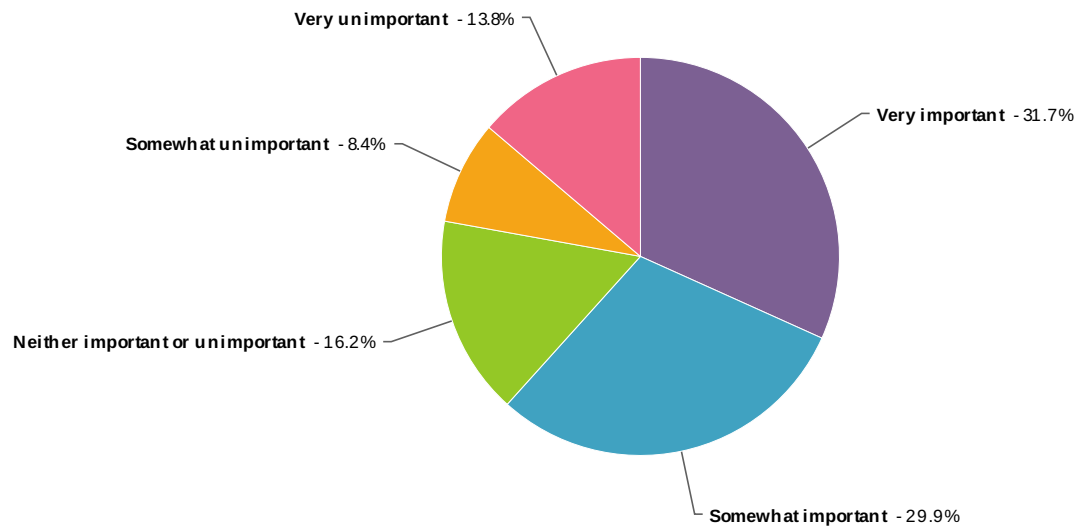
I live in the area	63.5%		106
I work in the area	7.8%		13
I own property in the area	13.2%		22
I recreate in the area	1.2%		2
I shop in the area	0.6%		1
I go to school in or near the area	0.6%		1
None of the above/other	13.2%		22
Total			167






6. Where do you live?



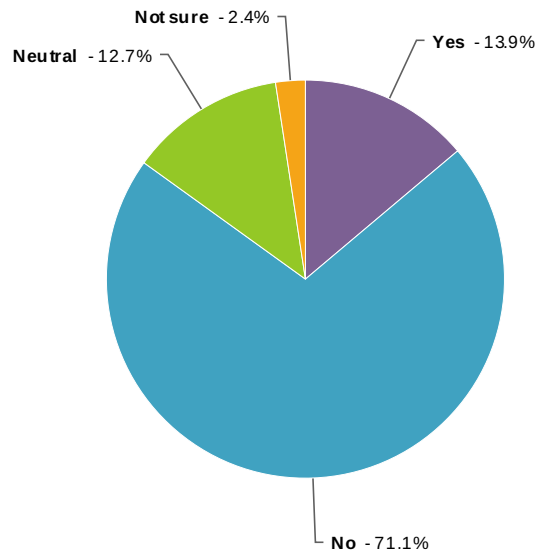
The Basalt Creek planning area	10.8%		18
Tualatin	34.1%		57
Wilsonville	36.5%		61
Other	18.6%		31
Total			167

7. How important is it to have retail services in Basalt Creek to support existing and future neighborhoods?



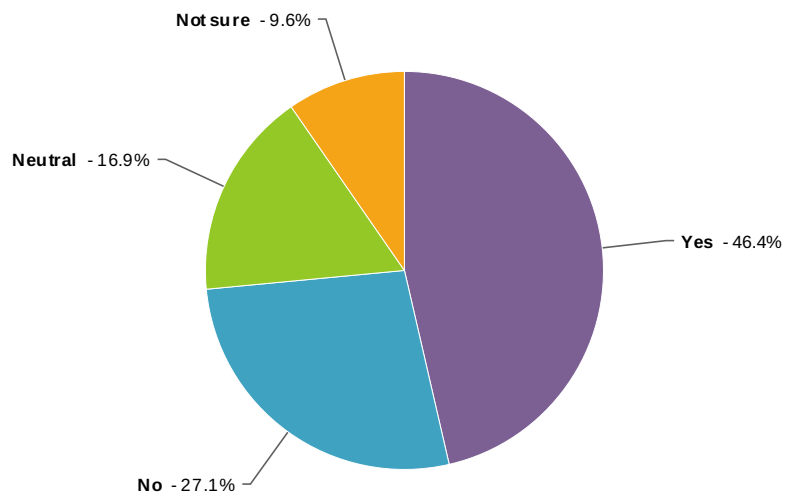
Very important	31.7%		53
Somewhat important	29.9%		50
Neither important or unimportant	16.2%		27
Somewhat unimportant	8.4%		14
Very unimportant	13.8%		23
Total			167

8. Is the following appropriate for Basalt Creek? Big Box retail (large-scale/auto-oriented)

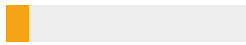


Yes	13.9%		23
No	71.1%		118
Neutral	12.7%		21
Not sure	2.4%		4
Total			166

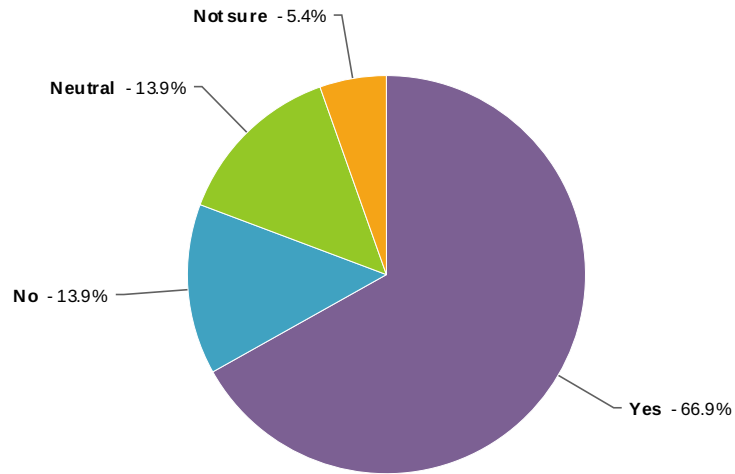
9. Is the following appropriate for Basalt Creek? Strip Retail (smaller-scale/auto-oriented)


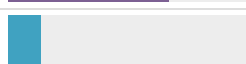
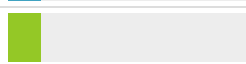



Yes	46.4%		77
No	27.1%		45
Neutral	16.9%		28

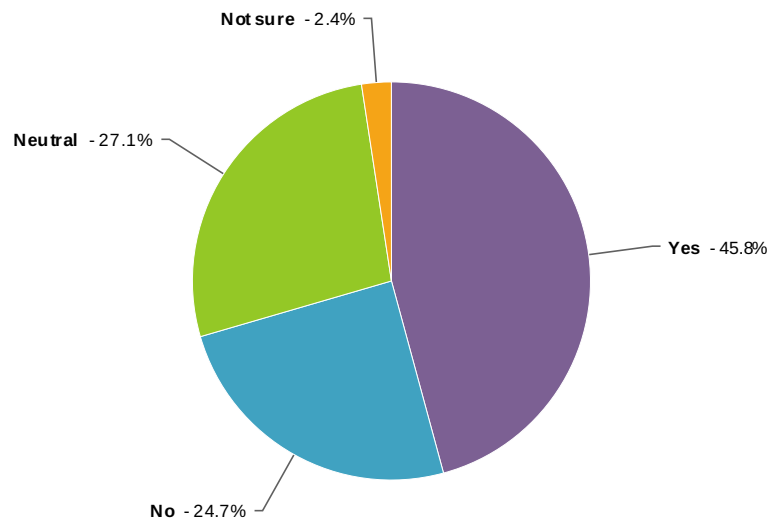
Not sure	9.6%		16
Total			166

10. Is the following appropriate for Basalt Creek? Neighborhood Retail (small-scale/oriented to pedestrians, transit and cars)



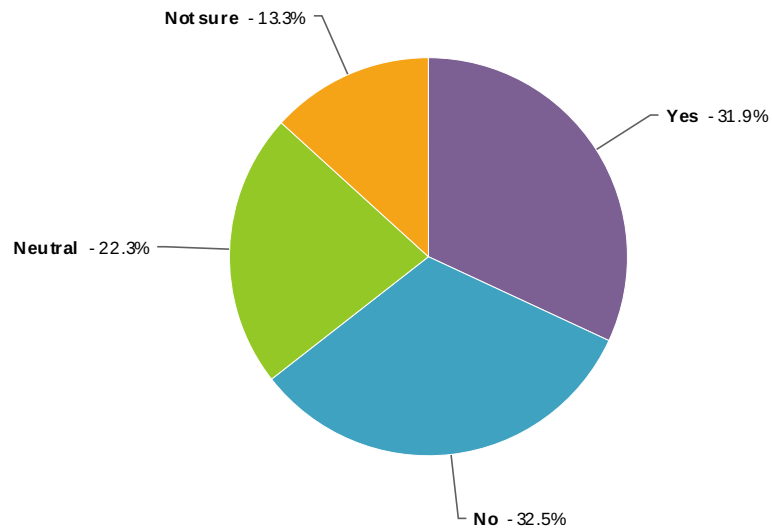
Yes	66.9%		111
No	13.9%		23
Neutral	13.9%		23
Not sure	5.4%		9
Total			166

11. Is the following appropriate for Basalt Creek? Corner/convenience retail (corner store)



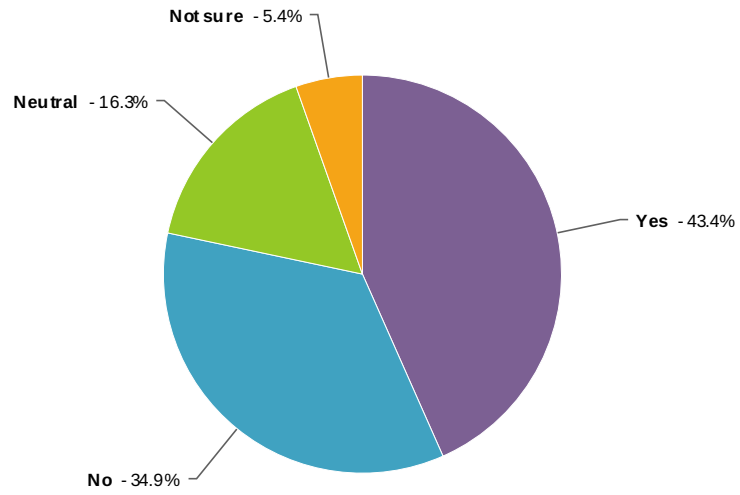
Yes	45.8%		76
No	24.7%		41
Neutral	27.1%		45
Not sure	2.4%		4
Total			166

12. Is the following appropriate for Basalt Creek? Retail internally oriented to employees on a campus



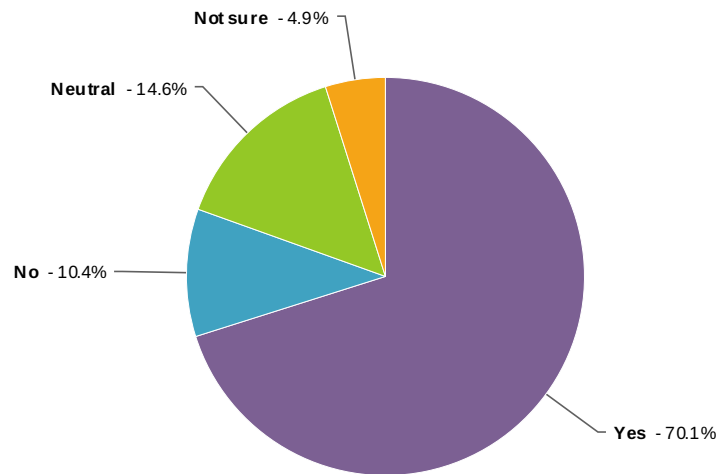
Yes	31.9%		53
No	32.5%		54
Neutral	22.3%		37
Not sure	13.3%		22
Total			166

13. Is the following appropriate for Basalt Creek? Supermarket

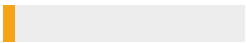


Yes	43.4%		72
No	34.9%		58
Neutral	16.3%		27
Not sure	5.4%		9
Total			166

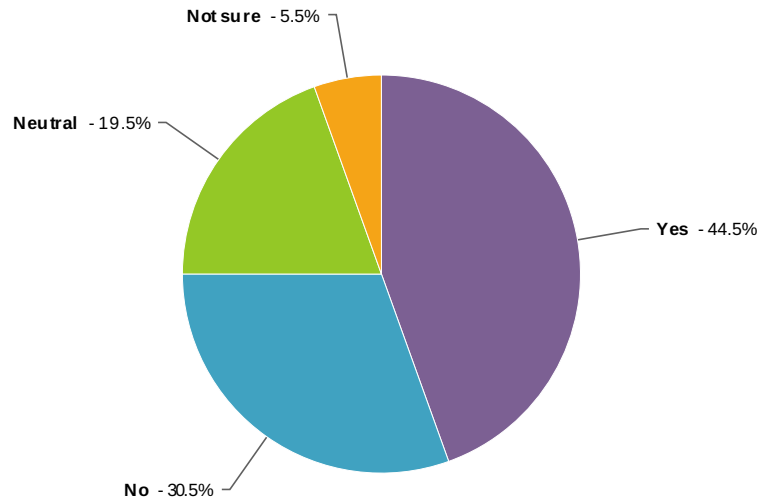
14. Is the following appropriate for Basalt Creek? Restaurants


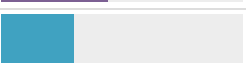




Yes	70.1%		115
No	10.4%		17
Neutral	14.6%		24

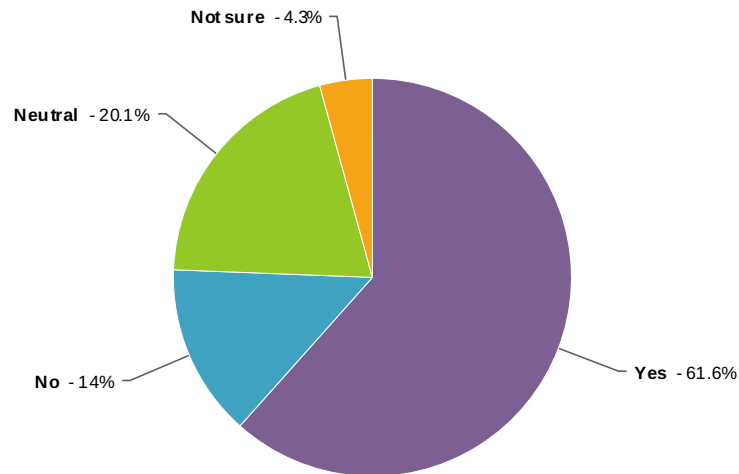
Not sure	4.9%		8
Total			164

15. Is the following appropriate for Basalt Creek? Medical Services



Yes	44.5%		73
No	30.5%		50
Neutral	19.5%		32
Not sure	5.5%		9
Total			164

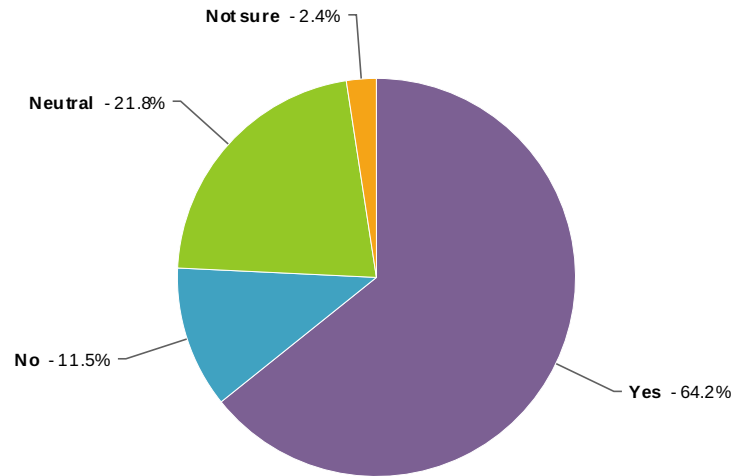
16. Is the following appropriate for Basalt Creek? Farm Stands



		
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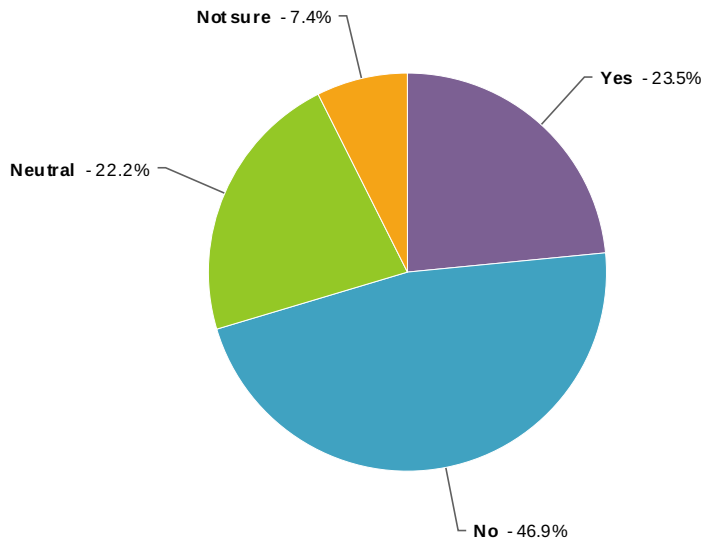
Yes	61.6%		101
No	14.0%		23
Neutral	20.1%		33
Not sure	4.3%		7
Total			164

17. Is the following appropriate for Basalt Creek? Coffee Shops



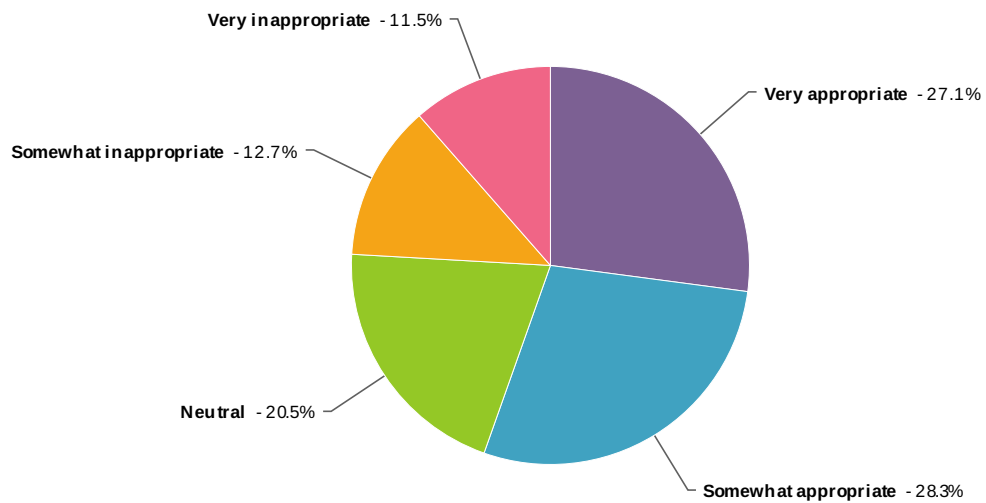
Yes	64.2%		106
No	11.5%		19
Neutral	21.8%		36
Not sure	2.4%		4
Total			165

18. Is the following appropriate for Basalt Creek? Contractor's Supplies and Services





Yes	23.5%		38
No	46.9%		76
Neutral	22.2%		36
Not sure	7.4%		12
Total			162

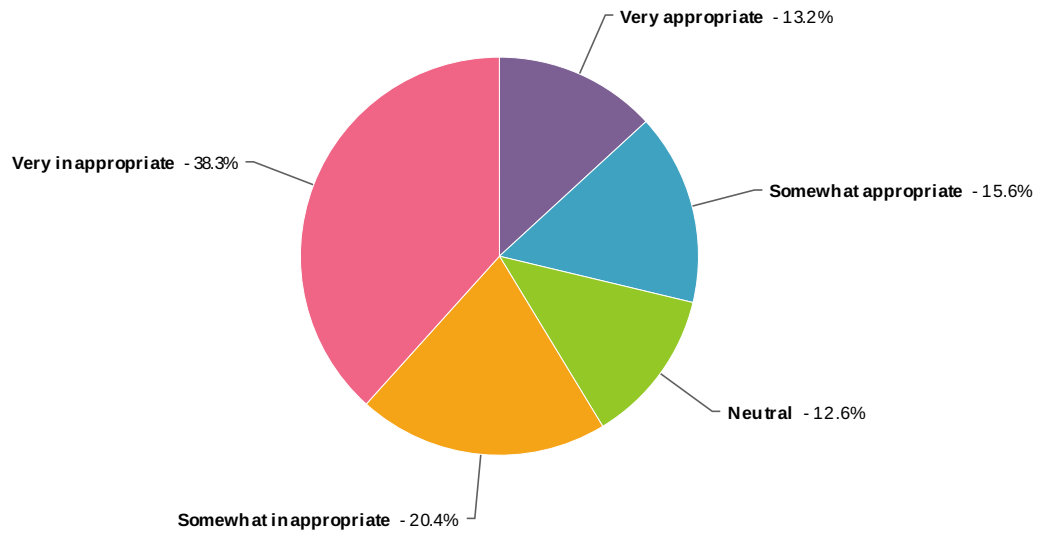
19. How appropriate is the following type of employment for this area?Office








Very appropriate	27.1%		45
Somewhat appropriate	28.3%		47
Neutral	20.5%		34

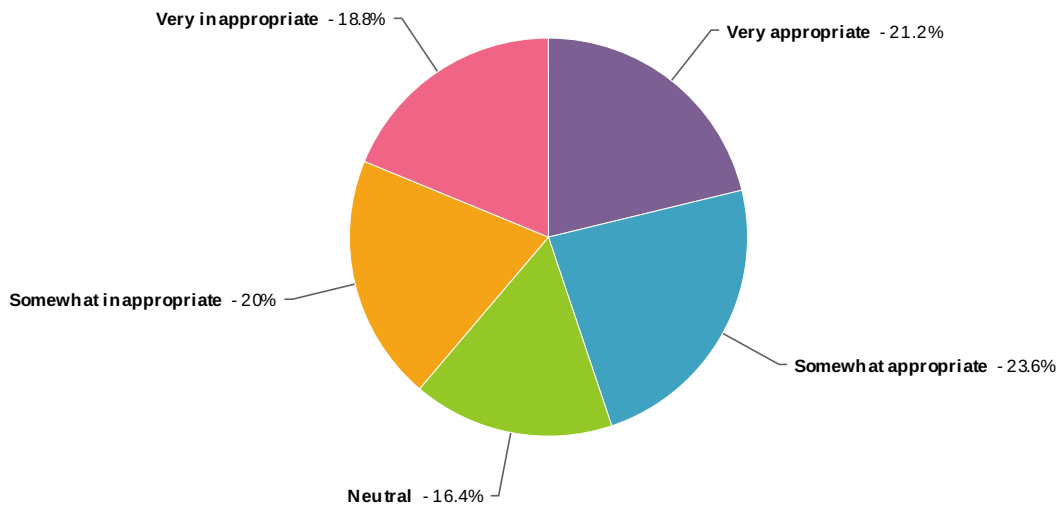
Somewhat inappropriate	12.7%		21
Very inappropriate	11.5%		19
Total			166

20. How appropriate is the following type of employment for this area? Warehousing



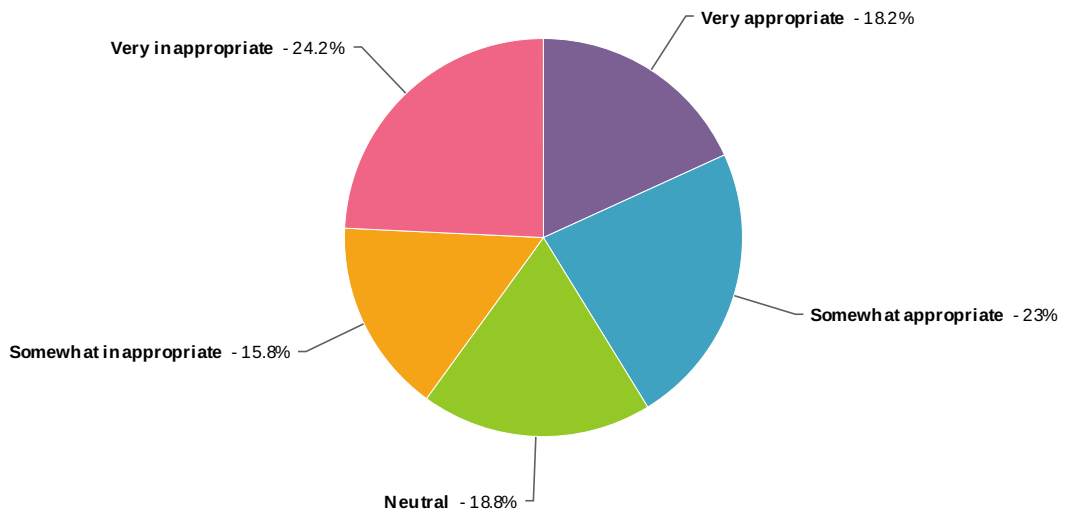
Very appropriate	13.2%		22
Somewhat appropriate	15.6%		26
Neutral	12.6%		21
Somewhat inappropriate	20.4%		34
Very inappropriate	38.3%		64
Total			167

21. How appropriate is the following type of employment for this area? Light Manufacturing



Very appropriate	21.2%		35
Somewhat appropriate	23.6%		39
Neutral	16.4%		27
Somewhat inappropriate	20.0%		33
Very inappropriate	18.8%		31
Total			165

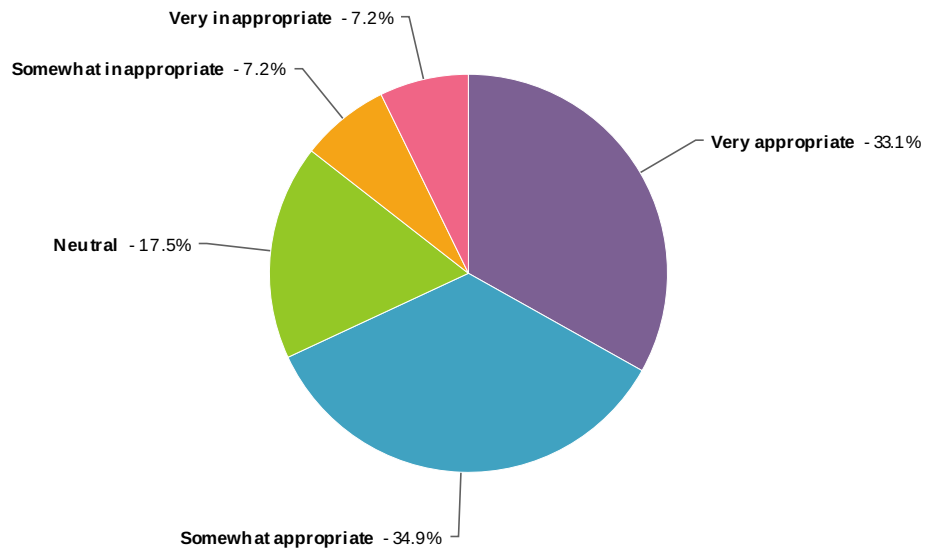
22. How appropriate is the following type of employment for this area? Industrial Flex Space



Very appropriate	18.2%		30
Somewhat appropriate	23.0%		38

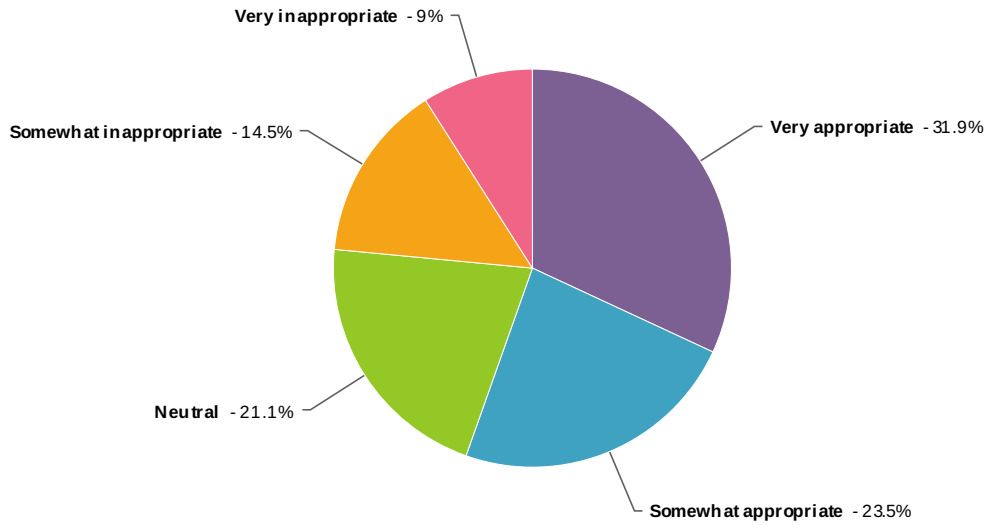
Neutral	18.8%		31
Somewhat inappropriate	15.8%		26
Very inappropriate	24.2%		40
Total			165

23. How appropriate is the following type of employment for this area? Small-Scale Retail



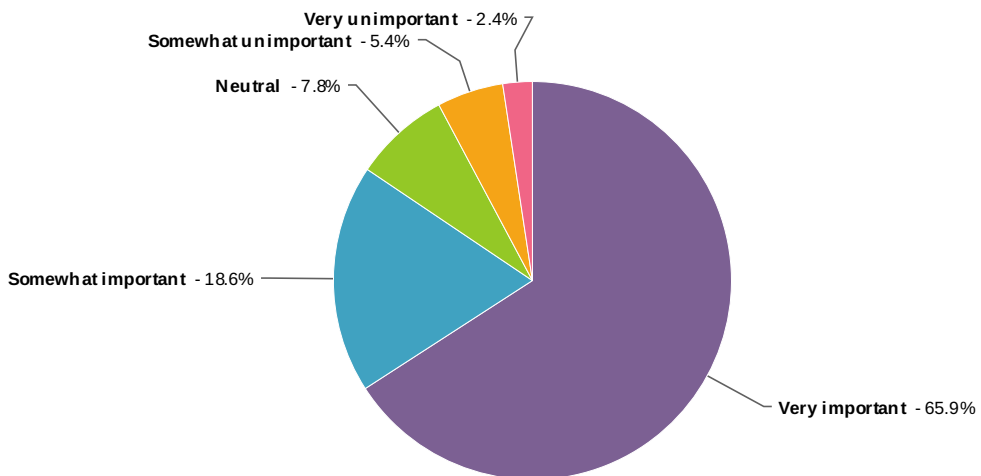
Very appropriate	33.1%		55
Somewhat appropriate	34.9%		58
Neutral	17.5%		29
Somewhat inappropriate	7.2%		12
Very inappropriate	7.2%		12
Total			166

24. How appropriate is the following type of employment for this area? A mix of all


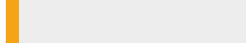
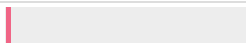


Very appropriate	31.9%		53
Somewhat appropriate	23.5%		39
Neutral	21.1%		35
Somewhat inappropriate	14.5%		24
Very inappropriate	9.0%		15
Total			166

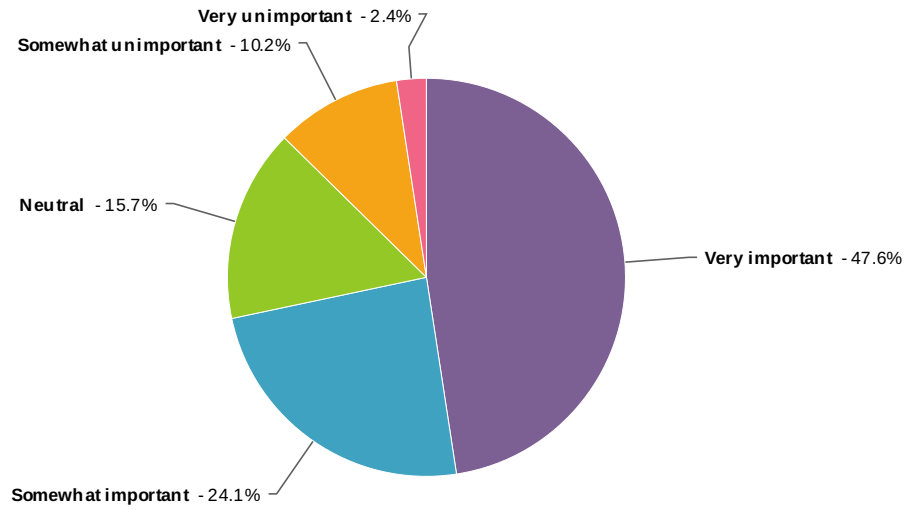
25. How important is protection of natural resources in Basalt Creek?


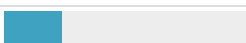
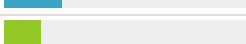
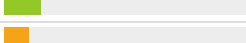
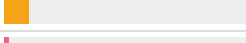


Very important	65.9%		110
Somewhat important	18.6%		31

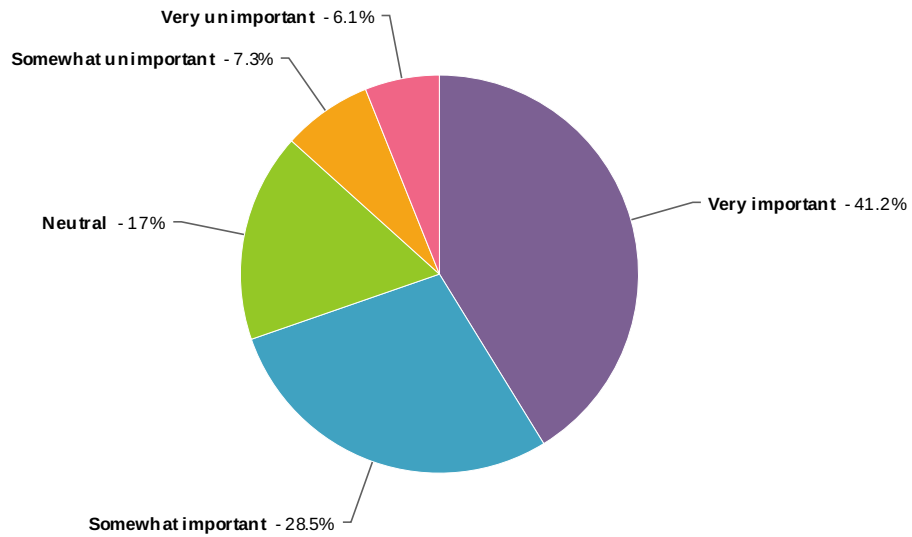
Neutral	7.8%		13
Somewhat unimportant	5.4%		9
Very unimportant	2.4%		4
Total			167

26. How important is protection of/access to historic resources in Basalt Creek?



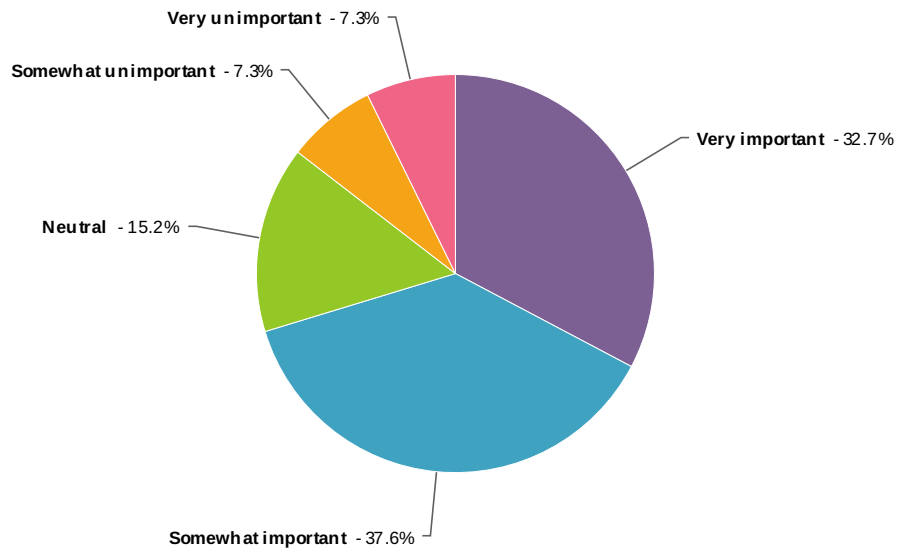
Very important	47.6%		79
Somewhat important	24.1%		40
Neutral	15.7%		26
Somewhat unimportant	10.2%		17
Very unimportant	2.4%		4
Total			166

27. How important is it that the public have access to Coffee Lake and Basalt Creek?


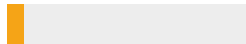
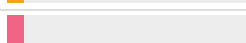


Very important	41.2%		68
Somewhat important	28.5%		47
Neutral	17.0%		28
Somewhat unimportant	7.3%		12
Very unimportant	6.1%		10
Total			165

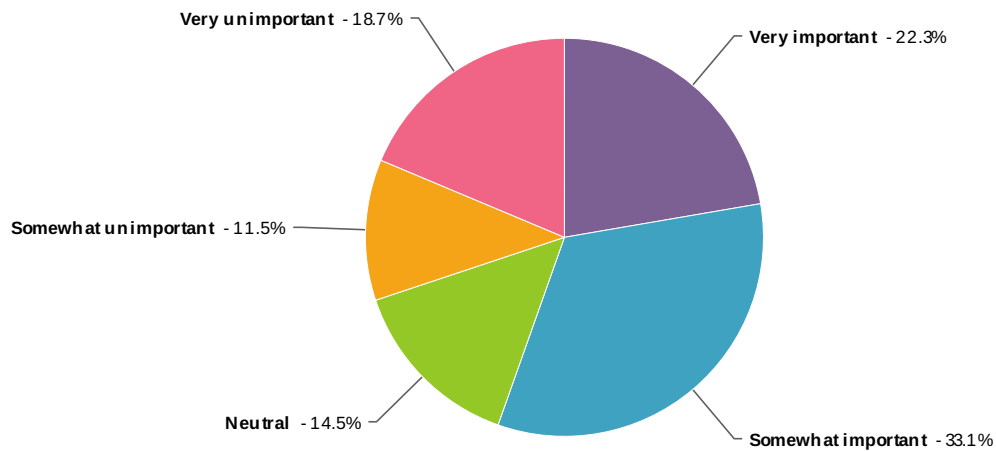
28. How important is it that the following be emphasized in this plan?Jobs

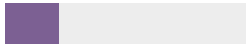
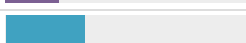
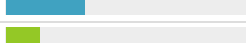
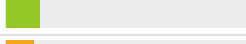
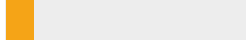


Very important	32.7%		54
Somewhat important	37.6%		62

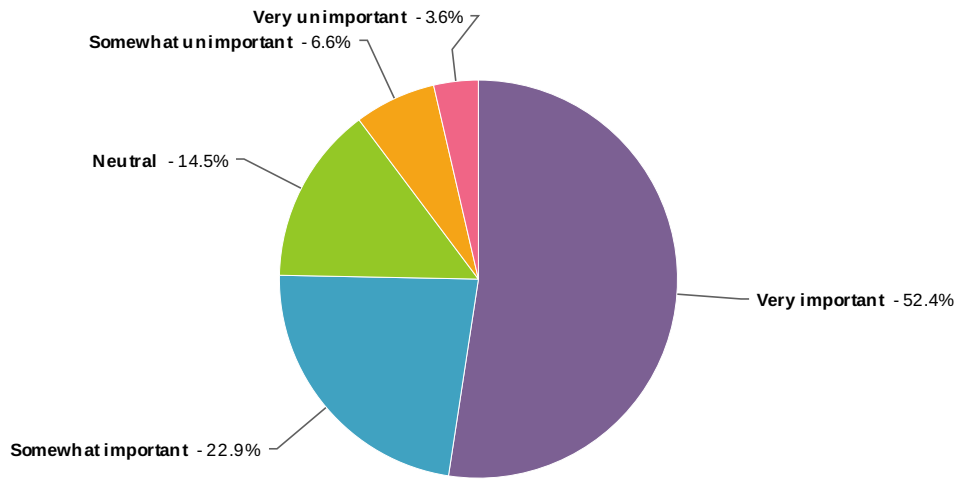
Neutral	15.2%		25
Somewhat unimportant	7.3%		12
Very unimportant	7.3%		12
Total			165

29. How important is it that the following be emphasized in this plan?Housing



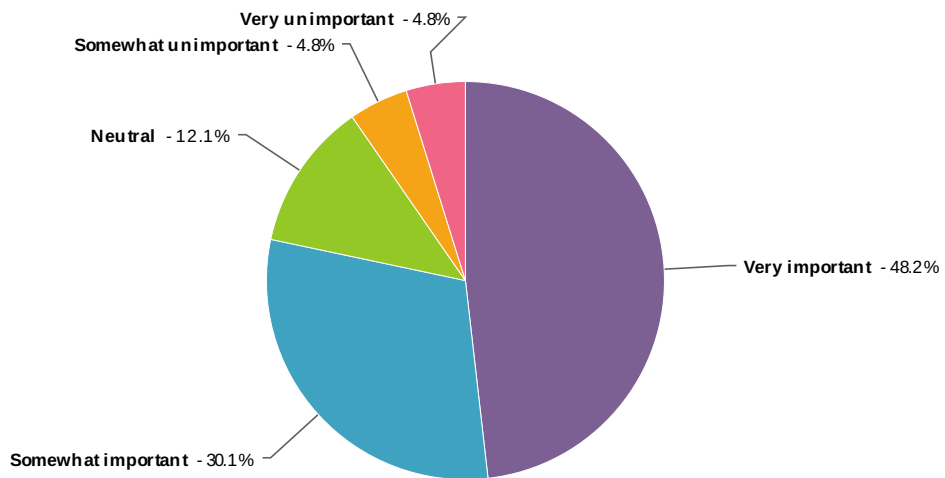
Very important	22.3%		37
Somewhat important	33.1%		55
Neutral	14.5%		24
Somewhat unimportant	11.5%		19
Very unimportant	18.7%		31
Total			166

30. How important is it that the following be emphasized in this plan?Environmental Protection


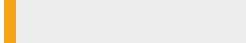
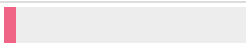


Very important	52.4%		87
Somewhat important	22.9%		38
Neutral	14.5%		24
Somewhat unimportant	6.6%		11
Very unimportant	3.6%		6
Total			166

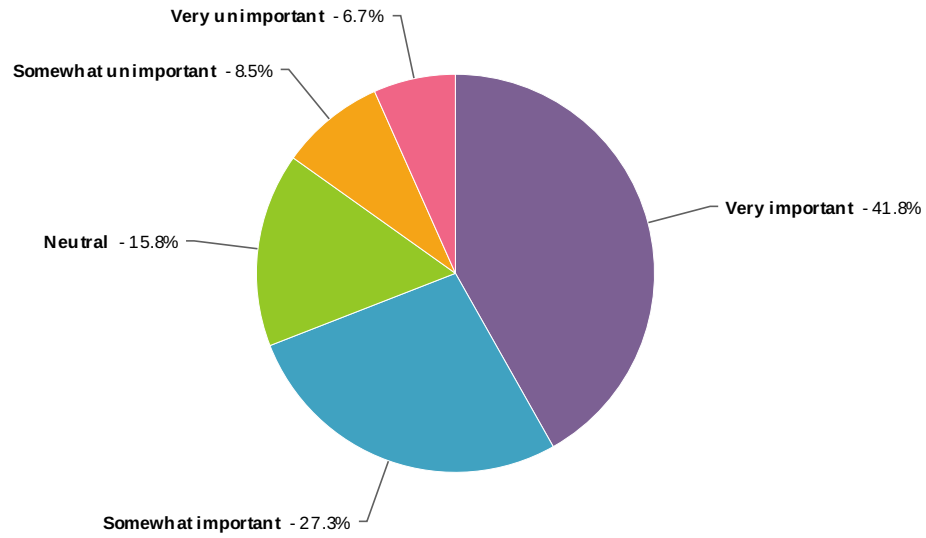
31. How important is it that the following be emphasized in this plan?Parks



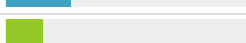
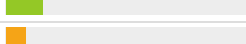
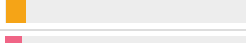


Very important	48.2%		80
Somewhat important	30.1%		50

Neutral	12.1%		20
Somewhat unimportant	4.8%		8
Very unimportant	4.8%		8
Total			166

32. How important is it that the following be emphasized in this plan? Transportation Choices



Very important	41.8%		69
Somewhat important	27.3%		45
Neutral	15.8%		26
Somewhat unimportant	8.5%		14
Very unimportant	6.7%		11
Total			165